



**APRIL 22, 2021 AGENDA
CITY OF CITRUS HEIGHTS CITY COUNCIL
7:00 PM REGULAR MEETING
City Hall Council Chambers
6360 Fountain Square Drive, Citrus Heights, CA**

PLEASE NOTE: In order to minimize the spread of the COVID 19 virus, Governor Newsom has issued Executive Orders that temporarily suspend some requirements of the Brown Act. Please be advised that the Council Chambers are closed to the public and that some, or all, Council Members may attend City Council meetings telephonically or otherwise electronically. The meeting will be held via webcast with **NO PHYSICAL LOCATION FOR PUBLIC ATTENDANCE.**

PLEASE SEE BELOW FOR ZOOM MEETING INFORMATION

Regular Meeting 7:00 p.m. Zoom Meeting – Members of the public may attend via Zoom in order to observe and address the meeting. Webinar link:

<https://us02web.zoom.us/j/87892609407>

For those individuals accessing the meetings through Zoom who wish to make a public comment, please use the Zoom hand raise function (or *9 if you join the webinar via telephone) and the host will unmute you when it is time to speak. Speakers will be limited to 3 minutes each. Alternatively, you may submit your comment via email to cityclerk@citrusheights.net or by completion of an online Speaker Card at <https://www.citrusheights.net/FormCenter/City-Council-Meetings-Speaker-Card-30>. Written public comments shall be limited to 250 words or less. Each comment will be read aloud by the City Clerk.

You are strongly encouraged to observe the City Council meetings on television live on Metro Cable 14, the government affairs channel on the Comcast, Consolidated Communications, and AT&T U-Verse cable systems and replayed on the following Monday at 9:00 a.m. Alternatively, members of the public can view the City Council meeting live webcast at <https://www.citrusheights.net/673/Live-City-Council-Meeting-Webcasts>.

If you need a disability-related modification or accommodation, to participate in this meeting, please contact the City Clerk's Office 916-725-2448, cityclerk@citrusheights.net, or City Hall 6360 Fountain Square Drive at least 48 hours prior to the meeting. TDD: California Relay Service 7-1-1.

April 22, 2021 Agenda Packet

Documents:

[APRIL 22, 2021 AGENDA PACKET.PDF](#)

CALL REGULAR MEETING TO ORDER

1. Flag Salute
2. Roll Call: Council Members: Bruins, Daniels, Schaefer, Middleton, Miller
3. Video Statement

APPROVAL OF AGENDA

PRESENTATIONS

4. Proclamation Of The City Of Citrus Heights Proclaiming April As Donate Life Month
5. Proclamation Of The City Of Citrus Heights Proclaiming April As Fair Housing Month

COMMENTS BY COUNCIL MEMBERS AND REGIONAL BOARD UPDATES

PUBLIC COMMENT

CONSENT CALENDAR

It is recommended that all consent items be acted on simultaneously unless separate discussion and/or action are requested by a Council Member.

6. SUBJECT: Approval Of Minutes
RECOMMENDATION: Approve the Minutes of the Special and Regular Meeting of April 8, 2021 and Special Meeting of April 12, 2021
7. SUBJECT: Police Department Vehicle Replacement
STAFF REPORT: R. Lawrence/ C. Burnett
RECOMMENDATION: The Following is Recommended
 - a. Adopt Resolution No. 2021-____, a Resolution of the City Council of the City of Citrus Heights, California, Approving the Transfer of \$140,000 from the General Fund to the Capital Replacement Fund for the Purchase of Police Department Vehicles
 - b. Adopt Resolution No. 2021-____, a Resolution of the City Council of the City of Citrus Heights, California, Approving the Purchase of Two Police Motorcycles utilizing Citizens' Option for Public Safety (COPS) Funding
8. SUBJECT: Placer-Sacramento Gateway Phase 1 Project, Auburn Boulevard Phase 2 Project – Approval Of Caltrans Baseline Agreement
STAFF REPORT: M. Poole
RECOMMENDATION: Adopt Resolution No. 2021-____, a Resolution of the City Council of the City of Citrus Heights, California, Authorizing the City Manager to Execute the Program Baseline Agreement for the Placer-Sacramento Gateway – Phase 1 Project, Auburn Boulevard Complete Streets Project Phase 2

REGULAR CALENDAR

9. SUBJECT: Approval Of Budget For Fiscal Years 2021-22 And 2022-23 And Authorization For Execution Of An Agreement With Greater Sacramento Economic Council
STAFF REPORT: W. Zenoni/ T. Nossardi
RECOMMENDATION: The Following is Recommended;

a. Adopt Resolution approving the City of Citrus Heights Two Year Budget for Fiscal Years 2021-22 and 2022-23

b. Adopt Resolution authorizing the City Manager to execute an agreement between the Greater Sacramento Economic Council and the City of Citrus Heights in an amount not to exceed \$35,238 for Fiscal Year 2021-22

10. SUBJECT: Transportation Impact Study And SB743 Implementation Guidelines
STAFF REPORT: C. McDuffee/ C. Kempenaar/ L. Blomquist
RECOMMENDATION: The Following is Recommended;

a. Review the Transportation Impact Study Guidelines and SB743 Guidelines

b. Adopt Resolution No. 2021-__ a Resolution of the City Council of the City of Citrus Heights, Adopting the Transportation Impact Study and SB743 Guidelines; finding that the Thresholds of Significance have been Promulgated Pursuant to the California Environmental Quality Act (CEQA) Guidelines 15064.7; and Determining the Adoption not a "Project" Pursuant to CEQA Guidelines Sections 15060(c)(3) and 15378

PUBLIC HEARINGS

11. SUBJECT: CARES Act Amendment No. 3 To The 2020-2024 Consolidated Plan And 2020 Action Plan For Coronavirus (CDBG-CV 1&3) Funding
STAFF REPORT: C. McDuffee/ S. Cotter
RECOMMENDATION: The Following is Recommended;

a. Hold a public hearing on the proposed CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and 2020 Action Plan for Coronavirus (CDBG-CV 1 & 3) Funding

b. Adopt Resolution No. 2021-____ Adopting the CARES Act Amendment No. 3 to the 2020-2024 CDBG Consolidated Plan and 2020 Action Plan (Exhibit A) and Authorizing the City Manager to Execute All Contracts Necessary to Carry Out the CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and 2020 Action Plan for Coronavirus (CDBG-CV 1 & 3) Funding

DEPARTMENT REPORTS

CITY MANAGER ITEMS

ITEMS REQUESTED BY COUNCIL MEMBERS / FUTURE AGENDA ITEMS

ADJOURNMENT

**CITY OF CITRUS HEIGHTS
CITY COUNCIL
Regular Meeting of Thursday, April 22, 2021
Regular Meeting 7:00 p.m.**

HOW TO PARTICIPATE:

The City of Citrus Heights welcomes your interest and involvement in the City's legislative process. If you would like to provide comments to the City Council, please use the Zoom hand raise function (or *9 if you join the webinar via telephone) and the host will unmute you when it is time to speak. Speakers will be limited to 3 minutes each. Alternatively, you may submit your comment via email to cityclerk@citrusheights.net or by completion of an online Speaker Card at <https://www.citrusheights.net/FormCenter/City-Council-Meetings-Speaker-Card-30>. Written public comments shall be limited to 250 words or less. Each comment will be read aloud by the City Clerk.

Regular Meeting 7:00 p.m. Zoom Meeting – Members of the public may attend via Zoom in order to observe and address the meeting. Webinar link:

<https://us02web.zoom.us/j/87892609407>

The City provides two ways to watch a City Council meeting.

ONLINE	ON TELEVISION
 <p>Watch the livestream and replay past meetings on the City website. www.citrusheights.net</p>	 <p>Watch live and replays of meetings on Sac Metro Cable, Channel 14.</p>

PLEASE NOTE: In order to minimize the spread of the COVID 19 virus, Governor Newsom has issued Executive Orders that temporarily suspend some requirements of the Brown Act. Please be advised that the Council Chambers are closed to the public and that some, or all, Council Members may attend City Council meetings telephonically or otherwise electronically. The meeting will be held via webcast with **NO PHYSICAL LOCATION FOR PUBLIC ATTENDANCE.**

If you need a disability-related modification or accommodation, to participate in this meeting, please contact the City Clerk's Office 916-725-2448, cityclerk@citrusheights.net, or City Hall 6360 Fountain Square Drive at least 48 hours prior to the meeting. TDD: California Relay Service 7-1-1.

April 16, 2021



Amy Van, City Clerk

**REGULAR CITY COUNCIL MEETING
7:00 PM**

CALL REGULAR MEETING TO ORDER

1. Flag Salute
2. Roll Call: Council Members: Bruins, Daniels, Schaefer, Middleton, Miller
3. Video Statement

APPROVAL OF AGENDA

PRESENTATIONS

4. Proclamation of the City of Citrus Heights Proclaiming April as Donate Life Month
5. Proclamation of the City of Citrus Heights Proclaiming April as Fair Housing Month

COMMENTS BY COUNCIL MEMBERS AND REGIONAL BOARD UPDATES

PUBLIC COMMENT

CONSENT CALENDAR

It is recommended that all consent items be acted on simultaneously unless separate discussion and/or action are requested by a Council Member.

6. **SUBJECT:** Approval of Minutes
RECOMMENDATION: Approve the Minutes of the Special and Regular Meeting of April 8, 2021 and Special Meeting of April 12, 2021
7. **SUBJECT:** Police Department Vehicle Replacement
STAFF REPORT: R. Lawrence/ C. Burnett
RECOMMENDATION: The Following is Recommended
 - a. Adopt Resolution No. 2021-____, a Resolution of the City Council of the City of Citrus Heights, California, Approving the Transfer of \$140,000 from the General Fund to the Capital Replacement Fund for the Purchase of Police Department Vehicles
 - b. Adopt Resolution No. 2021-____, a Resolution of the City Council of the City of Citrus Heights, California, Approving the Purchase of Two Police Motorcycles utilizing Citizens' Option for Public Safety (COPS) Funding
8. **SUBJECT:** Placer-Sacramento Gateway Phase 1 Project, Auburn Boulevard Phase 2 Project – Approval of Caltrans Baseline Agreement
STAFF REPORT: M. Poole

RECOMMENDATION: Adopt Resolution No. 2021-____, a Resolution of the City Council of the City of Citrus Heights, California, Authorizing the City Manager to Execute the Program Baseline Agreement for the Placer-Sacramento Gateway – Phase 1 Project, Auburn Boulevard Complete Streets Project Phase 2

REGULAR CALENDAR

9. **SUBJECT:** Approval of Budget for Fiscal Years 2021-22 and 2022-23 and Authorization for Execution of an Agreement with Greater Sacramento Economic Council

STAFF REPORT: W. Zenoni/ T. Nossardi

RECOMMENDATION: The Following is Recommended;

- a. Adopt Resolution approving the City of Citrus Heights Two Year Budget for Fiscal Years 2021-22 and 2022-23
- b. Adopt Resolution authorizing the City Manager to execute an agreement between the Greater Sacramento Economic Council and the City of Citrus Heights in an amount not to exceed \$35,238 for Fiscal Year 2021-22

10. **SUBJECT:** Transportation Impact Study and SB743 Implementation Guidelines

STAFF REPORT: C. McDuffee/ C. Kempenaar/ L. Blomquist

RECOMMENDATION: The Following is Recommended;

- a. Review the Transportation Impact Study Guidelines and SB743 Guidelines
- b. Adopt Resolution No. 2021-__ a Resolution of the City Council of the City of Citrus Heights, Adopting the Transportation Impact Study and SB743 Guidelines; finding that the Thresholds of Significance have been Promulgated Pursuant to the California Environmental Quality Act (CEQA) Guidelines 15064.7; and Determining the Adoption not a "Project" Pursuant to CEQA Guidelines Sections 15060(c)(3) and 15378

PUBLIC HEARING

11. **SUBJECT:** CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and 2020 Action Plan for Coronavirus (CDBG-CV 1&3) Funding

STAFF REPORT: C. McDuffee/ S. Cotter

RECOMMENDATION: The Following is Recommended;

- a. Hold a public hearing on the proposed CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and 2020 Action Plan for Coronavirus (CDBG-CV 1 & 3) Funding
- b. Adopt Resolution No. 2021-____ Adopting the CARES Act Amendment No. 3 to the 2020-2024 CDBG Consolidated Plan and 2020 Action Plan (Exhibit A) and Authorizing the City Manager to Execute All Contracts Necessary to Carry Out the CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and 2020 Action Plan for Coronavirus (CDBG-CV 1 & 3) Funding

DEPARTMENT REPORTS

CITY MANAGER ITEMS

ITEMS REQUESTED BY COUNCIL MEMBERS/ FUTURE AGENDA ITEMS

ADJOURNMENT

PROCLAMATION

of the
City of Citrus Heights
Proclaiming April as
Donate Life Month

WHEREAS, nearly 110,000 Americans are currently on the organ donation waiting list, including over 21,000 Californians; and

WHEREAS, 95 percent of Americans believe organ, eye and tissue donation is the right thing to do, yet approximately 50 percent nationally, and 37 percent in California, are actually registered to become donors; and

WHEREAS, Sierra Donor Services serves at the center of a dynamic, interconnected system that involves donor families, local hospitals and transplant centers across Northern California in the delicate and time-sensitive mission of saving lives through transplant; and

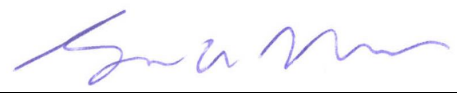
WHEREAS, in 2020, even in the midst of a global pandemic, Sierra Donor Services had its most successful year in its nearly 40 year history, making 396 organ transplants possible thanks to the selfless generosity of 139 Northern California organ donors; and

WHEREAS, increasing the number of registered organ, eye and tissue donors is a benefit to all Californians and provides more lifesaving opportunities across the state and nation; and


WHEREAS, registering to become an organ, eye and tissue donor is easily accomplished online at BeTheGiftToday.com or when registering or renewing a California driver's license or ID; and

NOW, THEREFORE, BE IT RESOVLED that the City Council of the City of Citrus Heights, does hereby proclaim April as "Donate Life Month" throughout the City of Citrus Heights.

IN WITNESS WHEREOF, we have set our hand and seal this 22th day of April 2021.



STEVE MILLER, MAYOR

ATTEST: 

AMY VAN, CITY CLERK

PROCLAMATION

of the
City of Citrus Heights
Proclaiming April as
Fair Housing Month

WHEREAS, Fair Housing Month is celebrated each April across the United States to commemorate the enactment of the federal Fair Housing Act of 1968; and

WHEREAS, the Fair Housing Act prohibits discrimination in the sale, rental and financing of dwellings, and in other housing-related transactions, based on race, color, national origin, religion, sex, familial status and disability; and

WHEREAS, as we recognize Fair Housing Month, it is timely that we reflect on the importance in communities, states, and the nation to affirm civil rights and oppose bigotry and discrimination; and

WHEREAS, the City Citrus Heights affirms that every American in choosing where they want to live has a right to live in dignity and safety and without fear of discrimination, and that the city supports state and federal protections against discrimination in the sale and rental of housing; and

WHEREAS, the City of Citrus Heights supports the education of homeowners, realtors, landlords, property managers, homebuyers, and tenants regarding their rights and responsibilities under federal and state fair housing laws; and

WHEREAS, the City of Citrus Heights is proud of its commitment to fair housing while recognizing the need to affirmatively further fair housing in its policies and practices and is committed to continuing to work to identify and remove impediments to fair housing choices.

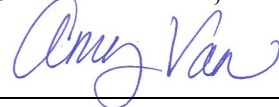
NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Citrus Heights, does hereby proclaim the month of April 2021 as Fair Housing Month and confirms its commitment to ensure there is equal access to housing for every member of our community.

IN WITNESS WHEREOF, we have set our hand and seal this 22nd day of April 2021.

ATTEST:



STEVE MILLER, MAYOR



AMY VAN, CITY CLERK

**CITY OF CITRUS HEIGHTS
CITY COUNCIL
MINUTES
Special/Regular Meeting of Thursday, April 8, 2021
Virtually from the City Hall Council Chambers
6360 Fountain Square Drive, Citrus Heights, CA**

CALL SPECIAL MEETING TO ORDER

The special council meeting was called to order at 5:00 p.m. by Mayor Miller.

- | | | | |
|----|------------|--------------------------|---|
| 1. | Roll Call: | Council Members present: | Bruins, Daniels, Schaefer, Middleton, Miller |
| | | Council Members absent: | None |
| | | Staff present: | Boyd, Cooley, Jones, Lawrence, McDuffee, Nossardi, Van, and Zenoni. |

PUBLIC COMMENT

None

STUDY SESSION

2. Review and Discuss Fiscal Year 2021/2022 and 2022/2023 Draft Annual Budget

City Manager Boyd stated the Council will receive an overview of the FY 2021-22 and FY 2022-23 proposed budget, along with Capital Improvement Program and Community Support funding.

Department representatives provided an overview of the accomplishments for the FY 2019-20 and 2020-21 two-year budget cycle and highlighted department goals included in the FY 2021-22 and 2022-23 budget cycle.

Interim Administrative Services Director Zenoni provided an overview of the General Fund and the proposed FY 2021-22 and 2022-23 budget. The proposed budget does not take into account any federal stimulus funds.

Operations Manager Cave provided an update on the draft Capital Improvement Project (CIP) list. She provided an overview of details for proposed capital improvements that the City desires to undertake in the next five years. The CIP is used as a supplement to the City's annual budget and identifies potential funding sources and expenditures to rehabilitate, restore, improve, and add to the City's infrastructure. The FY 2021-22 through 2025-26 CIP is organized around several project categories including transportation capital projects, transportation maintenance, storm water capital projects, and general-purpose projects. The following projects are included in the 2021-22 – 2025-26 CIP:

- Advanced Traffic Management
- Amsell Court Storm Drain Improvements
- Annual ADA Accessibility and Drainage Improvements
- Antelope Road/ i-80 Safety Improvements Project
- Arcade Cripple Creek Trail Project
- Auburn Boulevard Complete Streets Revitalization
- Blayden Court Storm Drain Improvements
- Bonita-Old Auburn Storm Drain Improvements

- Chula Vista Storm Drain Improvements
- Greenback Lane Complete Streets
- Mariposa Safe Routes to School Phase 4
- Mariposa-Sylvan Valley Storm Drain Improvements
- Minnesota, Anderson, and Canady Infiltration Basins and Storm Drain Improvements
- Multi-Modal Transportation Safety Program Improvements Project
- Neighborhood Creek Bank and Riparian Corridor Restoration
- Neighborhood Areas Drainage Master Plans
- Old Auburn Complete Streets Plan Phase 1
- Pavement Restoration Program
- San Juan Complete Streets
- Storm Drain Pipe Rehabilitation Program
- Sunrise Blvd. Complete Streets
- Various Signalized Intersection Safety Improvements
- Wonder Street Storm Drain Improvements

Assistant to the City Manager Cooley provided an overview of the Community Support Funding and the requests received. She addressed new sources of funding could be utilized to fulfill these requests, such as Permanent Local Housing Allocation, CDBG Cares Act, and the American Rescue Plan. She presented the following list of funding requests and the Finance Committee's recommendation to move certain applicants to alternate funding sources.

<u>Applicant</u>	<u>Community Support</u>	<u>Move to CDBG/Stimulus</u>
Campus Life		15,000
Citrus Heights Marching Band	8,000	
City Manager's Discretionary Fund	8,000	
Diversity Employment Outreach	2,499	
Meals on Wheels		82,000
River City Cat Rescue	5,000	
Single Mom Strong		9,100
Sunrise Christian Food Ministry		6,720
Total	\$18,499	\$112,820

The next steps for Community Support include CDBG-CV CARES (Round 3) Public Hearing on April 22 to fund eligible Community Support Funding applicants and reallocate the Round 1 Great Plates allocation of \$91,672 to supplemental Navigator Fund, transfer Navigator contract from General Fund/CDBG beginning in May/June, and creating a funding plan in May/June.

Public Comment

City Clerk Van read a public comment from Jennifer Massello with Meals on Wheels, "Meals on Wheels by ACC supports the City Staff's recommendation to move our funding request from General Funds to CDBG-CV funds and will work with the City Staff to find mutually beneficial solutions that allow MoW to continue to support the needs of Citrus Heights seniors during and after the pandemic."

Assistant to the City Manager Cooley provided an overview of the Neighborhood Improvement Project Funding program. Due to COVID-19 restrictions, no project applications have been received. After consulting with the Finance Committee, applications will be accepted on a continual basis to allow Neighborhoods to meet and discuss future projects.

ADJOURNMENT

Mayor Miller adjourned the special meeting at 6:45 p.m.

CALL REGULAR MEETING TO ORDER

The regular council meeting was called to order at 7:00 p.m. by Mayor Miller.

1. The Flag Salute was led by Mayor Miller.
2. Roll Call: Council Members present: Bruins, Daniels, Schaefer, Middleton, Miller
Council Members absent: None
Staff present: Boyd, Jones, Kempenaar, McDuffee, Reid, Van, and department directors.
3. The video statement was read by City Clerk Van.

APPROVAL OF AGENDA

ACTION: On a motion by Council Member Bruins, seconded by Vice Mayor Middleton, the City Council approved the agenda.

AYES:	Bruins, Daniels, Schaefer, Middleton, Miller
NOES:	None
ABSENT:	None

PRESENTATION

4. Presentation by Congressman Ami Bera on the American Rescue Plan

Congressman Bera presented an overview of the recently passed American Rescue Plan. Congressman Bera mentioned that with the creation of the American Rescue Plan, the focus was providing aide to small business owners and some of the most vulnerable populations effected by the COVID-19 pandemic. The City of Citrus Heights is estimated to receive over \$15 million dollars from the American Relief Plan. These funds can be used for responding to the COVID-19 public health emergency, providing hazard pay to essential state and local government employees, replace revenue lost by the pandemic, investments in water, sewer, or broad infrastructure, or funds can be transferred to private nonprofit organizations or special-purpose units that help provide government services. Funds cannot be utilized to offset pension funds. It is expected that the City of Citrus Heights will receive 50% of the allocation no later than 60 days after the bill was signed on March 11th. The remaining 50% will be sent no later than 12 months after the first payment. Funds will be available until December 31, 2024. Other highlights of the American Rescue Plan include funding for education, housing, healthcare, and small businesses.

5. Presentation by Gary Goodman with Sacramento – Yolo Mosquito and Vector Control District

Gary Goodman, District Manager of the Sacramento – Yolo Mosquito and Vector Control District provided an update on the current issues and challenges related to mosquito control. He encouraged residents to check their yards once a week and empty any stagnant water from buckets, containers, pet dishes, and recycle fountain water, which can serve as breeding sources for mosquitos. The Sacramento – Yolo Mosquito and Vector Control District has an integrated mosquito management approach, which includes public information, surveillance, biological control, ecological control, and chemical control. Goodman also discussed the new concern for the district being invasive mosquitos and the difficulty they have controlling the species. Residents are encouraged to report any still water, wear any repellent, and spread awareness.

COMMENTS BY COUNCIL MEMBERS AND REGIONAL BOARD UPDATES

Council Member Bruins provided an update from the Citrus Heights Water District webinar series regarding water wise tips.

Council Member Daniels requested City staff reach out to a CalTrans liaison regarding homeless encampments along I-80.

Council Member Schaefer acknowledged the celebration of Ramadan beginning April 12 through May 11.

Vice Mayor Middleton attended a Police Activities League Easter event. She also provided an update from the Sacramento Area Council of Governments (SACOG) Policy meeting, where Citrus Heights will be recognized for the Auburn Boulevard Revitalization Project.

Mayor Miller attended a Citrus Heights Rotary meeting where he gave a presentation. He also provided an update regarding project funding for the Auburn Boulevard Revitalization Project, which is receiving funding from the Sacramento Transportation Authority (STA) and SACOG.

PUBLIC COMMENT

City Clerk Van read the following public comment submitted online from Carly Haggett.

"I live on Meadowcreek way and would like to address city council about getting traffic control on our street. I have lived in my home for over 11 years and have noticed considerable decline in our neighborhood since we moved in. Traffic has increased substantially, and the speed of most drivers is outrageous. There have been several speeding accidents resulting to property damage in the last few years. I have almost been t-boned pulling into my driveway on several occasions. A few weeks ago, our next-door neighbors' car was struck and totaled while parked & shoved into our adjacent neighbor's yard. Our adjacent neighbor had a speeding driver crash in her front yard. Another driver speeding down the street ran the stop sign and crashed in his cars parked in his driveway. Many do not stop at any of the stop signs on the corner of Meadowcreek and park oaks. Since Sunday 4/4/21, our camera picked up over 345 cars. The kids in the neighborhood cannot ride their bikes or play out front due to traffic concerns. This must stop; we must do something to alert drivers of their speed and modify and/or correct them to slow down. I am requesting that this topic be put on an agenda to discuss not only my concerns but the concerns of my neighbors."

Mayor Miller requested traffic team staff reach out to Carly Haggett regarding her comments.

Ken Neutz addressed the Council regarding his business closure at Sunrise Roller Land due to COVID-19. Neutz addressed the difficulties his business has faced regarding re-opening and county COVID-19 restrictions.

CONSENT CALENDAR

6. **SUBJECT:** Approval of Minutes
RECOMMENDATION: Approve the Minutes of the Special and Regular Meeting of March 25, 2021
7. **SUBJECT:** Resolution Authorizing Memorandum of Understanding between Sunrise Recreation and Park District and City of Citrus
STAFF REPORT: C. McDuffee/ C. Kempenaar/ L. Blomquist
RECOMMENDATION: Adopt Resolution No. 2021-018, a Resolution of the City Council of the City of Citrus Heights, California, Authorizing the City Manager to enter into a Memorandum of Understanding with Sunrise Recreation and Park District for the Arcade Cripple Creek Trail in Tempo Park

ACTION: On a motion by Council Member Daniels, seconded by Council Member Bruins, the City Council adopted Consent Calendar Items 6 and 7.

AYES: Bruins, Daniels, Schaefer, Middleton, Miller
NOES: None
ABSENT: None

DEPARTMENT REPORTS

None

CITY MANAGER ITEMS

City Manager Boyd announced the opening of Green Acres Nursery on San Juan and Greenback. The development will create approximately 40 new jobs in Citrus Heights. City Manager Boyd addressed the increasing COVID vaccination number from Sacramento County, which could result in re-opening specific industry sectors.

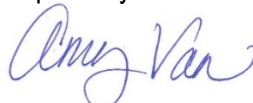
ITEMS REQUESTED BY COUNCIL MEMBERS/ FUTURE AGENDA ITEMS

Council Member Bruins requested City staff reach out to Sacramento County regarding re-opening Sunrise Roller Land.

ADJOURNMENT

Mayor Miller adjourned the regular meeting at 8:59 p.m.

Respectfully submitted,



Amy Van, City Clerk

**CITY OF CITRUS HEIGHTS
CITY COUNCIL
MINUTES
Special Meeting of Monday, April 12, 2021
Virtually from the Citrus Heights City Hall
6360 Fountain Square Drive, Citrus Heights, CA**

SPECIAL CITY COUNCIL MEETING CALLED TO ORDER

The special council meeting was called to order at 2:00 p.m. by Mayor Miller.

- | | | | |
|----|------------|--------------------------|--|
| 1. | Roll Call: | Council Members present: | Bruins, Daniels, Schaefer, Middleton, Miller |
| | | Council Members absent: | None |
| | | Staff present: | Boyd, Jones and Van. |

PUBLIC COMMENT

None

CLOSED SESSION

2. PUBLIC EMPLOYEE PERFORMANCE EVALUATION [requested by City Manager]
Pursuant to Government Code Section 54957:
Title: City Manager

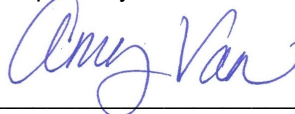
REPORT OUT OF CLOSED SESSION

There was no reportable action from closed session.

ADJOURNMENT

Mayor Miller adjourned the special meeting at 2:29 p.m.

Respectfully submitted,



Amy Van, City Clerk



CITY OF CITRUS HEIGHTS

CITY COUNCIL STAFF REPORT MEMORANDUM

DATE: April 22, 2021

TO: Mayor and City Council Members
Christopher W. Boyd, City Manager

FROM: Ronald A. Lawrence, Chief of Police
Cassandra Burnett, Senior Management Analyst

SUBJECT: Police Department Vehicle Replacement

Summary and Recommendation

Staff recommends the City Council adopt Resolution No. 2021-___ A Resolution of the City Council of the City of Citrus Heights, California, Approving the Transfer of \$140,000 from the General Fund to the Capital Replacement Fund for the Purchase of Police Department Vehicles.

Staff recommends the City Council adopt Resolution No. 2021-___ A Resolution of the City Council of the City of Citrus Heights, California, Approving the Purchase of Two Police Motorcycles utilizing Citizens' Option for Public Safety (COPS) Funding.

Fiscal Impact

The Fiscal Year 2020-21 General Fund Police Department budget includes \$140,000 for the purchase of equipment. By postponing other purchases to future years, these budgeted funds have not yet been expended.

There is no fiscal impact on the General Fund for the purchase of two police motorcycles. The entire cost of the motorcycles and related equipment will be funded by Citizen's Option for Public Safety (COPS) funds (223-21-461).

Background and Analysis

Prior to Fiscal Year 2018-19, funds were transferred annually from the General Fund to the Capital Replacement Fund for the purchase of public safety vehicles as needed. Due to funding constraints, the City has not budgeted this allocation during the past three fiscal years. At this

time, there is currently \$124,000 available in the Capital Replacement Fund to replace public safety vehicles.

Transfer of Funds from the General Fund to the Capital Replacement Fund

In an effort to be fiscally prudent, and knowing there is a need to have funds available to cover the cost of replacement, staff is requesting authorization to transfer \$140,000 from the Police Department's Fiscal Year 2020-21 General Fund budget for replacement equipment (100-21-310) to the Capital Replacement Fund (331-21-190) for the future purchase of replacement public safety vehicles.

Due to the COVID-19 pandemic, production for some vehicle manufactures decreased in 2020 resulting in less 2020 and 2021 vehicles being available on the market for purchase. This is the case for vehicles the Police Department utilizes. At the same time, the department is not in current need of vehicles, but it is anticipated that within the next year there will be patrol vehicles in need of replacement. The estimated cost to purchase and fully equip a marked police vehicle is approximately \$65,000 to \$75,000. For this reason, the transfer of funds is requested to ensure the City has adequate funding available to replace public safety vehicles when needed.

Purchase of Two Police Motorcycles

The Police Department purchased its entire BMW motorcycle fleet in 2013. Since that time, only one motorcycle has been replaced. Due to the unique demands emergency driving requires from a motorcycle and increasing costs of repairs and maintenance of aging vehicles, the department would like to start replacing these motorcycles before they become a serious safety risk.

The cost to purchase and fully equip a police motorcycle is approximately \$35,000. Funds to purchase these motorcycles will come from the Police Department's COPS funding grant. COPS funding is allocated to cities annually based on their population for frontline law enforcement. The department is required to encumber or spend these funds prior to June 30 of the following fiscal year or the funds must be returned to the County. The funds the Police Department will utilize to purchase the two replacement motorcycles are set to expire this year on June 30, 2021.

Attachments

1. Resolution No. 2021-____ A Resolution of the City Council of the City of Citrus Heights, California, Approving the Transfer of \$140,000 from the General Fund to the Capital Replacement Fund for the Purchase of Police Department Vehicles.
2. Resolution No. 2021-____ A Resolution of the City Council of the City of Citrus Heights, California, Approving the Purchase of Two Police Motorcycles utilizing Citizens' Option for Public Safety (COPS) Funding.

RESOLUTION NO. 2021- ____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CITRUS HEIGHTS,
CALIFORNIA, APPROVING THE TRANSFER OF \$140,000 FROM THE GENERAL FUND
TO THE CAPITAL REPLACEMENT FUND FOR THE PURCHASE OF POLICE
DEPARTMENT VEHICLES**

WHEREAS, the adopted Fiscal Year 2020-21 General Fund Police Department budget includes \$140,000 for the purchase of replacement equipment;

WHEREAS, the Police Department wishes to ensure that adequate funding is available when needed to replace public safety vehicles;

WHEREAS, the cost to replace and fully equip a marked patrol vehicles costs anywhere from \$65,000 to \$75,000; and

WHEREAS, due to the COVID-19 pandemic, it is anticipated that replacement Police Department vehicles will not be purchased this fiscal year.

NOW THEREFORE BE IT RESOLVED AND ORDERED that the City of Citrus Heights does hereby approve the transfer of \$140,000 from the General Fund (100-21-310) to the Capital Replacement Fund (331-21-190) for the purchase of Police Department vehicles.

The City Clerk shall certify the passage and adoption of this Resolution and enter it into the book of original resolutions.

PASSED AND ADOPTED by the City Council of the City of Citrus Heights, California, this 22nd day of April 2021 by the following vote, to wit:

AYES: **Council Members:**
NOES: **Council Members:**
ABSTAIN: **Council Members:**
ABSENT: **Council Members:**

Steve Miller, Mayor

ATTEST:

Amy Van, City Clerk

RESOLUTION NO. 2021- ____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CITRUS HEIGHTS,
CALIFORNIA, APPROVING THE PURCHASE OF TWO POLICE MOTORCYCLES
UTILIZING CITIZENS' OPTION FOR PUBLIC SAFETY (COPS) FUNDING**

WHEREAS, the City of Citrus Heights has been awarded Citizens' Option for Public Safety (COPS) funding;

WHEREAS, the City must expend or encumber these grant funds by June 30, 2021;

WHEREAS, the Police Department purchased its entire BMW motorcycle fleet in 2013, with the exception of one purchase in 2019;

WHEREAS, there are unique demands emergency driving requires from a motorcycle and increasing costs of repairs and maintenance of aging vehicles; and

WHEREAS, the cost to replace and fully equip two motorcycles is approximately \$70,000.

NOW THEREFORE BE IT RESOLVED AND ORDERED that the City of Citrus Heights does hereby approve the purchase of two police motorcycles utilizing Citizens' Option for Public safety (COPS) funding.

The City Clerk shall certify the passage and adoption of this Resolution and enter it into the book of original resolutions.

PASSED AND ADOPTED by the City Council of the City of Citrus Heights, California, this 22nd day of April 2021 by the following vote, to wit:

AYES: **Council Members:**
NOES: **Council Members:**
ABSTAIN: **Council Members:**
ABSENT: **Council Members:**

Steve Miller, Mayor

ATTEST:

Amy Van, City Clerk



CITY OF CITRUS HEIGHTS

CITY COUNCIL STAFF REPORT MEMORANDUM

DATE: April 22, 2021

TO: Mayor and City Council Members
Christopher W. Boyd, City Manager

FROM: Leslie Blomquist, City Engineer
Mary Poole, Operations Manager

SUBJECT: **Placer-Sacramento Gateway Corridor Phase 1, Auburn Boulevard Complete Streets Project, Cycle 2 Solutions for Congested Corridors Program (SCCP), Approval of Caltrans Baseline Agreement**

Summary and Recommendation

On December 2, 2020, the California Transportation Commission (CTC), approved a regional grant application for the Placer-Sacramento Gateway Plan's Phase 1 project submittal under Cycle 2 of the Solutions for Congested Corridors Program (SCCP). The Auburn Boulevard Complete Streets Project Phase 2 was included in the application and was approved for \$2,860,000 for the construction phase of the Project. One of the SCCP requirements includes entering into a Program Baseline Agreement with Caltrans related to the Project.

Staff recommends the City Council approve Resolution No. 2021-___, a Resolution of the City Council of the City of Citrus Heights, California, authorizing the City Manager to execute the Program Baseline Agreement for the Placer-Sacramento Gateway – Phase 1 project, Auburn Boulevard Complete Streets Project Phase 2.

Fiscal Impact

This action will have no impact on the FY 20-21 budget. The Project is included in the upcoming City budget and Capital Improvement Program (CIP) process for future fiscal years.

Background and Analysis

In July 2020, the Placer County Transportation Planning Agency (PCPTA), Capitol Corridor Joint Powers Authority (CCJPA), Caltrans District 3, and Sacramento Area Council of Governments (SACOG) in partnership with Sacramento Regional Transit District (SacRT), Sacramento County and the cities of Citrus Heights and Roseville submitted an application for the Placer-Sacramento Gateway Phase 1 Project under the Senate Bill 1 (SB1) Solutions for Congested Corridors Program

(SCCP). Auburn Boulevard Phase 2 was one of the eight regional projects included in the SCCP Cycle 2 application for the Placer-Sacramento Gateway Project Phase 1.

On December 2, 2020, the California Transportation Commission (CTC), approved the regional grant application for the Placer-Sacramento Gateway Plan's project submittal under Cycle 2 of the Solutions for Congested Corridors Program (SCCP). The Auburn Boulevard Complete Streets Project Phase 2 was approved for \$2,860,000 for the construction phase of the Project as part of the overall Placer-Sacramento Gateway Corridor Phase 1 award. One of the SCCP administrative requirements includes entering into a Program Baseline Agreement with Caltrans related to the Project.

The Project is located in Citrus Heights, on Auburn Boulevard between Oak Grove Avenue north to Orlando Avenue (City of Roseville), connecting to the Louis Orlando Transit Station. The Auburn Boulevard Complete Streets Project Phase 2 improvements include curb, gutter, sidewalk, bicycle lanes, transit stop access and amenity upgrades, traffic signal upgrades, decorative street lights, landscaped raised medians, drainage improvements, landscaping improvements, and a new gateway traffic signal near the north City limits.

Attachment

- 1) Resolution No. 2021-____, a Resolution of the City Council of the City of Citrus Heights, California, authorizing the City Manager execute the Program Baseline Agreement for the Placer-Sacramento Gateway – Phase 1 project, Auburn Boulevard Complete Streets Project Phase 2.
- 2) Program Baseline Agreement

RESOLUTION NO. 2021- ____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CITRUS HEIGHTS,
CALIFORNIA, AUTHORIZING THE CITY MANAGER TO
EXECUTE THE PROGRAM BASELINE AGREEMENT FOR THE
PLACER-SACRAMENTO GATEWAY PHASE 1 PROJECT,
AUBURN BOULEVARD COMPLETE STREETS PROJECT PHASE 2**

WHEREAS, in December 2020, the City was notified of a successful Solutions for Congested Corridors (SCCP) Cycle 2 Placer-Sacramento Gateway Corridor Project Phase 1 funding request to construct a portion of the Auburn Boulevard Complete Streets Project Phase 2 (Project);

WHEREAS, the City must execute a project-specific Program Baseline Agreement with Caltrans as part of the grant authorization process.

NOW, THEREFORE, BE IT RESOLVED AND ORDERED by the City Council of the City of Citrus Heights, the City Manager is hereby authorized to execute Program Baseline Agreement and related agreements and documents required for implementation of the Placer-Sacramento Gateway Corridor Phase 1 Project for the Auburn Boulevard Complete Streets Project Phase 2.

The City Clerk shall certify the passage and adoption of this Resolution and enter it into the book of original resolutions.

PASSED AND ADOPTED by the City Council of the City of Citrus Heights, California, this 22nd day of April 2021 by the following vote, to wit:

AYES: Council Members:
NOES: Council Members:
ABSTAIN: Council Members:
ABSENT: Council Members:

Steve Miller, Mayor

ATTEST:

Amy Van, City Clerk

ROAD REPAIR AND ACCOUNTABILITY ACT OF 2017
PROJECT BASELINE AGREEMENT
Placer-Sacramento Gateway - Phase 1

Resolution _____

(will be completed by CTC)

1. FUNDING PROGRAM

- ☐ Active Transportation Program
- ☐ Local Partnership Program (Competitive)
- ☒ Solutions for Congested Corridors Program
- ☐ State Highway Operation and Protection Program
- ☐ Trade Corridor Enhancement Program

2. PARTIES AND DATE

- 2.1 This Project Baseline Agreement (Agreement) for the *Placer-Sacramento Gateway - Phase 1*, effective on, _____ (will be completed by CTC), is made by and between the California Transportation Commission (Commission), the California Department of Transportation (Caltrans), the Project Applicant, *Caltrans, PCTPA, and SACOG*, and the Implementing Agency, *SacRT, Citrus Heights, Roseville, Sacramento County, Caltrans*, sometimes collectively referred to as the "Parties".

3. RECITAL

- 3.2 Whereas at its December 2, 2020 meeting the Commission approved the Solutions for Congested Corridors Program, and included in this program of projects the *Placer-Sacramento Gateway - Phase 1*, the parties are entering into this Project Baseline Agreement to document the project cost, schedule, scope and benefits, as detailed on the Project Programming Request Form attached hereto as Exhibit A and the Project Report attached hereto as Exhibit B, as the baseline for project monitoring by the Commission.
- 3.3 The undersigned Project Applicant certifies that the funding sources cited are committed and expected to be available; the estimated costs represent full project funding; and the scope and description of benefits is the best estimate possible.

4. GENERAL PROVISIONS

The Project Applicant, Implementing Agency, and Caltrans agree to abide by the following provisions:

- 4.1 To meet the requirements of the Road Repair and Accountability Act of 2017 (Senate Bill [SB] 1, Chapter 5, Statutes of 2017) which provides the first significant, stable, and on-going increase in state transportation funding in more than two decades.
- 4.2 To adhere, as applicable, to the provisions of the Commission:
- ☐ Resolution *Insert Number* , "Adoption of Program of Projects for the Active Transportation Program", dated _____
- ☐ Resolution *Insert Number* , "Adoption of Program of Projects for the Local Partnership Program", dated _____
- ☒ Resolution G-20-80 , "Adoption of Program of Projects for the Solutions for Congested Corridors Program", dated December 2, 2020
- ☐ Resolution *Insert Number* , "Adoption of Program of Projects for the State Highway Operation and Protection Program", dated _____
- ☐ Resolution *Insert Number* , "Adoption of Program of Projects for the Trade Corridor Enhancement Program", dated _____

- 4.3 All signatories agree to adhere to the Commission's Solutions for Congested Corridors Program, Guidelines. Any conflict between the programs will be resolved at the discretion of the Commission.
- 4.4 All signatories agree to adhere to the Commission's SB 1 Accountability and Transparency Guidelines and policies, and program and project amendment processes.
- 4.5 The SacRT, City of Citrus Heights, City of Roseville, Sacramento County, and Caltrans agrees to secure funds for any additional costs of the project.
- 4.6 The SacRT, City of Citrus Heights, City of Roseville, Sacramento County, and Caltrans agrees to report to Caltrans on a quarterly basis; after July 2019, reports will be on a semi-annual basis on the progress made toward the implementation of the project, including scope, cost, schedule, outcomes, and anticipated benefits.
- 4.7 Caltrans agrees to prepare program progress reports on a quarterly basis; after July 2019, reports will be on a semi-annual basis and include information appropriate to assess the current state of the overall program and the current status of each project identified in the program report.
- 4.8 The SacRT, City of Citrus Heights, City of Roseville, Sacramento County, and Caltrans agrees to submit a timely Completion Report and Final Delivery Report as specified in the Commission's SB 1 Accountability and Transparency Guidelines.
- 4.9 All signatories agree to maintain and make available to the Commission and/or its designated representative, all work related documents, including without limitation engineering, financial and other data, and methodologies and assumptions used in the determination of project benefits during the course of the project, and retain those records for four years from the date of the final closeout of the project. Financial records will be maintained in accordance with Generally Accepted Accounting Principles.
- 4.10 The Transportation Inspector General of the Independent Office of Audits and Investigations has the right to audit the project records, including technical and financial data, of the Department of Transportation, the Project Applicant, the Implementing Agency, and any consultant or sub-consultants at any time during the course of the project and for four years from the date of the final closeout of the project, therefore all project records shall be maintained and made available at the time of request. Audits will be conducted in accordance with Generally Accepted Government Auditing Standards.

5. SPECIFIC PROVISIONS AND CONDITIONS

5.1 Project Schedule and Cost

See Project Programming Request Form, attached as Exhibit A.

5.2 Project Scope

See Project Report or equivalent, attached as Exhibit B. At a minimum, the attachment shall include the cover page, evidence of approval, executive summary, and a link to or electronic copy of the full document.

5.3 Other Project Specific Provisions and Conditions

Attachments:

Exhibit A: Project Programming Request Form
Exhibit B: Project Report

SIGNATURE PAGE
TO
PROJECT BASELINE AGREEMENT

Placer-Sacramento Gateway - Phase 1

Resolution _____

Name

Date

Title

Project Applicant

April 7, 2021

Christopher W. Boyd

Date

City Manager

Implementing Agency

Name

Date

District Director

California Department of Transportation

Toks Omishakin

Date

Director

California Department of Transportation

Mitchell Weiss

Date

Executive Director

California Transportation Commission

Amendment (Existing Project) <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO					Date	04/09/2021 09:45:54
Programs <input type="checkbox"/> LPP-C <input type="checkbox"/> LPP-F <input type="checkbox"/> SCCC <input type="checkbox"/> TCEP <input type="checkbox"/> STIP <input type="checkbox"/> Other						
District	EA	Project ID	PPNO	Nominating Agency		
03			1531	Caltrans HQ		
County	Route	PM Back	PM Ahead	Co-Nominating Agency		
Sacramento	H40			Sacramento Area Council of Governments, Placer County Tr		
				MPO	Element	
				SACOG	Local Assistance	
Project Manager/Contact			Phone	Email Address		
Leslie Blomquist			916-727-4770	lblomquist@citrusheights.net		

Project Title

PSGC Phase 1 - Auburn Boulevard Complete Streets

Location (Project Limits), Description (Scope of Work)

In the city of Citrus Heights, on Auburn Boulevard between Oak Grove Avenue north to Orlando Avenue (City of Roseville), connection to the Louis-Orlando Transit Station. This project will reconstruct 4,400LF of this aging, vehicle-oriented corridor. Project will construct new curb, gutter, sidewalk, bicycle lanes, transit stop access and amenity upgrades, traffic signal upgrades, decorative streets lights, landscaped raised medians, drainage improvements, landscaping improvements and a new gateway traffic signal near the north City limit.

Component	Implementing Agency
PA&ED	City of Citrus Heights
PS&E	City of Citrus Heights
Right of Way	City of Citrus Heights
Construction	City of Citrus Heights

Legislative Districts

Assembly:	4	Senate:	8	Congressional:	7
-----------	---	---------	---	----------------	---

Project Milestone	Existing	Proposed
Project Study Report Approved		
Begin Environmental (PA&ED) Phase	11/12/2014	11/12/2014
Circulate Draft Environmental Document Document Type (ND/MND)/CE	10/06/2015	10/06/2015
Draft Project Report	11/03/2015	11/03/2015
End Environmental Phase (PA&ED Milestone)	12/07/2015	12/07/2015
Begin Design (PS&E) Phase	06/24/2016	06/24/2016
End Design Phase (Ready to List for Advertisement Milestone)	08/30/2021	08/30/2021
Begin Right of Way Phase	06/24/2016	06/24/2016
End Right of Way Phase (Right of Way Certification Milestone)	04/22/2021	09/30/2021
Begin Construction Phase (Contract Award Milestone)	04/12/2022	12/08/2022
End Construction Phase (Construction Contract Acceptance Milestone)	12/31/2023	01/23/2025
Begin Closeout Phase	03/15/2024	03/15/2025
End Closeout Phase (Closeout Report)	09/30/2024	09/30/2025

Date 04/09/2021 09:45:54

Purpose and Need

The Project will address deficiencies in the existing infrastructure causing obstacles for pedestrians, bicyclists, and public transit riders attempting to navigate Auburn Boulevard between Antelope Road and Orlando Avenue (directly adjacent to Louis Orlando Transit Center in City of Roseville). The project area currently (1) lacks bike routes, (2) poses obstacles for pedestrians due to the location of utility equipment on the sidewalks, (3) has very limited transit stop amenities, and (4) requires operational improvements along the roadway to improve safety for active transportation users as well as vehicle traffic. Auburn Boulevard generally runs parallel to Interstate 80 in Sacramento County and as such is it a regional transportation corridor for commuters as well as those accessing medical and other services in Roseville. The transit station at Louis-Orlando (northern limits of project) also has a bike-link program for bike rentals. The Project completes a multi-phased regeneration project transforming the existing substandard infrastructure into a complete street.

NHS Improvements ☐ YES ☒ NO Roadway Class 1 Reversible Lane Analysis ☐ YES ☒ NO
Inc. Sustainable Communities Strategy Goals ☒ YES ☐ NO Reduce Greenhouse Gas Emissions ☒ YES ☐ NO

Project Outputs

Category	Outputs	Unit	Total
ADA Improvements	Install accessible pedestrian signal	EA	14
Active Transportation	Crosswalk	EA	5
ADA Improvements	Repair existing sidewalk	LF	5,200
Active Transportation	Pedestrian/Bicycle facilities miles constructed	Miles	0.99
ADA Improvements	Repair/upgrade curb ramp	EA	33
ADA Improvements	New curb ramp installed	EA	13
Operational Improvement	Intersection / Signal improvements	EA	5

Date 04/09/2021 09:45:54

Additional Information

This project is part of the Sacramento-Placer Gateway Project Phase 1 SCCP application.

The Placer-Sacramento Gateway Corridor Phase 1 improvements support the following goals and policies identified in the SACOG 2020 Metropolitan Transportation Plan/Sustainable Communities Strategy (MTP/SCS):

Goal 1: Build vibrant places for today's and tomorrow's residents.

- Policy 1: Provide incentives, information, tools, technical assistance, and encouragement to support implementation of the Sacramento region's Sustainable Communities Strategy through:
 - o Revitalization of urban, suburban, and rural centers and corridors;
 - o Complete communities that include a balance of homes, jobs, services, amenities, and diverse transportation options; and
 - o Complete streets that provide safe, comfortable, and equitable facilities for people of all ages and abilities to walk, bike, and ride transit.
- Policy 2: Pursue funding opportunities that support the infrastructure improvements needed to support new housing and employment opportunities in existing urban, suburban, and rural communities.

Goal 2: Foster the next generation of mobility solutions.

- Policy 4: Pursue flexibility in state and federal funding sources to enable testing and implementation of innovative mobility solutions that are affordable, accessible, and reduce greenhouse gas emissions
- Policy 7: Support transit agencies and local governments looking to secure funds to improve the frequency, hours of service, and coverage of productive bus service (including bus rapid transit, express bus, and more frequent fixed-route service).
- Policy 8: Support more seamless travel through better traveler information for trip planning, reliable service and coordination between operators for transit, shared mobility and other first/last mile connections.

Goal 4: Build and maintain a safe, resilient, and multimodal transportation system

- Policy 19: Transit expansion, particularly light rail and other fixed infrastructure transit options, should be targeted at communities with supportive land use policies and development patterns that will generate transit ridership and improve the cost recovery rates for transit service.
- Policy 20: Prioritize cost effective safety improvements that will help the region eliminate fatal transportation related accidents.
- Policy 22: Invest in bicycle and pedestrian infrastructure to encourage healthy, active transportation trips and provide recreational opportunities for residents and visitors.
- Policy 23: Prioritize and incentivize transportation investments that benefit environmental justice communities.
- Policy 24: Invest in transportation improvements that improve access to major economic assets and job centers.
- Policy 25: Prioritize investments in transportation improvements that reduce greenhouse gas emissions and vehicle miles traveled.

Performance Indicators and Measures						
Measure	Required For	Indicator/Measure	Unit	Build	Future No Build	Change
Congestion Reduction	LPPF, LPPC, SCCP	Project Area, Corridor, County, or Regionwide VMT per Capita and Total VMT	Total Miles	22,583,529	22,602,243	-18,714
			VMT per Capita	29.92	29.95	-0.03
	LPPF, LPPC, SCCP	Person Hours of Travel Time Saved	Person Hours	2,991,330	3,009,718	-18,388
			Hours per Capita	3.96	3.99	-0.03
	LPPF, LPPC, SCCP	Daily Vehicle Hours of Delay	Hours	8,281	8,331	-50
	Optional	Percent Change in Non-Single Occupancy Vehicle Travel	%	20.81	20.81	0
	Optional	Per Capita and Total Person Hours of Delay per Year	Person Hours	2,991,330	3,009,718	-18,388
			Hours per Capita	3.96	3.99	-0.03
Throughput	Optional	Bicyclist/ Pedestrian Screen Line Counts	# of Bikes	450	230	220
			# of Pedestrians	195	100	95
	Optional	Peak Period Person Throughput by Applicable Mode	# of Persons	10,985	10,380	605
	Optional	Passengers Per Vehicle Service Hour	# of Passengers	102	96	6
System Reliability	LPPF, LPPC, SCCP	Peak Period Travel Time Reliability Index	Index	1.02	1.04	-0.02
	LPPF, LPPC, SCCP	Transit Service On-Time Performance	% "On-time"	97.8	94.5	3.3
Air Quality & GHG	LPPF, LPPC, SCCP, TCEP	Particulate Matter	PM 2.5 Tons	1,204.72	1,205	-0.28
			PM 10 Tons	1,289.71	1,290	-0.29
	LPPF, LPPC, SCCP, TCEP	Carbon Dioxide (CO2)	Tons	159,422,178	159,476,158	-53,980
	LPPF, LPPC, SCCP, TCEP	Volatile Organic Compounds (VOC)	Tons	21,338.83	21,348	-9.17
	LPPF, LPPC, SCCP, TCEP	Sulphur Dioxides (SOx)	Tons	1,568.45	1,569	-0.55
	LPPF, LPPC, SCCP, TCEP	Carbon Monoxide (CO)	Tons	488,100.71	488,276	-175.29
	LPPF, LPPC, SCCP, TCEP	Nitrogen Oxides (NOx)	Tons	117,294.04	117,339	-44.96
Safety	LPPF, LPPC, SCCP, TCEP	Number of Non-Motorized Fatalities and Non-Motorized Serious Injuries	Number	77.4	77.6	-0.2
	LPPF, LPPC, SCCP, TCEP	Number of Fatalities	Number	9.38	9.83	-0.45
	LPPF, LPPC, SCCP, TCEP	Fatalities per 100 Million VMT	Number	0.11	0.12	-0.01
	LPPF, LPPC, SCCP, TCEP	Number of Serious Injuries	Number	159.52	163.8	-4.28
	LPPF, LPPC, SCCP, TCEP	Number of Serious Injuries per 100 Million VMT	Number	1.94	1.99	-0.05
	Optional	Number of Property Damage Only and Non-Serious Injury Collisions	Number	1,966	2,090	-124
	Optional	Accident Cost Savings	Dollars	-95,700,000	0	-95,700,000

Performance Indicators and Measures						
Measure	Required For	Indicator/Measure	Unit	Build	Future No Build	Change
Accessibility	LPPF, LPPC, SCCP	Number of Jobs Accessible by Mode	Number	687,439	687,439	0
	LPPF, LPPC, SCCP	Number of Destinations Accessible by Mode	Number	360	360	0
	LPPF, LPPC, SCCP	Percent of Population Defined as Low Income or Disadvantaged Within 1/2 Mile of Rail Station, Ferry Terminal, or High-Frequency Bus Stop	%	71.8	70.5	1.3
Economic Development	LPPF, LPPC, SCCP, TCEP	Jobs Created (Direct and Indirect)	Number	1,461	0	1,461
Cost Effectiveness	LPPF, LPPC, SCCP, TCEP	Cost Benefit Ratio	Ratio	2.46	0	2.46

District	County	Route	EA	Project ID	PPNO
03	Sacramento	H40			1531

Project Title

PSGC Phase 1 - Auburn Boulevard Complete Streets

Existing Total Project Cost (\$1,000s)									Implementing Agency
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	
E&P (PA&ED)									City of Citrus Heights
PS&E	1,528							1,528	City of Citrus Heights
R/W SUP (CT)									City of Citrus Heights
CON SUP (CT)									City of Citrus Heights
R/W	2,990							2,990	City of Citrus Heights
CON		12,867						12,867	City of Citrus Heights
TOTAL	4,518	12,867						17,385	

Proposed Total Project Cost (\$1,000s)									Notes
E&P (PA&ED)									
PS&E	1,528							1,528	
R/W SUP (CT)									
CON SUP (CT)									
R/W	2,990							2,990	
CON		12,867		4,056	4,944			21,867	
TOTAL	4,518	12,867		4,056	4,944			26,385	

Fund #1:	CMAQ - Congestion Mitigation (Committed)								Program Code
	Existing Funding (\$1,000s)								20.30.010.820
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	Funding Agency
E&P (PA&ED)									Sacramento Area Council of Governm
PS&E	1,353							1,353	
R/W SUP (CT)									
CON SUP (CT)									
R/W	2,647							2,647	
CON									
TOTAL	4,000							4,000	

Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									
PS&E	1,353							1,353	
R/W SUP (CT)									
CON SUP (CT)									
R/W	2,647							2,647	
CON									
TOTAL	4,000							4,000	

Fund #2:	Local Funds - Agency (Committed)								Program Code
Existing Funding (\$1,000s)									20.10.400.100
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	Funding Agency
E&P (PA&ED)									City of Citrus Heights
PS&E	175							175	
R/W SUP (CT)									
CON SUP (CT)									
R/W	343							343	
CON									
TOTAL	518							518	
Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									
PS&E	175							175	
R/W SUP (CT)									
CON SUP (CT)									
R/W	343							343	
CON									
TOTAL	518							518	
Fund #3:	RSTP - STP Local (Committed)								Program Code
Existing Funding (\$1,000s)									20.30.010.810
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	Funding Agency
E&P (PA&ED)									Sacramento Area Council of Governm Regional Funding Program
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		4,000						4,000	
TOTAL		4,000						4,000	
Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		4,000						4,000	
TOTAL		4,000						4,000	

Fund #4:	State SB1 ATP - Active Transportation Program - SB1 (Committed)								Program Code
Existing Funding (\$1,000s)									20.30.720.100
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	Funding Agency
E&P (PA&ED)									ATP Grant Funds
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		1,525						1,525	
TOTAL		1,525						1,525	
Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		1,525						1,525	
TOTAL		1,525						1,525	
Fund #5:	Local Funds - Agency (Committed)								Program Code
Existing Funding (\$1,000s)									20.10.400.100
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	Funding Agency
E&P (PA&ED)									City of Citrus Heights local funding for non-ATP eligible construction including utility undergrounding; included in approved CIP + additional 82k in local funds (4,482,000 total)
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		4,482						4,482	
TOTAL		4,482						4,482	
Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		4,482						4,482	
TOTAL		4,482						4,482	

Fund #6:	State SB1 SCCP - Solution for Congested Corridors Program (Committed)								Program Code
Existing Funding (\$1,000s)									20.30.210.350
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	Funding Agency
E&P (PA&ED)									SCCP Phase 1 -Auburn Boulevard Complete Streets funding request
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		2,860						2,860	
TOTAL		2,860						2,860	
Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		2,860						2,860	
TOTAL		2,860						2,860	
Fund #7:	Other Fed - TRANSPORTATION IMPROVEMENTS (Uncommitted)								Program Code
Existing Funding (\$1,000s)									
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	Funding Agency
E&P (PA&ED)									Sacramento Area Council of Governm
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									
Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									9,000,000 in CON funds recommended for the 2021 Regional Funding awards, to be considered by SACOG Board on 04.15.21
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON				3,380	4,944			8,324	
TOTAL				3,380	4,944			8,324	

Fund #8:	Other State - STA Smart Growth Program (Uncommitted)								Program Code
Existing Funding (\$1,000s)									
Component	Prior	21-22	22-23	23-24	24-25	25-26	26-27+	Total	Funding Agency
E&P (PA&ED)									Sacramento Area Council of Governm
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									
Proposed Funding (\$1,000s)									Notes
E&P (PA&ED)									Included in SACOG 20/21 Funding Recommendations - STA Smart Growth Funds
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON				676				676	
TOTAL				676				676	

Complete this page for amendments only

Date 04/09/2021 09:45:54

District	County	Route	EA	Project ID	PPNO
03	Sacramento	H40			1531

SECTION 1 - All Projects

Project Background

Programming Change Requested

Reason for Proposed Change

If proposed change will delay one or more components, clearly explain 1) reason for the delay, 2) cost increase related to the delay, and 3) how cost increase will be funded

Other Significant Information

SECTION 2 - For SB1 Project Only

Project Amendment Request (Please follow the individual SB1 program guidelines for specific criteria)

Approvals

I hereby certify that the above information is complete and accurate and all approvals have been obtained for the processing of this amendment request.

Name (Print or Type)	Signature	Title	Date

SECTION 3 - All Projects

Attachments

- 1) Concurrence from Implementing Agency and/or Regional Transportation Planning Agency
- 2) Project Location Map

**INITIAL STUDY
MITIGATED NEGATIVE DECLARATION
AUBURN BOULEVARD COMPLETE STREETS PROJECT,
PHASE 2
CITY OF CITRUS HEIGHTS, CALIFORNIA**



NOVEMBER 2015



**INITIAL STUDY
MITIGATED NEGATIVE DECLARATION
AUBURN BOULEVARD COMPLETE STREETS PROJECT, PHASE 2
CITY OF CITRUS HEIGHTS, CALIFORNIA**

Submitted to:

City of Citrus Heights
6237 Fountain Square Drive
Citrus Heights, CA 95621

Prepared by:

Dokken Engineering
110 Blue Ravine Road, Suite 200
Folsom, California 95630

NOVEMBER 2015



**CITRUS
HEIGHTS**
CALIFORNIA

General Information About This Document

For individuals with sensory disabilities, this document can be made available in Braille, in large print, on audiocassette, or on computer disk. To obtain a copy in one of these alternate formats, please call or write to City of Citrus Heights, Planning Division, Attn: Casey Kempenaar, Senior Planner, 7927 Auburn Boulevard, CA 95621, (916) 727-4740.

TABLE OF CONTENTS

General Information About This Document	i
INTRODUCTION	1
PROJECT BACKGROUND	1
SECTION 1.0 – BACKGROUND.....	8
SECTION 2.0 – ADDITIONAL ENVIRONMENTAL FACTORS AFFECTED	13
SECTION 3.0 – ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED.....	16
SECTION 4.0 – ENVIRONMENTAL CHECKLIST	18
SECTION 5.0 – COMMENTS AND COORDINATION.....	79
SECTION 6.0 – REFERENCES.....	80

List of Tables

Table 1. Required Permits.....	12
Table 2. Attainment at the Project Location	24
Table 3. Special Status Species with Potential to Occur in the Project Vicinity	29
Table 4. Population Density and Associated Ambient Noise Levels.....	63
Table 5. Discussions with Developers and Property Owners	79

List of Figures

Figure 1. Project Vicinity.....	2
Figure 2. Project Location.....	4
Figure 3. Project Features	6

List of Appendices

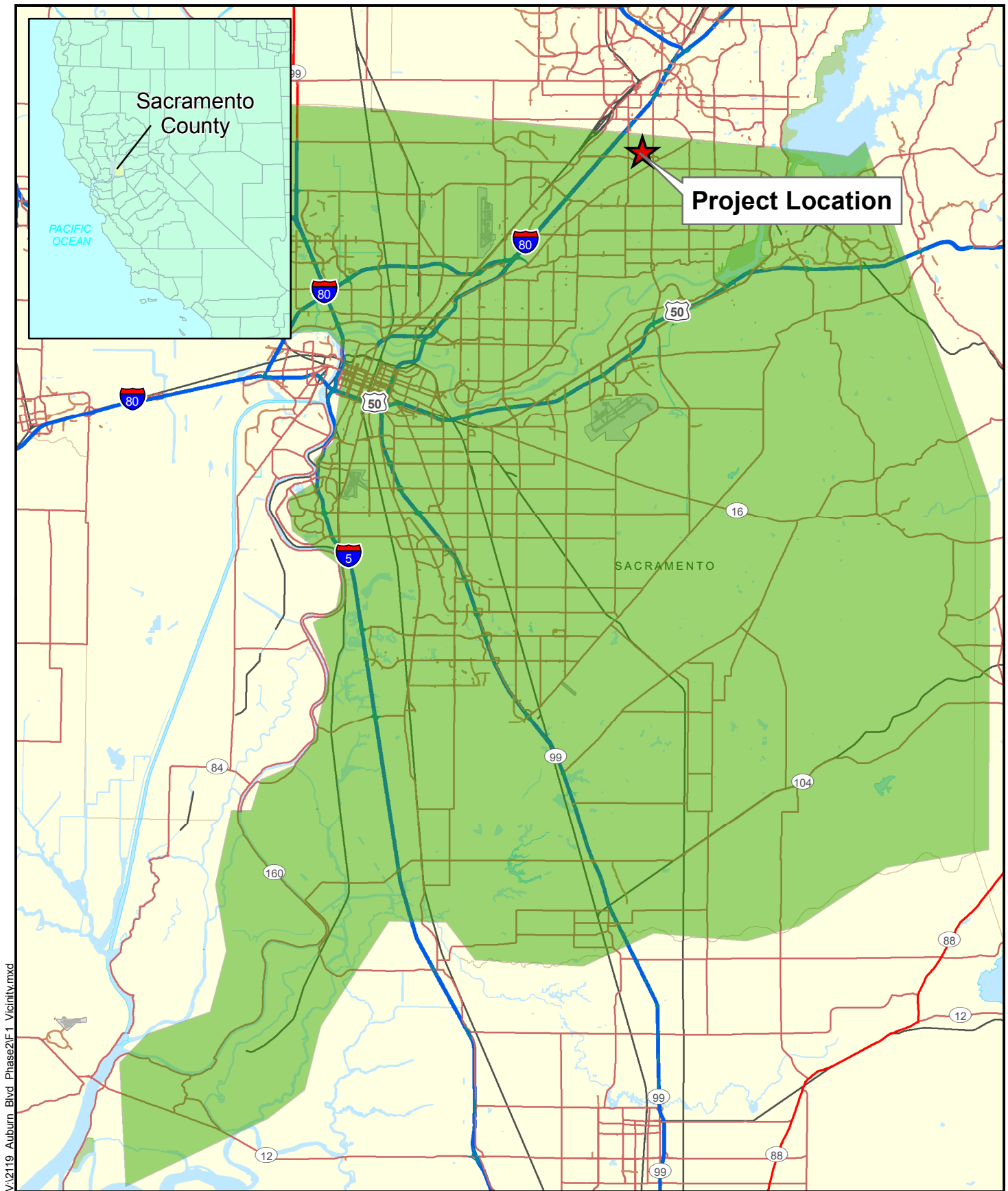
Appendix A – Representative Site Photographs	
Appendix B – Auburn Boulevard Specific Plan Final Environmental Impact Report	
Appendix C – CNDDB, USFWS, and CNPS Special Status Species Database Results	
Appendix D – FEMA Firmette Map	
Appendix E – Mitigation Monitoring Plan	
Appendix F – Draft Initial Study/Mitigated Negative Declaration Comment Letters and Responses	

INTRODUCTION

This Initial Study with Mitigated Negative Declaration was prepared for the Auburn Boulevard Complete Streets, Phase 2 Project (Project), located in the City of Citrus Heights and the City of Roseville, California (Figure 1 Project Vicinity, Figure 2 Project Location, Figure 3 Project Features and Appendix A). The Phase 2 Project is 0.99 miles and begins on the north side of Cripple Creek and ends at Whyte Avenue. The Project is located within both the City of Citrus Heights and the City of Roseville. The project lies within the following: Township (T) 10North and Range (R) 6East, Section 23 of the Citrus Heights United States Geological Survey (USGS) 7 ½ Minute Quadrangles. The City of Citrus Heights (City) proposes to make improvements to Auburn Boulevard in order to upgrade the corridor's image and improve its function as a transportation facility serving adjacent land uses. The project would include widening Auburn Boulevard to accommodate bike lanes, construction of new curbs, gutters and sidewalks. The City of Citrus Heights is the California Environmental Quality Act (CEQA) lead for this project.

PROJECT BACKGROUND

In February of 2005, the City of Citrus Heights adopted a specific plan to guide the revitalization and enhancement of Auburn Boulevard between Sylvan Corners and Interstate 80. The purpose of the specific plan is to improve the corridor's image and commercial competitiveness in the region, and improve its function as a transportation facility serving adjacent land uses as well as provide for better connections with the neighborhoods bordering the corridor. The adopted specific plan project area is located within the Auburn Boulevard corridor from Old Auburn Road to the northern City limits of Citrus Heights, and includes approximately 460 acres. As part of that specific plan, the City proposes to make improvements to Auburn Boulevard which will include widening Auburn Boulevard to accommodate bike lanes, construction of new curbs, gutters and sidewalks. Phase I of the project which extended from Sylvan Corners to north of the Cripple Creek Bridge was analyzed and approved under the 2005 Auburn Boulevard Specific Plan EIR (Appendix B). Construction on the Phase I Project was recently completed. The Auburn Boulevard Complete Streets, Phase 2 Project, which extends north of City limits to Whyte Avenue within the City of Roseville, will be examined within this document. For the purposes of this document, the Final Auburn Boulevard Specific Plan EIR (2005), City of Citrus Heights General Plan FEIR (2011), City of Roseville General Plan (2015) and all associated technical studies will be referenced for this particular segment of Auburn Boulevard.



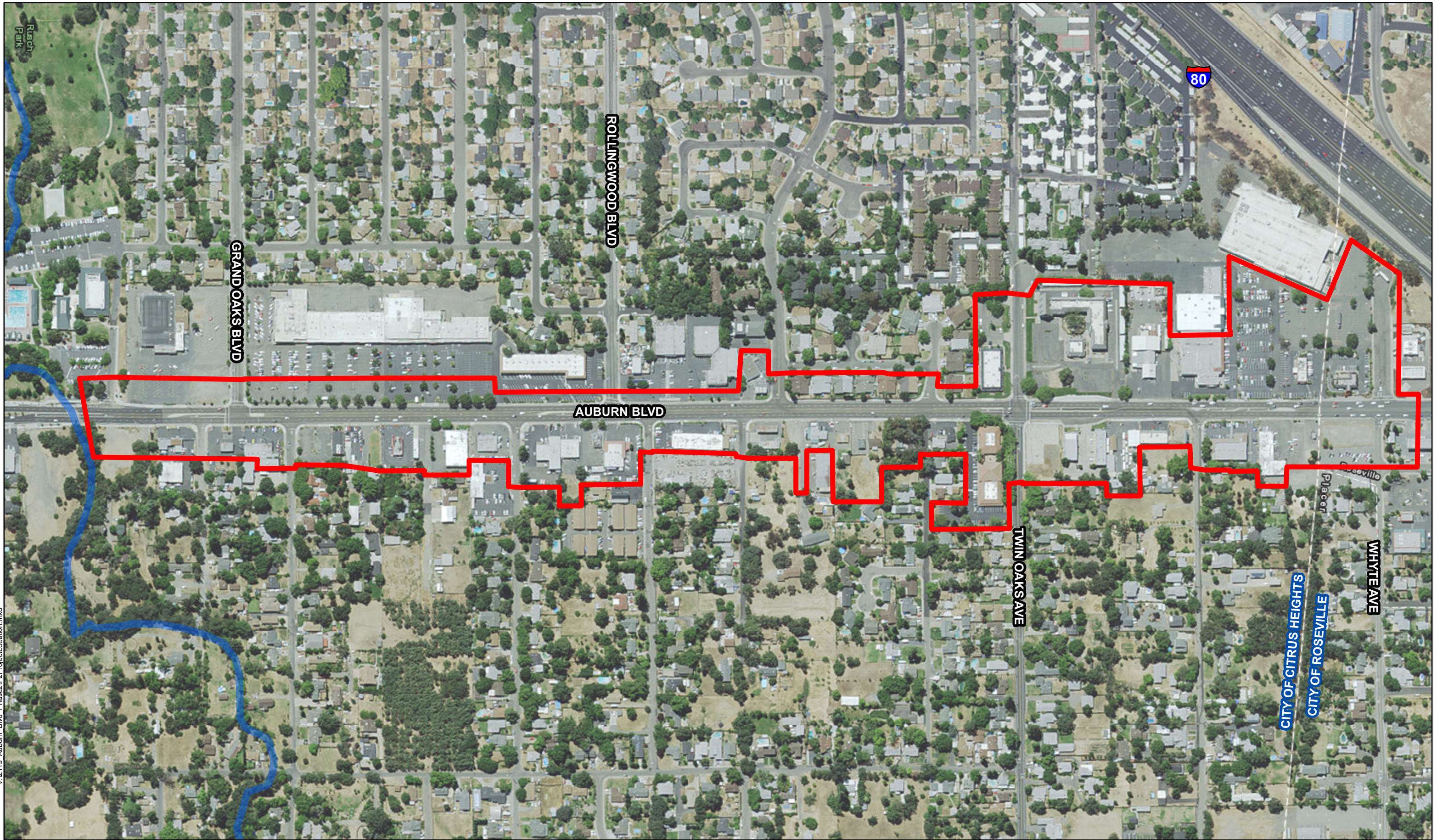
V:\2119 Auburn Blvd Phase2\F1 Vicinity.mxd

Source: ESRI 2008; Dokken Engineering 7/13/2015; Created By: astorck

FIGURE 1
Project Vicinity

CMAQ5475(038)

Auburn Boulevard Complete Streets, Phase 2 Project
City of Citrus Heights, Sacramento County, California



V:\2119_Auburn_Bldv_Phase2\F2ProjectLocation.mxd

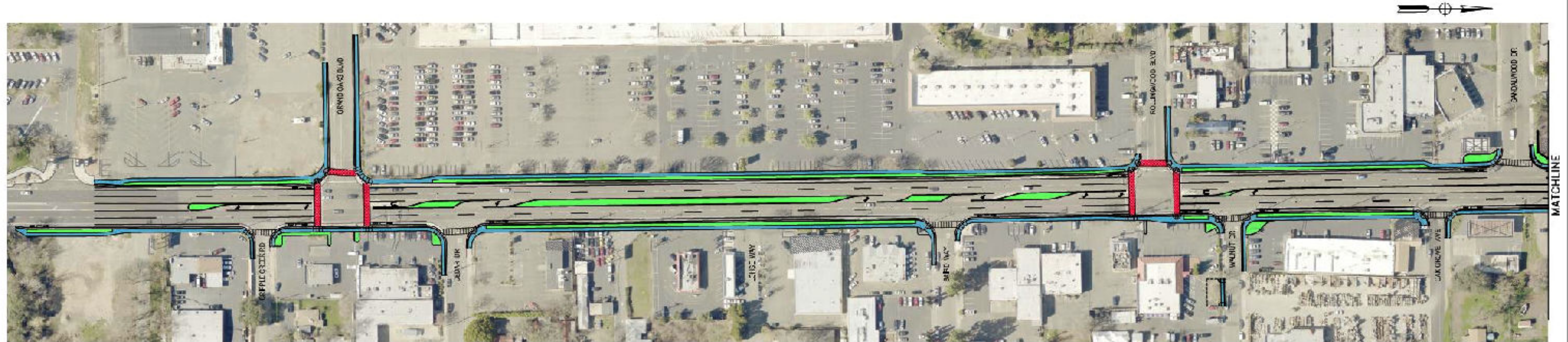
Source: ESRI 2013; Dokken Engineering 9/11/2015; Created By: astorck



0 500 1,000 1,500 Feet

 Project Area

Figure 2
Project Location
Auburn Boulevard Complete Streets, Phase 2 Project
City of Citrus Heights, Sacramento County, California



LEGEND

- PROPOSED SIDEWALK
- PROPOSED LANDSCAPING
- PROPOSED STAMPED CROSSWALK



V:\2119_Auburn_Bvd_Phase2\SMND\F3\project\features.mxd

Source: ESRI 2013; Dokken Engineering 10/1/2015; Created By: carleneg



0 500 1,000 1,500 Feet

Figure 3
Project Features
Auburn Boulevard Complete Streets, Phase 2 Project
City of Citrus Heights, California

CITY OF CITRUS HEIGHTS

INITIAL STUDY CHECKLIST

The California Environmental Quality Act (CEQA) requires the Lead Agency to examine the effects of a project on the physical conditions that exist within the area that would be affected by the project. CEQA also requires a discussion of any inconsistency between the project and applicable general plans and regional plans.

An inconsistency between the project and an adopted plan for land use development in a community would not constitute a physical change in the environment. When a project diverges from an adopted plan, however, it may affect planning in the community regarding infrastructure and services, and the new demands generated by the project may result in later physical changes in response to the project.

In the same manner, the fact that a project brings new people or demand for housing to a community does not, by itself, change the physical conditions. An increase in population may, however, generate changes in retail demand or demand for governmental services, and the demand for housing may generate new activity in residential development. Physical environmental impacts that could result from implementing the project are discussed in the appropriate technical sections.

This section of the initial study identifies the applicable land use designations, plans and policies, and permissible densities and intensities of use, and discusses any inconsistencies between these plans and the project. This section also discusses agricultural resources and the effect of the project on these resources.

For the purposes of this document, the City of Citrus Heights General Plan EIR (2011), City of Roseville General Plan (2015), and the Auburn Boulevard Specific Plan EIR (2005) were referenced.

SECTION 1.0 - BACKGROUND

1. Project Title: Auburn Boulevard Complete Streets, Phase 2
2. Lead Agency: City of Citrus Heights
Planning Division
7927 Auburn Boulevard
Citrus Heights, CA 95621
3. Contact Person: Casey Kempenaar, Senior Planner
City of Citrus Heights
Planning Division
7927 Auburn Boulevard
Citrus Heights, CA 95621
Phone: (916)727-4740
E-mail: ckempenaar@citrusheights.net
4. Project Location: The Phase 2 Project is 0.99 mile and begins on the north side of Cripple

Creek and ends at Whyte Avenue. The Project is located within both the City of Citrus Heights and the City of Roseville. The project lies within the following: Township (T) 10North and Range (R) 6East, Section 23 of the Citrus Heights United States Geological Survey (USGS) 7 ½ Minute Quadrangles.

5. Applicant: City of Citrus Heights (City)
6. General Plan: City of Citrus Heights General Plan EIR (2011), City of Roseville General Plan (2015) and the Auburn Boulevard Specific Plan EIR (2005).
7. Zoning: The portion of the project located within the City of Citrus Heights is zoned for Special Planning Area (SPA). The portion of the project located within the City of Roseville is zoned for Neighborhood Commercial, General Commercial and Community Commercial. The project is located within the boundaries of the Boulevard Plan Specific Plan.
8. Description of Project: The Phase 2 Project is 0.99 mile and begins on the north side of Cripple Creek and ends at Whyte Avenue. The Project is located within both the City of Citrus Heights and the City of Roseville. Project components include widening of Auburn Boulevard to accommodate bike lanes, construction of new curbs, gutters and sidewalks. Minor relocation of utilities and right of way acquisition will be required. Project components within the City of Roseville will consist of approximately 230 feet of roadway improvements along Auburn Boulevard, south of the intersection of Whyte Avenue and Auburn Boulevard (see Figures 2 and 3).

Project components within both the City of Citrus Heights and the City of Roseville would include undergrounding of existing overhead utilities (electrical and communications, etc.) and roadway improvements, also referred to as complete streets, including:

- Pedestrian safety improvements
- Bus pull-outs
- ADA improvements
- Installation of approximately 9,600 lineal feet of bike lanes and sidewalks
- Planting of street trees and landscaping buffer where feasible
- Installation of energy-efficient street lights
- Installation of Landscaped Medians
- Traffic Signal installation and modification

Need

The City of Citrus Heights identified a need to address land use, community design and circulation issues along the existing Auburn Boulevard Corridor.

Land Use:

- The City of Citrus Heights, City of Roseville and Auburn Boulevard now lie at the center of the growing Sacramento Region. The specific plan area is surrounded by major employment centers and

major shopping areas, yet fragmented development patterns, poor site design and access problems limit the area's economic potential.

- Within the specific plan area there is a rough interface between commercial and residential uses. Parking lots, service areas, trash containers, and utilitarian structures occupy the interface between commercial and residential areas. There are land use conflicts due to hours of operation, outdoor storage, shipping and receiving activities and privacy concerns. In some cases, buildings and parcels originally designed for retail uses have been adapted for other uses that involve activities that are incompatible with adjacent or nearby residential areas, such as outdoor storage of materials and vehicles or frequent deliveries.
- A buildings condition survey conducted by the City as part of the Citrus Heights Redevelopment Plan found that along the northern section of Auburn Boulevard numerous structures require moderate or extensive rehabilitation, are dilapidated and require almost total rehabilitation or are suffering from deferred maintenance. A survey of parcel conditions showed that a substantial portion of parcels in the specific plan area are subdivided lots of irregular form and shape, have inadequate size for development, and are in multiple ownership (City of Citrus Heights 2003).

Community Design:

- Overhead transmission lines, light poles, and other utilities along the corridor limit opportunities for adding street trees and landscaping.
- Storm water runoff from parking lots sheet flow to the creek, dumpsters are pushed against the open space, and commercial buildings have been designed on the sites without regard to these important community resources.
- Signage along the corridor is distracting and fragments the visual experience.

Circulation:

- 2015 Average Daily Traffic (ADT) volumes on Auburn Boulevard range from 27,000 to 29,100 vehicles. The corridor is considered a four-lane, low-access-control arterial that currently operates at Level of Service (LOS) "E" conditions on a daily basis. According to the City General Plan EIR, year 2035 traffic demands on the corridor are projected to increase to 28,500 to 32,400 vehicles per day and corridor operations are projected to degrade to LOS F conditions under the existing capacity classification, however City policy allows LOS exceptions to this corridor (City of Citrus Heights General Plan EIR, Policy 29.1 and 29.2).
- There are several unsignalized (side-street-stop-sign controlled) intersections and driveways along the entire length of the corridor. The left-turn egress/ingress movements from/to these side-streets carry relatively low traffic volume demands, however several of these left-turn movements operate at peak hour LOS "F" conditions.
- There were 51 reported accidents (over a 36-month period extending between January 1, 2012 and December 31, 2014) on the segment of Auburn Blvd. extending from approximately 200 feet south of Grand

Oaks Blvd. to 100 feet north of Linden Avenue The majority of accidents involved rear-end and broadside collisions. Unsafe speed and automobile right-of-way violation were the most frequently reported primary collision factors for these accidents.

- The sidewalk system is incomplete and narrow, has many vertical obstructions, is interrupted by numerous curb cuts, and provides little separation from high speed traffic. In some areas there are no sidewalks and very few streets trees to provide shade. These deficiencies make walking a difficult and unpleasant experience for residents.
- Many residential areas rely on connections to Auburn Boulevard. Side streets do not align east and west of Auburn Boulevard, making pedestrian crossings difficult.
- There are no bike lanes along this segment of Auburn Boulevard.

Purpose

The purpose of the project is for the improvement of Auburn Boulevard in order to upgrade the corridors image, improve its function as a transportation facility serving adjacent land uses, improve commercial competitiveness in the region and improve its function as a transportation facility serving adjacent land uses and provide for better connections with the neighborhoods bordering the corridor.

Construction Access, Staging and Methods:

Project Access and Staging Areas

To allow equipment to access the project site, access would be through I-80 located to the north of the project area, and Antelope Road located 450 feet south of the project area. Construction and equipment staging will be at one of several commercial business parking lots located along Auburn Boulevard for the duration of the project.

Pavement construction for new roadway and shoulder widening will require excavations of 28 inches in depth. Additional excavation of 6' in depth, or more, will be required in isolated locations for placement of drainage facilities and underground utility lines. Traffic signs and striping will be installed during and after construction.

Anticipated Construction Equipment

Typical construction equipment would include the following:

- Crane
- Backhoe
- Excavator
- Concrete saw (removal of existing road)
- Cement truck
- Paver
- Rollers

- Motor grader
- Dump truck
- Light tools (ie. saws, jackhammer)

Most construction related noise would occur during the road improvements. This operation would likely include noise from concrete hammers and jackhammers. All construction work for the project will comply with the City of Citrus Heights and City of Roseville Standard Construction Specifications (or Best Management Practices).

Utilities

There are existing overhead utility lines located along the east side of Auburn Boulevard. The utilities involved include electric (SMUD), telephone (Surewest), and cable television (Comcast). The project proposes to underground these lines along the east side of the roadway either below the sidewalk or adjacent to the easterly curb line. Trenching depths of six (6) feet or more will be required in some locations for underground utilities. Private easements may be required for additional utility company facilities.

Permits

The permits, reviews and approvals listed below would be required for project construction.

Table 1. Required Permits, Reviews and Approvals

Responsible Agency	Permit/Approval	Status
Regional Water Quality Control Board	National Pollutant Discharge Elimination System General Permit 2009-0009-DWQ for Storm Water Discharges Associated with Construction Activity	Will be obtained prior to construction.
City of Roseville – Public Works Engineering Division	Encroachment Permit	Will be obtained prior to construction.

Coordination Efforts:

The project area exceeds 1 acre, therefore a National Pollutant Discharge Elimination System 402 General Permit for Storm Water Discharges Associated with construction activity will be obtained prior to construction. A City of Roseville Encroachment Permit will also be obtained prior to construction. No further permits are required for this project.

9. Surrounding Land Uses and Setting:

Auburn Boulevard is classified as an arterial street and is adjacent to local streets, residences, and businesses.

SECTION 2.0 – ADDITIONAL ENVIRONMENTAL FACTORS AFFECTED

The Initial Study is also intended to assess whether any environmental effects of the project are susceptible to substantial reduction or avoidance by the choice of specific revisions in the project, by the imposition of conditions, or by other means [15152(b)(2)] of the California Environmental Quality Act (CEQA) Guidelines. If such revisions, conditions, or other means are identified, they will be identified as mitigation measures.

This Initial Study relies on State CEQA Guidelines 15064 and 15604.4 in its determination of the significance of environmental effects. According to 15064, the findings as to whether a project may have one or more significant effects shall be based on substantial evidence in the record, and that controversy alone, without substantial evidence of a significant effect, does not trigger the need for an EIR.

This Mitigated Negative Declaration is a subsequent environmental document that tiers off the General Plan Program EIRs and the Auburn Boulevard Plan EIR, therefore impacts that were adequately addressed in the General Plan EIRs and the Auburn Boulevard Plan EIR do not need to be repeated in this Mitigated Negative Declaration. The following listed topics were addressed in the Program EIR for the Citrus Heights General Plan, City of Roseville General Plan, and the Auburn Boulevard Specific Plan EIR and require no further discussion:

- Aesthetics
- Agricultural and Forestry Resources
- Cultural Resources
- Geology and Soils
- Greenhouse Gas Emissions
- Land Use/Planning
- Mineral Resources
- Population/Housing
- Public Services
- Recreation
- Transportation/Traffic
- Utilities and Service Systems

The Auburn Boulevard Complete Streets Phase 2 Project Mitigated Negative Declaration is a subsequent environmental document to the General Plan EIR and the Auburn Boulevard Plan EIR. As such, this MND incorporates all applicable mitigation measures adopted with the Final EIR for the General Plan and the Auburn Boulevard Plan EIR. The following is a summary of additional mitigation measures that were not included in the General Plan FEIR or the Auburn Boulevard Plan FEIR. These measures, along with applicable measures from the Auburn Boulevard Plan FEIR and General Plan FEIR that are listed in Section 4.0, will be adopted in a Mitigation, Monitoring, and Reporting Plan for the Auburn Boulevard Complete Streets Phase 2 Project Mitigated Negative Declaration.

ADDITIONAL MITIGATION MEASURES INCORPORATED WITHIN THE PROJECT

The following are additional mitigation measures applicable to the project that were not included within the City of Citrus Heights General Plan FEIR, City of Roseville General Plan, or the Auburn Boulevard Plan FEIR:

Air Quality

AQ-2: Route and schedule construction traffic to avoid peak travel times as much as possible, to reduce congestion and related air quality impacts caused by idling vehicles along local roads.

Biological Resources

B-4: If construction is planned to occur during the raptor nesting season (February – August) a preconstruction raptor nesting survey shall be conducted by a qualified biologist within 7 days prior to vegetation removal. Vegetation surveyed shall include all trees, 10 feet or taller and containing a dbh of 2 inches or greater. Within 2 weeks of the nesting raptor survey, all vegetation cleared by the biologist shall be removed by the contractor.

A minimum 500 foot no-disturbance buffer shall be established around any active raptor nest to limit the impacts of construction activities. The contractor shall immediately stop work in the nesting area until the appropriate buffer is established and is prohibited from conducting work that could disturb the birds (as determined by the project biologist and in coordination with wildlife agencies) in the buffer area until a qualified biologist determines the young have fledged.

B-5: If ground disturbance or vegetation removal is to take place during the breeding season (February – August), a pre-construction nesting bird survey shall be conducted within 7 days prior to vegetation removal. Vegetation surveyed shall include all trees, bushes, tall grasses and emergent vegetation. Within 2 weeks of the nesting bird survey, all vegetation cleared by the biologist shall be removed by the contractor.

A minimum 100 foot no-disturbance buffer shall be established around any active nest to limit the impacts of construction activities. The contractor shall immediately stop work in the nesting area until the appropriate buffer is established and is prohibited from conducting work that could disturb the birds (as determined by the project biologist and in coordination with wildlife agencies) in the buffer area until a qualified biologist determines the young have fledged.

Hazards and Hazardous Material

HM-4: Any leaking transformers observed during the course of the project should be considered a potential polychlorinated biphenyl (PCB) hazard. Should leaks from electrical transformers (that will either remain within the construction limits or will require removal and/or relocation) be encountered during construction, the transformer fluid should be sampled and analyzed by qualified personnel for detectable levels of PCB's. Should PCBs be detected, the transformer should be removed and disposed of in accordance with Title 22, Division 4.5 of the California Code of Regulations and any other appropriate regulatory agency. Any stained soil encountered below electrical transformers with detectable levels of PCB's should also be handled and disposed of in accordance with Title 22, Division 4.5 of the California Code of Regulations and any other appropriate regulatory agency.

HM-5: Based on preliminary plans, right-of-way acquisition may be required at the Towne Mart gas station at Sandalwood Drive and the abandoned gas station at Oak Grove Avenue. Should final plans indicate that a portion of this parcel will be acquired for new right-of-way, a preliminary environmental screening (limited subsurface sampling and laboratory analysis) should be performed during the PS&E for potentially elevated levels of petroleum hydrocarbons and MTBE contamination within the limits of construction, and/or right-of way acquisition, adjacent to the existing gas stations. Should the preliminary screening encounter elevated levels of petroleum hydrocarbons and/or MTBE a limited Phase II Initial Site Assessment should be performed. The Phase II Initial Site Assessment should consist of subsurface sampling and laboratory analysis and be of sufficient quantity to define

the extent and concentration of contamination within the areal extent and depths of planned construction activities adjacent to the existing gas stations. The Phase II Initial Site Assessment should also provide both a Health and Safety Plan for worker safety and a Work Plan for handling and disposing contaminated soil during construction.

HM-6: The potential exists for hazardous contamination from historic chemical spills at Paradise Cleaners, which is located near the intersection of Auburn Boulevard and Baird Way. At the time of the Initial Site Assessment, there were no documented reports of soil/groundwater contamination related to chemical discharge from Paradise Cleaners. If a potential hazardous contamination is detected, soil samples should be gathered and tested to determine the chemical levels within the soil.

HM-7: To avoid impacts from pavement striping during construction it is recommended that removal requirements for yellow striping and pavement marking materials be performed in accordance with Caltrans Standard Special Provision 14-11.07 REMOVE YELLOW TRAFFIC STRIPE AND PAVEMENT MARKING WITH HAZARDOUS WASTE RESIDUE.

HM-8: As is the case for any project that proposes excavation, the potential exists for unknown hazardous contamination to be revealed during project construction (such as previously undetected petroleum hydrocarbon contamination from nearby gas stations). Should any previously unknown hazardous waste/material be encountered during construction, the procedures outlined in Caltrans Hazards Procedures for Construction shall be followed.

HM-9: If the project area is anticipated to change (due to a change in the project or staging area), further investigation for potential hazardous waste generators would be required to determine their impact to the revised project limits.

Hydrology/Water Quality

H-3: The Project would require a NPDES General Construction Permit for Discharges of storm water associated with construction activities (Construction General Permit 2009-0009-DWQ). A SWPPP would also be developed and implemented as part of the Construction General Permit.

H-4: The construction contractor shall adhere to the SWRCB Order No. 2009-0009-DWQ NPDES Permit pursuant to Section 402 of the CWA. This permit authorizes storm water and authorized non-storm water discharges from construction activities. As part of this Permit requirement, a SWPPP shall be prepared prior to construction consistent with the requirements of the RWQCB. This SWPPP shall incorporate all applicable BMPs to ensure that adequate measures are taken during construction to minimize impacts to water quality.

Noise

N-4: The Contractor shall follow City of Citrus Heights and City of Roseville noise ordinances for construction activities:

- Do not exceed 65 dBA at 50 feet from the job site activities from 8 p.m. to 7 a.m.
- Use an alternative waiting method instead of a sound signal unless required by safety laws.
- Equip an internal combustion engine with the manufacturer-recommended muffler.
- Do not operate an internal combustion engine on the job site without the appropriate muffler.

SECTION 3.0 - ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|---|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture Resources | <input checked="" type="checkbox"/> Air Quality |
| <input checked="" type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology /Soils |
| <input checked="" type="checkbox"/> Hazards & Hazardous Materials | <input checked="" type="checkbox"/> Hydrology / Water Quality | <input type="checkbox"/> Land Use / Planning |
| <input type="checkbox"/> Mineral Resources | <input checked="" type="checkbox"/> Noise | <input type="checkbox"/> Population / Housing |
| <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation | <input type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance | |

The City has prepared an Initial Study for this project, and following public review, has determined that for resource areas not checked above, the project would not result in any significant impacts that cannot be mitigated to a less-than-significant level or are not sufficiently addressed by the 2005 ABSP EIR, City of Citrus Heights General Plan FEIR or the City of Roseville General Plan FEIR. This Initial Study has concluded that the project would incrementally contribute to, but not exceed, certain significant impacts previously identified in the 2005 ABSP EIR, and that for such impacts, no new mitigation measures, other than those previously identified in the 2005 ABSP EIR, are required. The project could result in new potentially significant Air Quality, Biological Resources, Hazards and Hazardous Materials, Hydrology/Water Quality, and/or Noise impacts that were not sufficiently addressed and mitigated by the 2005 ABSP EIR, therefore a Tiered Mitigated Negative Declaration is appropriate.

DETERMINATION: (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- ☐ I find that the project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- ☒ The project MAY incrementally contribute to, but not exceed, certain significant cumulative impacts previously identified in the 2005 ABSP EIR, and that for such impacts, no new mitigation measures, other than those previously identified in the 2005 ABSP EIR, are required. In addition, the project MAY result in potentially significant impacts not previously identified in the 2005 ABSP EIR, but project specific mitigation measures would reduce the effect of such impacts to a point that clearly no significant impacts would occur. On the basis of the Tiered Initial Study and implementation of all project specific mitigation measures, there is no substantial evidence that the project as mitigated may have a significant effect on the environment. A MITIGATED NEGATIVE DECLARATION will be prepared.
- ☐ I find that the project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

- ☐ I find that although the project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the project, nothing further is required.

Signature

Date

Name

SECTION 4.0 - ENVIRONMENTAL CHECKLIST

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
I. AESTHETICS - Would the project:					
a. Have a substantial adverse effect on a scenic vista?		X			
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within state scenic highway?		X			
c. Substantially degrade the existing visual character or quality of the site and its surroundings?		X			
d. Create a new source of substantial light glare which would adversely affect day and nighttime views in the area?		X			

Discussion

- a. No Impact. The project would not cause adverse effects on a scenic vista (ABSP 2005).
- b. No Impact. There are no state or locally designated scenic highways in the vicinity of the project. Auburn Boulevard is not a designated or eligible scenic highway. Therefore, no impact would occur (ABSP 2005).
- c. Less-than-Significant with mitigation incorporated. See the ABSP EIR Mitigation Measure A-1.
- d. Less-than-Significant with mitigation incorporated. See the ABSP EIR Mitigation Measure A-2.

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

A-1: Implementation of the City of Citrus Heights General Plan Policy 37.1 and compliance with the Tree Preservation Ordinance will encourage the retention of native oaks in the landscape of the specific plan area and will mitigate for the visual impacts resulting from the removal of native oak trees.

See Mitigation Measures for biological resources, B-1, B-2A and B-2B and B-2C which address impacts to the oak woodland and riparian habitat adjacent to Cripple Creek.

A-2: Implementation of the Boulevard Plan's Principles and Design Guidelines and enforcement of the City's Zoning Code Landscaping and Lighting standards will avoid impacts associated with light and glare.

Additional Project-Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
II. AGRICULTURE RESOURCES - In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:					
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?		X			
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?		X			
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)) or timberland (as defined by Public Resources Code section 4526)?		X			
d. Result in the loss of forest land or conversion of forest land to non-forest use?		X			
e. Involve other changes in the existing environment, which, due to their location or nature, could result in		X			

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
II. AGRICULTURE RESOURCES - In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:					
conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?					

Discussion

- a. No Impact. The project site would not convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) to non-agricultural use (ABSP 2005).
- b. No Impact. The project would not conflict with existing zoning for agricultural use, or a Williamson Act contract (ABSP 2005).
- c. No Impact. The project does not occur within areas zoned for agriculture or forest lands; therefore, the project would not cause conflicts within existing zoning, or require rezoning of forest land or timberland (ABSP 2005).
- d. No Impact. The project would not result in the loss of forest land or conversion of forest land to non-forest use (ABSP 2005).
- e. No Impact. The project would not involve other changes in the existing environment that could result in the conversion of Farmland to non-agricultural use or conversion of forest land to non-forest use (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

None.

Additional Project-Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Not applicable.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
III. AIR QUALITY - Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:					
a. Conflict with or obstruct implementation of the applicable air quality plan?		X			
b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation?			X		
c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?			X		
d. Expose sensitive receptors to substantial pollutant concentrations?		X			
e. Create objectionable odors affecting a substantial number of people?		X			

Setting

For the purposes of this document, the City of Citrus Heights General Plan EIR (2011), City of Roseville General Plan (2015), and the Auburn Boulevard Specific Plan EIR (2005) were referenced for this section.

The project is located within the Sacramento Valley Air Basin (SVAB), in the region administered by the Sacramento Metropolitan Air Quality Management District (SMAQMD), which administers air quality in the City of Citrus Heights, and the Placer County Air Pollution Control District (PCAPCD) which administers air quality in the City of Roseville. The SVAB has a Mediterranean climate, characterized by hot, dry summers and mild, rainy winters. During the year, the temperature may range from 20 to 115 degrees Fahrenheit with summer highs usually in the 90s and winter lows occasionally below freezing. Average annual rainfall is about 20 inches with snowfall being very rare. Summer high temperatures as measured at the Sacramento

Executive Airport Federal Aviation Administration - Flight Service Station (the nearest climatic monitoring station to the Auburn Blvd Complete Phase 2 Project) average in the low 90s °F and summer low temperatures average in the upper 50s °F. Winter conditions are characterized by occasional rainstorms and/or occasional snow, interspersed with stagnant and sometimes foggy weather. Wintertime high temperatures average in the mid-upper 50s °F and winter low temperatures average in the high 30s °F (Western Regional Climate Center 2015).

Standards of Significance

For purposes of this Initial Study, air quality impacts may be considered significant if construction and/or implementation of the Project would result in the following impacts that remain significant after implementation of General Plan policies or mitigation from the City of Citrus Heights General Plan General Plan EIR or the City of Roseville General Plan:

- Construction emissions of NO_x above 85 pounds per day for City of Citrus Heights, and 82 pounds per day for City of Roseville;
- Operational emissions of NO_x or ROG above 65 pounds per day for City of Citrus Heights and 82 pounds per day for City of Roseville;
- Violation of any air quality standard or contribute substantially to an existing or projected air quality violation;
- PM₁₀ concentrations equal to or greater than five percent of the State ambient air quality standard (i.e., 50 micrograms/cubic meter for 24 hours) in areas where there is evidence of existing or projected violations of this standard. However, if project emissions of NO_x and ROG are below the emission thresholds given above, then the project would not result in violations of the PM₁₀ ambient air quality standards;
- CO concentrations that exceed the 1-hour state ambient air quality standard (i.e., 20.0 ppm) or the 8-hour state ambient standard (i.e., 9.0 ppm); or
- Exposure of sensitive receptors to substantial pollutant concentrations.

Ambient air quality standards have not been established for toxic air contaminants (TAC). TAC exposure is deemed to be significant if:

- TAC exposures create a risk of 10 in 1 million for stationary sources, or substantially increase the risk of exposure to TACs from mobile sources.

Summary of Analysis under the City of Citrus Heights General Plan EIR, and the City of Roseville General Plan

City of Citrus Heights

The General Plan EIR addressed the potential effects of the General Plan on ambient air quality and the potential for exposure of people, especially sensitive receptors such as children or the elderly, to unhealthful pollutant concentrations. See General Plan EIR, Chapter 4.3.

Policies in the General Plan in Environmental Resources were identified as mitigating potential effects of development that could occur under the General Plan, including calling for the City to work with the California Air Resources Board and the Sacramento Metropolitan Air Quality Management District (SMAQMD) to meet state and federal air quality standards. The Master EIR identified exposure to sources of toxic air contaminants (TAC) as a potential effect. Policies in the General Plan would reduce the effect to a less-than-significant level. The policies include requiring consideration of current guidance provided by the Air Resources Board and SMAQMD; requiring development adjacent to stationary or mobile TAC sources to be designed with consideration of such exposure in design, landscaping and filters.

City of Roseville

Policies in the General Plan in Environmental Resources were also identified as mitigating potential effects of development that could occur under the General Plan, including calling for the City to work with the California Air Resources Board and the Placer County Air Pollution Control District (PCAPCD) to meet state and federal air quality standards. The General Plan EIR identified exposure to sources of toxic air contaminants (TAC) as a potential effect. Policies in the general Plan would reduce the effect to a less-than-significant level. The policies include requiring consideration of current guidance provided by the Air Resources Board and PCAPCD; requiring development adjacent to stationary or mobile TAC sources to be designed with consideration of such exposure in design, landscaping and filters.

Discussion

- a. No Impact. Implementation of the project would not conflict with or obstruct implementation of the SMAQMD or PCAPCD Air Quality Plans (SMAQMD 2015a)(PCAPCD 2009).
- b. Less than significant with mitigation incorporated. SMAQMD and PCAPCD has established screen-level criteria for the assessment of significant impacts from construction-related emissions of fugitive dust. These criteria are based on a projects maximum actively disturbed area. Construction activities that would disturb less than 15.0 acres per day would be required to implement the appropriate level of mitigation, identified as “Basic Construction Emission Control Practices,” for all projects to further minimize construction-related impacts regardless of the CEQA significance determination. Because the project would disturb an area less than 15 acres, BMPs have been included from the “Basic Construction Emission Control Practices” to reduce construction-related emissions of fugitive dust. See City Code: 15.40.050 and 15.44.170; SMAQMD Rule 403 (Fugitive Dust) and their Basic Construction Emissions Control Practices, and PCAPCD Rule 228 (Fugitive Dust).

PM₁₀ emissions are assumed to be below the thresholds because Construction NO_x emissions are temporary. There are no construction ROG thresholds, and both NO_x and ROG operational thresholds are not expected to be exceeded. Therefore, the project would not result in an additional significant impact that was not addressed in the City of Citrus Heights General Plan EIR or City of Roseville General Plan.

- c. Less than significant with mitigation incorporated. The project would have short-term impacts resulting from the following construction-related sources: 1) construction and demolition equipment emissions; 2) dust from construction operations; and 3) emissions from construction vehicles.

As shown in Table 2, the project is located in an area in nonattainment for 1-hour Ozone for State standards, nonattainment for 8-hour Ozone for both Federal and State standards, and nonattainment for Particulate Matter under 2.5 micrometers for Federal standards and State standards.

Table 2. Attainment at the Project Location

Criteria Pollutant	Attainment Status	
	Federal	State
City of Citrus Heights		
O ₃ – 1-hour	N/A	Nonattainment - Serious
O ₃ – 8-hour	Nonattainment	Nonattainment
PM ₁₀	Nonattainment	Nonattainment
PM _{2.5}	Nonattainment	Nonattainment
CO	Unclassified/Attainment	Attainment
NO ₂	Unclassified/Attainment	Attainment
SO ₂	Unclassified	Attainment
Sulfates	N/A	Attainment
Lead	Attainment	Attainment
Hydrogen Sulfide	N/A	Unclassified
Visibility Reducing Particles	N/A	Unclassified
City of Roseville		
Ozone	Serious Non-attainment	Non-attainment/Serious
Carbon Monoxide	Unclassified/Attainment*	Unclassified
Nitrogen Dioxide	Unclassified	Attainment
Sulfur Dioxide	Attainment	Attainment
PM ₁₀	Unclassified/Attainment	Non-attainment
Source: California Air Resources Board, 2015, Placer County Air Pollution Control District, 2009.		
*The Sacramento Valley Air Basin portion of Placer County, which includes Roseville, is designated as an attainment area for CO.		

Temporary/Construction Impacts

During construction, short-term degradation of air quality may occur due to the release of particulate emissions (airborne dust) generated by excavation, grading, hauling, and other activities related to construction. Emissions from construction equipment also are anticipated and would include carbon monoxide (CO), nitrogen oxides (NO_x), volatile organic compounds (VOCs), directly-emitted particulate matter (PM₁₀ and PM_{2.5}), and toxic air contaminants such as diesel exhaust particulate matter. Ozone is a regional pollutant that is derived from NO_x and VOCs in the presence of sunlight and heat.

Heavy trucks and construction equipment powered by gasoline and diesel engines would generate CO, SO₂, NO_x, VOCs and some soot particulate (PM₁₀ and PM_{2.5}) in exhaust emissions. If construction activities were to increase traffic congestion in the detour area, CO and other emissions from traffic would increase slightly while those vehicles are delayed. These emissions would be temporary and limited to the immediate area surrounding the construction site and detour area. Dust generated will result in a temporary, local impact, limited to areas of construction. Dust control practices will be incorporated into the project to mitigate this potential impact. The dust control practices will comply with the current Caltrans' Standard Specifications.

Permanent Impacts

The project is exempt from all project-level conformity requirements because it falls under exempt projects (widening narrow pavements [no additional travel lanes]) listed in 40 CFR 93.126.

Because construction and operational emissions are temporary and expected to be below the thresholds, the project is not expected to violate any air quality standards. The project would not increase the capacity of the roadway, no additional trips or delays are expected to result from the project. The project is exempt from all project-level conformity requirements because it falls under exempt projects (widening narrow pavements or reconstructing bridges [no additional travel lanes]) listed in 40 CFR 93.126.

Therefore the project would not result in additional significant impact that was not addressed in the City of Citrus Heights General Plan EIR or the City of Roseville General Plan. See Mitigation Measures A-1 through A-3.

- d. Less than significant. Although the nearest sensitive receptors are residences located directly adjacent to Auburn Boulevard, construction activities, which involve the use of diesel-powered equipment, are short-term and emissions are expected to be well below the thresholds. Operational emissions are not expected to increase, as discussed for Question B. Despite a low-impact expectation for this project, measures for construction activities are still recommended to further reduce impacts on sensitive receptors.

SMAQMD and PCAPCD defines sensitive receptors as facilities that house or attract children, the elderly, people with illnesses, or others who are especially sensitive to the effects of air pollutants or may experience adverse effects from unhealthful concentrations of air pollutants. Hospitals, clinics, schools, convalescent facilities, and residential areas are examples of sensitive receptors. The nearest sensitive receptors in the vicinity of the project site are residences directly adjacent to the project site.

Construction activities are anticipated to involve the operation of diesel-powered equipment. In 1998, the CARB identified diesel exhaust as a TAC. Cancer health risks associated with exposures to diesel exhaust typically are associated with chronic exposure, in which a 70-year exposure period often is assumed. Although elevated cancer rates can result from exposure periods of less than 70 years, acute exposure (i.e., exposure periods of 2 to 3 years) to diesel exhaust typically are not anticipated to result in an increased health risk because acute exposure typically does not result in exposure concentrations that would represent a health risk. Health impacts associated with exposure to diesel exhaust from project construction are not anticipated to be significant because construction activities are expected to occur well below the 70-year exposure period used in health risk assessments. Therefore, construction of the project is not anticipated to result in an elevated cancer risk to exposed persons. No mitigation is required. Therefore, the project would not result in an additional significant impact that was not addressed in the City of Citrus Heights General Plan EIR or the City of Roseville General Plan.

- e. No Impact. The project would not include industrial or intensive agriculture uses; therefore, the project would not create odors or toxic air contaminants. No odors would be generated by potential uses. Potential effects related to air quality and odors would be less than significant, and no mitigation would be necessary.

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

AQ-1A: Inhalable Particulate Matter: The following mitigation measures shall be incorporated into the project to minimize the generation of PM10 dust during construction.

- enclose, cover, or water twice daily all soil piles;
- water exposed soil with adequate frequency for continued moist soil;
- water all haul roads twice daily; and
- cover loads of all haul/dump trucks securely.

AQ-1B: The demolition or renovation of asbestos-containing building material is subject to the limitations of the National Emissions Standards for Hazardous Air Pollutants (NESHAP) regulations as listed in the Code of Federal Regulations (40CFR Part 61, Subpart M) requiring notification and inspection. Most demolitions and many renovations are subject to a CAL-OSHA Certified asbestos inspection prior to the start of activity. SMAQMD Rule 902, which requires District consultation and permit, applies to demolition, renovation or removal of asbestos-containing material. Compliance with these regulations is considered to reduce this impact to a less than- significant level

Additional Project Level Mitigation Measures

AQ-2: Route and schedule construction traffic to avoid peak travel times as much as possible, to reduce congestion and related air quality impacts caused by idling vehicles along local roads.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
IV. BIOLOGICAL - Would the project:					
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?		X			
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife		X			

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
IV. BIOLOGICAL - Would the project:					
Service?					
c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?		X			
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?			X		
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?		X			
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?		X			

Regulatory Setting

The following city, State, and federal statutes pertain to the project:

- National Environmental Policy Act (42 USC 4321 et seq.)
- Federal Endangered Species Act (16 USC 1531-1543)
- Section 404 of the Clean Water Act (33 USC 1251-1376)
- Fish and Wildlife Coordination Act (16 USC 661-6660)
- Executive Order 11990, Protection of Wetlands (May 24, 1977)
- Migratory Bird Treaty Act of 1918 (USC 703-711)
- California Environmental Quality Act (PRC 21000 et seq.)
- California Endangered Species Act (CDFW Code 2050 et seq.)
- Native Plant Protection Act (CDFW Code 1900-1913)
- City of Citrus Heights Tree Preservation and Protection Ordinance (Code 106.39)
- City of Roseville Tree Preservation Ordinance (Roseville Municipal Code Chapter 19.66)

Federal Endangered Species Act

The Federal Endangered Species Act defines ‘take’ (Section 9) and prohibits ‘taking’ of a listed endangered or threatened species (16 USC 1532, 50 CFR 17.30). If a federally listed species could be harmed by a project, Section 7 or 7 consultations must be initiated, and an Incidental Take Permit must be obtained (16 USC 1539, 50 CFR 13).

Federal Migratory Bird Treaty Act

Migratory birds are protected under the federal Migratory Bird Treaty Act (MBTA) of 1918 (16 USC 703-711). The MBTA makes it unlawful to take, possess, buy, sell, purchase, or barter any migratory bird listed in 50 CFR Part 10 including feathers or other parts, nests, eggs, or products, except as allowed by implementing regulations (50 CFR 21). All migratory bird species are protected by the MBTA. Any removal of active nests during the breeding season or any disturbance that results in the abandonment of nestlings is considered a ‘take’ of the species under federal law.

Setting

For the purposes of this document, the Auburn Boulevard Specific Plan EIR (2005), the City of Citrus Heights General Plan EIR, and all associated technical studies were referenced for this section.

The Auburn Boulevard corridor is dominated by urban uses. Few remnant natural habitats are present in the area immediately adjacent to the roadway. These habitats have been highly disturbed through urbanization and various degrees of landform alteration. Small stands of native oaks trees are found within the Project area. Landscaped areas or areas of ruderal vegetation are most common along the corridor (see Figure 3).

The project occurs within the Sacramento Valley floristic region and USFS ecological subsection 262Ag (Hardpan Terraces), which is a geologically characterized by low hills and alluvial plains.

PAR Environmental Services conducted a biological survey of the project area on August 24, 2004 to characterize the environmental setting on and adjacent to the project within the City of Citrus Heights, and Dokken Engineering conducted a preliminary database search on June 24, 2015. The database search was performed to confirm special status species with the potential to occur within the previously surveyed project area within the City of Citrus Heights, and the additional project area within the City of Roseville.

A literature research was conducted through the USFWS Planning Species List, CDFW, CNDDDB and the CNPS Electronic Inventory of Rare and Endangered Plants to identify habitats and special status species with the potential to occur within the project area for Citrus Heights USGS 7.5-minute quadrangle (see Figure 2 Project Location and Figure 3 Project Features).

These database searches identified special status species within the USFWS jurisdiction that may be affected by the project. In addition, a query of the USFWS’s Critical Habitat Portal was conducted to identify potential critical habitat designations within the vicinity of the project. A query of the CNDDDB database provided a list of known occurrences for special status species. The CNPS database search purpose was to identify special status plant species with the potential to occur within the Citrus Heights, California USGS 7.5-minute quadrangle (Appendix C).

Sensitive Habitats

Sensitive habitats include sensitive natural plan communities and other habitats designated and/or regulated by California Department of Fish and Wildlife (CDFW), U.S. Fish and Wildlife Service (USFWS), and U.S. Army Corps of Engineers (USACE). Under Section 404 of the Clean Water Act (CWA), wetlands and other waters of the U.S. are subject to the jurisdiction of USACE. Aquatic habitats may also receive protection

under California statutes including Section 1602 of the California Fish and Wildlife Code and the California Porter-Cologne Water Quality Control Act.

Special Status Species

Special status species are plants and animals in the following categories:

- Species that are listed under the federal Endangered Species Act (ESA) and/or California Endangered Species Act (CESA) as rare, threatened, or endangered;
- Species considered as candidates and for state or federal listing as threatened or endangered;
- Wildlife designated by CDFW as species of special concern; and
- Plants ranked by CDFW as “rare, threatened, or endangered” in California.

The California Natural Diversity Database (CNDDDB), maintained by the CDFW, is considered as the most current and reliable tool for tracking occurrences of special status species in California.

Special Status Species Evaluation

The special status species evaluation considers those species identified as having relative scarcity and/or declining populations by the USFWS or CDFW. Special status species include those formally listed as threatened or endangered, those for formal listing, candidates for federal listing, and those classified as Species of Concern by USFWS or Species of Special Concern by CDFW. Species considered to be “special animals” or “fully protected” by the CDFW or rare, threatened, or endangered in California by the California Native Plant Society (CNPS) were also included in the evaluation.

Setting and Methods

Queries of the USFWS Planning Species list, CNDDDB Electronic Inventory of Rare and Endangered Plants, and CNPS database queries, conducted on June 25, 2015 identified several special status species with the potential to be impacted by the project. Field surveys were also previously conducted in August 2004 to document existing biological resources, detect potential jurisdictional waters of the U.S. and State, and search for suitable habitat and presence of Federal and State protected species. Potential impacts to resources were analyzed based on the project design and ecological resources identified in the field surveys. Table 3 provides a summary of all species identified in the search results, a description of the habitat requirements for each species, and conclusions regarding the potential for each species to occur within the project area.

Table 3: Special Status Species with Potential to Occur in the Project Vicinity				
Common Name	Common Name	Common Name	Common Name	Common Name
Plant Species				
Dwarf downingia	<i>Downingia pusilla</i>	Fed: CA: CNPS:	-- -- 2.2	An annual herb inhabiting vernal pools and mesic valley and foothill grassland communities. Flowers March-May (3-1,460 feet).
Boggs Lake hedge-hyssop	<i>Gratiola heterosepala</i>	Fed: CA: CNPS:	-- E 1B. 2	An annual herb inhabiting clay soils and shallow waters of marshes and swamps, lake margins, and vernal pools. Flowers April-August (33-7792 feet).
				Presumed Absent; The project area lacks the species' requisite vernal pools and mesic grassland community; habitat unsuitable for dwarf downingia. Nearest CNDDDB occurrence is 5 miles south west of the project area.
				Presumed Absent; Soils within the project vicinity are loams and the project area lacks requisite clay soils; habitat unsuitable for Boggs Lake hedge-hyssop.

Table 3: Special Status Species with Potential to Occur in the Project Vicinity				
Common Name	Common Name	Common Name	Common Name	Common Name
				Nearest CNDDDB occurrence is 4 miles north of the project area.
Legenere	<i>Legenere limosa</i>	Fed: CA: CNPS:	-- -- 1B. 1	An annual herb inhabiting wet areas, vernal pools, and ponds. Flowers May-June (0-2,887). Presumed Absent; The creek within proximity to the project area is likely too regularly maintained for species occurrence. Nearest CNDDDB occurrence is approximately 4 miles from project location; species presumed absent.
Ahart's dwarf rush	<i>Juncus leiospermus</i> var. <i>ahartii</i>	Fed: CA: CNPS:	-- -- 1B. 2	An annual herb inhabiting grassland swales, gopher mounds and vernal pool margins of mesic valley and foothill grassland communities. Flowers March – May (98-751 feet). Presumed Absent; The project area lacks the requisite vernal pools and mesic grassland community; habitat unsuitable for Ahart's dwarf rush. Nearest CNDDDB occurrence is 12 miles south of the project area.
Sacramento Orcutt grass	<i>Orcuttia viscida</i>	Fed: CA: CNPS:	E E 1B. 1	An annual herb inhabiting vernal pools. Flowers April-July (98-328 feet). Presumed Absent; The project area lacks the requisite vernal pools and site elevation is outside the species range; habitat unsuitable for Sacramento Orcutt grass. Nearest occurrence is 5 miles south east of the project area.
Sanford's arrowhead	<i>Sagittaria sanfordii</i>	Fed: CA: CNPS:	-- -- 1B. 2	A perennial rhizomatous herb inhabiting freshwater marshes, swamps, ponds and ditches. Flowers May-October (0-2,132 feet). Presumed Absent; The creek adjacent to the project area is potentially suitable habitat for the species. However, the project area is highly disturbed and regularly maintained. In addition, the nearest CNDDDB occurrence is approximately 2 miles northwest from the project area.
Big-scale balsamroot	<i>Balsamorhiza macrolepis</i>	Fed: CA: CNPS:	-- -- 1B. 2	Inhabits chaparral, valley and foothill grassland, cismontane woodland. Presumed Absent; The project area is highly disturbed and urbanized, and lacks the requisite habitat for Big-scale balsamroot. Nearest CNDDDB occurrence is 6 miles north of the project area.
Red Bluff dwarf rush	<i>Juncus leiospermus</i> var. <i>leiospermus</i>	Fed: CA: CNPS:	-- -- 1B. 1	Chaparral, valley and foothill grassland, cismontane woodland. Presumed Absent; The project area is highly disturbed and urbanized, and lacks the requisite habitat for Red Bluff dwarf rush. Nearest CNDDDB occurrence is 6.5 miles north of the project area.
Pincushion navarretia	<i>Navarretia myersii</i> ssp. <i>Myersii</i>	Fed: CA: CNPS:	-- -- 1B. 1	Inhabits vernal pools and clay soils within nonnative grassland. Presumed Absent; The project area lacks the requisite vernal pools and grassland community; habitat unsuitable for pincushion navarretia. Nearest CNDDDB occurrence is 11 miles southeast of the project area.
Hispid salty bird's-beak	<i>Chloropyron molle</i> ssp. <i>Hispidum</i>	Fed: CA: CNPS:	-- -- 1B. 1	Inhabits meadows and seeps, playas, valley and foothill grassland. Presumed Absent; The project area lacks the requisite meadows, seeps, playas, valley or foothill grassland habitat. Project area is considered unsuitable for Hispid

Table 3: Special Status Species with Potential to Occur in the Project Vicinity				
Common Name	Common Name	Common Name	Common Name	Common Name
				salty bird's beak. Nearest CNDDB occurrence is 7 miles northeast of the project area.
Avian Species				
Tricolored blackbird	<i>Agelaius tricolor</i>	Fed: CA: DFG:	-- -- SSC	<p>Prefers freshwater marsh, swamp and wetland communities, but utilize agricultural or upland habitats that can support large colonies often in the Central Valley area. Requires protected dense nesting habitat protected from predators, be within 3-5 miles to a suitable foraging area with insect prey and within 0.3 miles of open water. Suitable foraging includes wetland, pastureland, rangeland, at dairy farms, and in some irrigated croplands (silage, alfalfa, etc.). Nests mid-march - early August, but may extend until October/November in the Sacramento Valley region.</p> <p>Presumed Absent; The project area does not contain the dense emergent wetland or an adequate source of open water needed throughout the breeding season. The project area is urbanized with unsuitable foraging habitat for Tricolored blackbird. Nearest CNDDB occurrence is 5 miles northeast of the project area.</p>
Golden eagle	<i>Aquila chrysaetos</i>	Fed: CA: DFG:	-- -- FP	<p>Inhabits grasslands, deserts, savannahs, and early successional stages of forest and shrub habitats. Requires open terrain for hunting, often utilizing rolling foothills and mountain terrain, wide arid plateaus deeply cut by streams and canyons, open mountain slopes, and cliffs and rock outcrops. Home range dependent on prey availability and habitat openness; estimated at 48 mi² in northern California. Species nests on cliffs and large trees in open areas; breeds January-August (0-11,000 feet).</p> <p>Presumed Absent; The project area does not contain open grassland habitat needed for the species. The project area is highly disturbed and urbanized with unsuitable habitat for golden eagle. Nearest CNDDB occurrence is 12 miles south of the project area.</p>
Burrowing owl	<i>Athene cucularia</i>	Fed: CA: DFG:	-- -- SSC	<p>Species inhabits arid, open areas with sparse vegetation cover such as deserts, abandoned agricultural areas, grasslands, and disturbed open habitats. Requires friable soils for burrow construction (Below 5,300 feet).</p> <p>Presumed Absent; Project site is highly disturbed, developed and too frequently managed for species occurrence; habitat unsuitable for burrowing owl. Nearest CNDDB occurrence is 9 miles north west of the project area.</p>
Swainson's hawk	<i>Buteo swainsoni</i>	Fed: CA: DFG:	-- T --	<p>Inhabits grasslands with scattered trees, juniper-sage flats, riparian areas, savannahs, and agricultural or ranch lands with groves or lines of trees. Requires adjacent suitable foraging areas such as grasslands, alfalfa or grain fields that support a stable rodent prey base. Breeds March to late August.</p> <p>Presumed Absent; The project area lacks open grassland suitable for the species foraging. Additionally, the nearest CNDDB occurrence is 10 miles northwest from project location.</p>
White-tailed kite	<i>Elanus leucurus</i>	Fed: CA: DFG:	-- -- FP	<p>Inhabits rolling foothills and valley margins with scattered oaks and river bottomlands or marshes next to deciduous woodland. Prefers open grasslands, meadows or marshes for</p> <p>Presumed Absent; The project area lacks open grassy field suitable for the species foraging. The nearest CNDDB occurrence is approximately 5 mile from</p>

Table 3: Special Status Species with Potential to Occur in the Project Vicinity				
Common Name	Common Name	Common Name	Common Name	Common Name
				foraging close to isolated, dense-topped trees for nesting and perching. Breeds Feb- Oct.
Grasshopper sparrow	<i>Ammodramus savannarum</i>	Fed: CA: DFG:	-- -- SSC	Inhabits dense grasslands on rolling hills, lowland plains, in valleys and on hillsides on lower mountain slopes.
Purple martin	<i>Progne subis</i>	Fed: CA: DFG:	-- -- SSC	Present in California as a summer migrant, arriving in March and departing by late September. Inhabits valley foothill and montane hardwood/hardwood-conifer, coniferous habitats and riparian habitats. Nests in tall, old, isolated trees or snags in open forest or woodland and in proximity to a body of water. Frequently nests within former woodpecker cavities; may nest in human-made structures such as nesting boxes, under bridges and in culverts. Breeds April-August.
Bank swallow	<i>Riparia riparia</i>	Fed: CA: DFG:	-- T --	A migratory colonial nester inhabiting lowland and riparian habitats west of the desert during spring - fall. Majority of current breeding populations occur along the Sacramento and Feather rivers in the north Central Valley. Requires vertical banks or cliffs with fine textured/sandy soils for nesting (tunnel and burrow excavations). Nests exclusively near streams, rivers, lakes or the ocean. Breeds May-July.
Mammal Species				
American badger	<i>Taxidea taxus</i>	Fed: CA: DFG:	-- -- SSC	Prefers treeless, dry, open stages of most shrub and herbaceous habitats with friable soils and a supply of rodent prey. Species also inhabits forest glades and meadows, marshes, brushy areas, hot deserts, and mountain meadows. Species maintains burrows within home ranges estimated between 338-1,700 acres, dependent on seasonal activity. Burrows are frequently re-used, but new burrows may be created nightly. Young are born in March and April within burrows dug in relatively dry, often sandy, soil, usually in areas with sparse overstory cover. Species is somewhat tolerant of human activity, but is sensitive to

Table 3: Special Status Species with Potential to Occur in the Project Vicinity					
Common Name	Common Name	Common Name	Common Name	Common Name	Common Name
				automobile mortality, trapping, and persistent poisons (up to 12,000 feet).	
Pallid bat	<i>Antrozous pallidus</i>	Fed: CA: DFG:	-- -- SSC	Inhabits deserts, grasslands, shrub lands, woodlands and forests. Most common in open, dry habitats with rocky areas for roosting.	Presumed Absent; The project area lacks the shrub land and forest habitat necessary for roosting. The project area is highly disturbed and lacks the necessary habitat for American badger. Nearest CNDDDB occurrence is 10 miles north of project area.
Reptile Species					
Western pond turtle	<i>Emys marmorata</i>	Fed: CA: DFG:	-- -- SSC	A fully aquatic turtle of ponds, marshes, rivers, streams and irrigation ditches with aquatic vegetation. Requires basking sites and suitable (sandy banks or grassy open field) upland habitat for reproduction (Sea level-4,690 feet).	Presumed Absent; The project area does not contain open grassy fields necessary for reproduction. Nearest CNDDDB occurrence is approximately 6 miles from project location; no occurrences are documented within the Cripple Creek drainage.
Giant garter snake	<i>Thamnophis gigas</i>	Fed: CA: DFG:	T T --	Inhabits marsh, swamp, wetland (including agricultural wetlands), sloughs, ponds, rice fields, low gradient streams and irrigation/drainage canals adjacent to uplands. Ideal habitat contains both shallow and deep water with variations in topography. Species requires adequate water during the active season (April-November), emergent, herbaceous wetland vegetation, such as cattails and bulrushes, for escape cover and foraging habitat and mammal burrows estivation. Requires grassy banks and openings in waterside vegetation for basking and higher elevation uplands for cover and refuge from flood waters during winter dormant season. Species is extremely shy and sensitive to disturbance.	Presumed Absent; Cripple Creek does not contain adequate water during the species active season; habitat unsuitable for giant garter snake. The nearest CNDDDB occurrence is greater than 10 miles from the project location. No occurrences are documented within Cripple Creek.
Amphibian Species					
California tiger salamander	<i>Ambystoma californiense</i>	Fed: CA: DFG:	T T SSC	Inhabits annual grasslands and the grassy understory of valley-foothill hardwood communities. Requires underground refuges, especially ground squirrel burrows and vernal pools or other seasonal water sources for breeding.	Presumed Absent; The project area lacks the annual grasslands and vernal pools required for breeding. The project area is highly disturbed and lacks the required habitat for California tiger salamander. The nearest CNDDDB occurrence is greater than 10 miles from the project area.
California red-legged frog	<i>Rana draytonii</i>	Fed: CA:	T --	Inhabits lowlands and foothills in or near permanent sources of deep	Presumed Absent; Cripple Creek in proximity to the project

Table 3: Special Status Species with Potential to Occur in the Project Vicinity					
Common Name	Common Name	Common Name	Common Name	Common Name	Common Name
		DFG:	SSC	water with dense, shrubby or emergent riparian vegetation. Requires 11-20 weeks of permanent water for larval development and must have access to estivation habitat. Occurs from elevations near sea level to 5,200 ft.	area does not contain adequate deep water or estivation habitat for the species. CNDDDB records show the nearest species occurrence is greater than 10 miles from the project area.
Western spadefoot	<i>Spea hammondi</i>	Fed: CA: DFG:	-- -- SSC	Inhabits burrows within grassland and valley foothill hardwood woodland communities. Requires vernal, shallow, temporary pools formed by heavy winter rains for reproduction. Breeds late winter-March.	Presumed Absent; Cripple Creek in proximity to the project area does not contain preferred valley foothill hardwood woodland communities necessary for western spadefoot. Nearest CNDDDB occurrence is 3.5 miles north west of the project area.
Invertebrate Species					
Vernal pool fairy shrimp	<i>Branchinecta lynchi</i>	Fed: CA: DFG:	T -- --	Endemic to the grasslands of the Central Valley, Central Coast mountains and South Coast Mountains, in astatic rain-filled pools. Inhabits small, clear-water sandstone-depression pools and grassed swale, earth slump, or basalt-flow depression pools. Species is dependent on seasonal fluctuations.	Presumed Absent; The project area lacks requisite vernal sandstone-depression pools and grassed swale, earth slump, or basalt-flow depression pools required for vernal pool fairy shrimp; habitat unsuitable. The nearest CNDDDB occurrence is greater than 10 miles from the project area.
Vernal pool tadpole shrimp	<i>Lepidurus packardii</i>	Fed: CA: DFG:	E -- --	Inhabits vernal pools and swales containing clear to highly turbid waters such as pools located in grass bottomed swales of unplowed grasslands, old alluvial soils underlain by hardpan, and mud-bottomed pools with highly turbid water.	Presumed Absent; The project area lacks requisite vernal pools and grassed swales required for vernal pool tadpole shrimp; habitat unsuitable. The nearest CNDDDB occurrence is greater than 10 miles from the project area.
Valley elderberry longhorn beetle	<i>Desmocerus californicus dimorphus</i>	Fed: CA: DFG:	T -- --	Requires elderberry shrubs (<i>Sambucus</i> sp.) as host plants. Typically in moist valley oak woodlands associated with riparian corridors in the lower Sacramento River and upper San Joaquin River drainages. Prefers elderberries 2-8 inches in diameter; some preference toward 'stressed' elderberries.	Presumed Absent; The project area lacks the requisite elderberry shrub habitat for valley elderberry longhorn beetle. The project area is highly disturbed and considered unsuitable habitat for Valley elderberry longhorn beetle. The nearest CNDDDB occurrence is 5 miles south east of the project area.
Fish Species					
Central Valley steelhead	<i>Oncorhynchus mykiss</i>	Fed: CA: DFG:	T -- --	Spawning occurs in small tributaries on coarse gravel beds in riffle areas. Central Valley steelhead are found in the Sacramento River system; the principal remaining wild populations spawn annually in Deer and Mill Creeks in Tehama County, in the lower Yuba River, a small	Presumed Absent; Cripple Creek in proximity to the project area does not provide adequate water, substrates, or connectivity to known river populations; habitat unsuitable for Central Valley steelhead. CNDDDB records show the nearest species

Table 3: Special Status Species with Potential to Occur in the Project Vicinity					
Common Name	Common Name	Common Name		Common Name	Common Name
				population in the lower Stanislaus River and, though potentially extirpated, from the San Joaquin basin.	is 1.75 miles northwest.
Delta smelt	<i>Hypomesus transpacificus</i>	Fed: CA: DFG:	T E --	Occurs within the Sacramento-San Joaquin Delta and seasonally within the Suisun Bay, Carquinez Strait and San Pablo Bay. Most often occurs in partially saline waters.	Presumed Absent; Cripple Creek in proximity to the project area is outside the range of the species; habitat unsuitable for delta smelt. The nearest CNDDDB occurrence is 5 miles from the project area.
Federal Designations (Fed): (FESA, USFWS) C: Federal candidate D: Federally delisted E: Federally listed, endangered T: Federally listed, threatened				State Designations (CA): (CESA, CDFG) E: State-listed, endangered T: State-listed, threatened FP: CDFG Fully Protected	
Other Designations DFG_SSC: DFG Species of Special Concern DFG_FP: DFG Fully Protected California Native Plant Society Designations: <i>*Note: according to CNPS (Skinner and Pavlik 1994), plants on Lists 1B and 2 meet definitions for listing as threatened or endangered under Section 1901, Chapter 10 of the CFG Code. This interpretation is inconsistent with other definitions.</i> 1A: Plants presumed extinct in California. 1B: Plants rare and endangered in California and throughout their range. 2: Plants rare, threatened, or endangered in California but more common elsewhere in their range. 3: Plants about which need more information; a review list. 4: Plants of limited distribution; a watch list. Plants 1B, 2, and 4 extension meanings: _1 Seriously endangered in California (over 80% of occurrences threatened / high degree and immediacy of threat) _2 Fairly endangered in California (20-80% occurrences threatened) _3 Not very endangered in California (<20% of occurrences threatened or no current threats known)					
Potential for Occurrence Criteria: Present: Species was observed on site during a site visit or focused survey. High: Habitat (including soils and elevation factors) for the species occurs on site and a known occurrence has been recorded within 5 miles of the site. Low-Moderate: Either low quality habitat (including soils and elevation factors) for the species occurs on site and a known occurrence exists within 5 miles of the site; or suitable habitat strongly associated with the species occurs on site, but no records were found within the database search. Presumed Absent: Focused surveys were conducted and the species was not found, or species was found within the database search but habitat (including soils and elevation factors) do not exist on site, or the known geographic range of the species does not include the survey area. Source: (CNDDDB 2015), (CNPS 2015), (Miller and Hornaday 1999), (Shuford and Gardali 2008), (Kyle, Keiller 2011) (Zeiner 1988-1990), (University of California 2012). (University of California Davis 2012). (USFWS 2015)					

Standards of Significance

Based on Appendix G of the State CEQA Guidelines, an impact on biological resources is considered significant if implementation of the General Plan and GGRP would:

- Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by DFG or USFWS;
- Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, or regulations, or by DFG or USFWS;
- Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including but not limited to marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means;
- Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of nursery sites by native wildlife;

- Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance; or
- With the provisions of an adopted habitat conservation plan (HCP), natural community conservation plan (NCCP), or other approved local, regional, or state HCP.

The City of Citrus Heights and City of Roseville General Plan established local policies protecting biological resources. There would be no conflict with any local policies or ordinances protecting biological resources. Compliance with local tree preservation ordinances will be required prior to the start of construction and will be discussed further as a potentially significant impact.

No adopted or planned HCP or NCCP covers the planning area. For this reason, there would be no conflicts between the project and any HCP or NCCP. This significance criterion is not discussed further in the EIR.

Summary of Analysis under the City of Citrus Heights General Plan EIR, and the City of Roseville General Plan

City of Citrus Heights

Chapter 4.6.1 of the General Plan EIR evaluated the effects of the General Plan on biological resources within the general plan policy area. The General Plan EIR identified potential impacts in terms of degradation of the quality of the environment or reduction of habitat or population below self-sustaining levels of special status birds, through the loss of both nesting and foraging habitat.

Policies in the General Plan were identified as mitigating the effects of development that could occur under the provisions of the General Plan. Policy ER 34.3 requires the City to consider the potential impact on sensitive plants for each project and to require pre-construction surveys when appropriate.

The General Plan EIR concluded that the cumulative effects of development that could occur under the General Plan would be less than significant as they related to effects on special status plant species, reduction of habitat for special status invertebrates, loss of habitat for special status birds, loss of habitat for special status amphibians and reptiles, loss of habitat for special status mammals, special status fish and, in general, loss of riparian habitat, wetlands and sensitive natural communities.

City of Roseville

Chapter 5 of the Open Space and Conservation Element evaluated effects of the General Plan on biological resources within the City of Roseville area. The General Plan identified potential impacts in terms of degradation of the quality of the environment or reduction of habitat or population below self-sustaining levels of special status birds, through the loss of both nesting and foraging habitat.

Policies in the General Plan were identified as mitigating the effects of development that could occur under the provisions of the General Plan. Policies 1 through 13 require the City to consider the potential impact on existing habitats and sensitive species.

The General Plan concluded that the cumulative effects of development that could occur under would be less than significant with mitigation incorporated as they related to effects on special status plant species, reduction of habitat for special status invertebrates, loss of habitat for special status birds, loss of habitat for special status amphibians and reptiles, loss of habitat for special status mammals, special status fish and, in general, loss of riparian habitat, wetlands and sensitive natural communities.

Discussion

- a. No impact. As addressed within the 2005 ABSP FEIR, all special status species are presumed absent within the project area. Considering the amount of development and hardscape in the project area, the current frequency and volume of human activity, the amount of affected foraging habitat within the project limits, anticipated absence of species nesting, and the implementation of minimization and avoidance measures incorporated into the project design, the project would not impact the viability of the overall population and further consultation under CESA is not anticipated. To minimize and avoid potential impacts to potential special status species occurrences and vegetation clearing, the project would comply with mitigation measures B-2 through B-5 (ABSP 2005).
- b. No Impact. As addressed within the 2005 ABSP FEIR, Riparian habitat and other sensitive natural communities were not observed within the project construction limits. The project construction will take place within landscaped and developed areas (ABSP 2005).
- c. No impact. As addressed within the 2005 ABSP FEIR, Cripple Creek is the closest waterway to the project area and is located 180 feet southwest of the project area. Cripple Creek is a major tributary of Arcade Creek, which is a tributary to the Sacramento River, and is considered Waters of the U.S. However, no impacts to Cripple Creek or direct runoff would occur due to the project. No vernal pools or wetland habitat occurs within the proximity to the project site. The project would result in no permanent or temporary impacts to Waters of the U.S. or State (ABSP 2005).
- d. Less-than-Significant with Mitigation Incorporated. No interference would take place due to the project with the movement of any native resident or migratory fish or wildlife species or with any established native resident or migratory wildlife corridors. Project construction will take place on landscaped and hardscape area. However, trees will be removed due to project construction. Therefore, to protect migratory birds B-2 through B-5 will be implemented to ensure protection of migratory nesting birds.
- e. Less-than-Significant with Mitigation Incorporated. As addressed within the 2005 ABSP FEIR, the project is subject to the City of Citrus Heights and the City of Roseville Tree Ordinance. At this time, approximately 57 trees will be impacted by the project (Arborist Report & Tree Inventory Summary, 2015). The trees will be removed prior to construction and replanted after construction along the widened road and within existing medians. According to the City of Citrus Heights and City of Roseville Tree Preservation and Protection Regulations, native oak trees 6 inches or more in diameter, and mature trees 19 inches or more in diameter are protected and require a permit for removal. Some removals may require the replanting of trees or the payment of a mitigation fee and a report from a certified arborist. An arborist survey will be conducted by Acorn Arboricultural Services, Inc. to determine species found within the project area, and, if necessary, a tree removal permit will be obtained from both Cities prior to construction. Mitigation measure B-2 through B-5 will also be implemented to further reduce impacts to vegetation within the project area (ABSP 2005).
- f. No Impact. As addressed within the 2005 ABSP FEIR, the project will not conflict with any locally adopted Habitat Conservation Plan, Natural Community Conservation Plan, etc. Within the project area, there are no resource based plans that address special status species (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

City of Citrus Heights General Plan EIR Mitigation Measure B-3 and City of Roseville General Plan Mitigation Measure 4.9-3 have been incorporated within Mitigation Measures B-4 and B-5 of this section.

Applicable ABSP EIR Mitigation Measures Incorporated

B-1: To ensure consistency with the City of Citrus Heights' Policy 37.1, which requires incorporation of existing trees into development projects, building envelopes for future development projects should be configured to minimize impacts to trees to the extent feasible. The following measures shall be implemented:

1. Building envelopes should be established on plans and specifications for the future development projects to designate the area needed for construction of roads, driveways, and building pads.
2. These building envelopes should be large enough to include not only the improvements, but also work areas for heavy equipment, staging areas, and equipment and material lay down areas.
3. To protect trees elsewhere on construction sites, no construction activities or use of heavy equipment should occur outside of the building envelopes.
4. Oaks that fall within the building envelope but which are not slated for removal should be protected by the following measures, which should be implemented during all construction phases of the project:

- a. Plans and specifications should clearly state protection procedures for oaks to be preserved on the project site. The specifications should also require contractors to stay within designated work areas and should include a provision for penalties if oak trees are damaged;
- b. No vehicles, construction equipment, mobile offices, or materials should be parked or located within the driplines of oaks and other trees that are to be preserved;
- c. Soil surface removal should not occur within the driplines of oaks to be preserved. No cuts or trenching should occur within the dripline. If this area cannot be avoided, then the tree should be added to the list of oaks to be replaced through an on-site planting;
- d. Earthen fill deep should not be placed within the driplines of oak trees to be retained, and no fill should be placed within five feet of their trunks.
- e. Paving should not be placed within the dripline of oaks to be retained;
- f. Underground utility line trenching should not occur within the driplines of oaks to be retained. If it is absolutely necessary to install underground utilities within the driplines of oak trees, the trench should either be bored or drilled but not within five feet of the trunk and a certified arborist should be retained to monitor this construction and repair or wrap any damaged roots.
- g. Living Among the Oaks: A Management Guide for Landowners (UC Cooperative Extension, Berkeley) in Appendix H should be used by the City as a guide in reviewing landscape plans. The information should be distributed to landowners and developers to provide information and guidelines for preparing landscape plans and for protecting oaks after construction is complete.

B-1B: Prepare and Implement Oak Replacement and Management Plan (Oak Woodland Replacement): In order to compensate for impacts due to removal of native oak trees found within oak woodland and/or riparian habitats (as opposed to isolated landscape or street trees), the following measures shall be implemented:

1. Oak trees shall be planted on project sites or off-site in numbers and species composition similar to those impacted.
2. Prior to approval of development or redevelopment projects, a qualified biologist or arborist should make an accurate count of the number, diameter, and species of trees that would be removed within each building envelope or area subject to disturbance. Based on the estimate, an Oak Replacement and Management Plan (Oak Plan) should be prepared in accordance with the City of Citrus Heights Tree Ordinance, and consistent with the City of Citrus Heights General Plan biological resource goals and policies.

a. The goals of the Oak Plan should be to replace trees lost by the project to create healthy, self-sustaining habitats that are not dependent on maintenance or irrigation following the minimum maintenance period.

The functions and values of the created habitat should approximate those of the affected habitats, i.e., the functions and values of oak woodland rather than an ornamental landscape planting.

b. At a minimum, the Oak Plan should include clear success criteria, monitoring and reporting requirements, and a contingency plan should the responsible parties fail to meet the success criteria that ensure that mitigation goals and ratios are met. The Oak Plan should also include details for the species, size of plants and quantities, planting techniques, techniques for protecting the trees from herbivory, and irrigation, weed control and maintenance plan, and monitoring requirements.

B-1C: Prepare and Implement Oak Replacement and Maintenance Plan (Landscape Tree Replacement): In order to compensate for impacts due to removal of native oak trees found within landscape settings (i.e. isolated landscape or street trees), the following measures shall be implemented:

1. Oaks trees shall be planted on project sites or off-site in numbers and species composition similar to those impacted.

a. Prior to approval of development or redevelopment projects, a qualified biologist or arborist should make an accurate count of the number, diameter, and species of trees that would be removed within each building envelope or area subject to disturbance. Based on the estimate, an Oak Replacement and Maintenance Plan (Oak Plan) should be prepared in accordance with the City of Citrus Heights Tree Ordinance, and consistent with the City of Citrus Heights General Plan biological resource goals and policies.

The goals of the Oak Plan, at a minimum, should be to replace trees lost by the project in an appropriate landscape setting that will allow trees to thrive and be self-sustaining and not dependent on maintenance or irrigation following the minimum maintenance period. Replacement within the specific plan area's planned landscape areas as street trees, trees for public space landscape or roadway medians, should be emphasized when identifying replanting sites. Replacement in a natural habitat setting as described in Measure B-2B would also accomplish these oak tree replacement goals.

B-1D: Preconstruction Tree Survey: Prior to construction, a qualified biologist or arborist should make an accurate count of the number, diameter, condition and species of trees that would be removed by the roadway improvement project. An Oak Tree Replacement and Management Plan shall be prepared in accordance with Mitigation Measures B-2A, B-2B and B-2C described above.

B-2: Avoid Impacts to Nesting Birds

1. If tree removal for construction will occur during the nesting season (February through July), a minimum of two preconstruction surveys should be conducted in construction areas for nesting birds. Surveys shall be conducted by a qualified wildlife biologist.
2. Surveys should be conducted no more than 14 days prior to the initiation of tree-removal activities during the early part of the breeding season (February through April) and no more than 30 days prior to the initiation of these activities during the late part of the breeding season (May through July).
3. If the surveyor deems that an active bird nest is close enough to the construction area to be disturbed, he or she should (in consultation with CDFG) determine the extent of the construction-free buffer zone to be established around the nest.
4. Trees should be removed outside the nesting season (February through July), or after a qualified wildlife biologist verifies that the nest is empty and the nest tree is no longer used by the adults and young birds.

- B-3:** Avoid Introduction and Spread of New Noxious Weeds. In the vicinity of Cripple Creek, during construction only certified weed-free straw will be used and all disturbed soils will be thoroughly covered with straw (or mulch or chips created on-site during tree removal) upon completion of grading. No seed mixes should be used unless consisting of locally native grasses and forbs.

Additional Project Level Mitigation Measures

- B-4:** Pursuant to the City of Citrus Heights General Plan EIR Mitigation Measure B-3 and the City of Roseville Mitigation Measure 4.9-3, if construction is planned to occur during the raptor nesting season (February – August) a preconstruction raptor nesting survey shall be conducted by a qualified biologist within 7 days prior to vegetation removal. Vegetation surveyed shall include all trees, 10 feet or taller and containing a dbh of 2 inches or greater. Within 2 weeks of the nesting raptor survey, all vegetation cleared by the biologist shall be removed by the contractor.

A minimum 500 foot no-disturbance buffer shall be established around any active raptor nest to limit the impacts of construction activities. The contractor shall immediately stop work in the nesting area until the appropriate buffer is established and is prohibited from conducting work that could disturb the birds (as determined by the project biologist and in coordination with wildlife agencies) in the buffer area until a qualified biologist determines the young have fledged.

- B-5:** Pursuant to the City of Citrus Heights General Plan EIR Mitigation Measure B-3 and the City of Roseville Mitigation Measure 4.9-3, if ground disturbance or vegetation removal is to take place during the breeding season (February – August), a pre-construction nesting bird survey shall be conducted within 7 days prior to vegetation removal. Vegetation surveyed shall include all trees, bushes, tall grasses and emergent vegetation. Within 2 weeks of the nesting bird survey, all vegetation cleared by the biologist shall be removed by the contractor.

A minimum 100 foot no-disturbance buffer shall be established around any active nest to limit the impacts of construction activities. The contractor shall immediately stop work in the nesting area until the appropriate buffer is established and is prohibited from conducting work that could disturb the birds (as determined by the project biologist and in coordination with wildlife agencies) in the buffer area until a qualified biologist determines the young have fledged.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
V. CULTURAL RESOURCES - Would the project:					
a. Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?		X			
b. Cause a substantial adverse change in the significance of an archaeological		X			

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
V. CULTURAL RESOURCES - Would the project:					
resource pursuant to §15064.5?					
c. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?		X			
d. Disturb any human remains, including those interred outside of formal cemeteries?		X			
e. Cause a substantial adverse change in the significance of a tribal cultural resource as defined in Public Resource Code 21074?					X

Discussion

- a. No Impact. Per the Auburn Boulevard Specific Plan EIR, there are no known historical resources within the project area. There are no significant cultural resources located within proximity of the project area. The existing site was previously graded and developed (ABSP 2005). On June 23, 2015, Dokken Engineering Archaeologist, Namat Hosseinion, P.I., conducted a pedestrian field survey of the additional project area located within the City of Roseville. No potential cultural resources or historical resources were observed within the project area.
- b. No impact. Per the Auburn Boulevard Specific Plan EIR, there are no known archaeological resources within proximity of the project area in the City of Citrus Heights (ABSP 2005). During the June 23, 2015 pedestrian survey, no additional potential archaeological resources were observed within the City of Roseville portion of the project.
- c. The project is not anticipated to impact paleontological resources. The project area has been disturbed previously by construction of the surrounding development and industrial uses. As documented in the City of Citrus Heights General Plan FEIR, the City of Roseville General Plan, and Auburn Boulevard Specific Plan EIR, the general City of Citrus Heights and project area is not considered sensitive for paleontological resources (ABSP 2005).
- d. Less-than-Significant with Mitigation Incorporated. No human remains or cemeteries were identified within proximity of the project area. See CR-1 and CR-2 of the Auburn Boulevard Specific Plan EIR.
- e. No Impact. Effective July 1, 2015, CEQA was revised to include early consultation with California Native American tribes and consideration of tribal cultural resources (TCRs). These changes were enacted through Assembly Bill 52 (AB 52). By including TCRs early in the CEQA process, AB 52 intends to ensure that local and Tribal governments, public agencies, and project proponents would have information available, early in the project planning process, to identify and address potential

adverse impacts to TCRs. CEQA now establishes that a “project with an effect that may cause a substantial adverse change in the significance of a TCR is a project that may have a significant effect on the environment” (PRC § 21084.2).

To help determine whether a project may have such an adverse effect, the PRC requires a lead agency to consult with any California Native American tribe that requests consultation and is traditionally and culturally affiliated with the geographic area of a project. That consultation must take place prior to the determination of whether a negative declaration, mitigated negative declaration, or environmental impact report is required for a project (PRC § 21080.3.1). Consultation must consist of the lead agency providing formal notification, in writing, to the tribes that have requested notification or projects within their traditionally and culturally affiliated area. AB 52 stipulates that the Native American Heritage Commission (NAHC) shall assist the lead agency in identifying the California Native American tribes that are traditionally and culturally affiliated within the project area. If the tribe wishes to engage in consultation on the project, the tribe must respond to the lead agency within 30 days of receipt of the formal notification. Once the lead agency receives the tribe’s request to consult, the lead agency must then begin the consultation process within 30 days. If a lead agency determines that a project may cause a substantial adverse change to TCRs, the lead agency must consider measures to mitigate that impact. Consultation concludes when either: 1) the parties agree to measures to mitigate or avoid a significant effect, if a significant effect exists, on a TCR, or 2) a party, acting in good faith and after reasonable effort, concludes that mutual agreement cannot be reached (PRC § 21080.3.2). Under existing law, environmental documents must not include information about the locations of an archaeological site or sacred lands or any other information that is exempt from public disclosure pursuant to the Public Records act. TCRs are also exempt from disclosure.

For purposes of this study, the term “tribal cultural resource” refers to either of the following:

1. Sites, features, places, cultural landscapes, sacred places, and objects with cultural value to a California Native American tribe that are either of the following:
 - a. Included or determined to be eligible for inclusion in the California Register of Historical Resources
 - b. Included in a local register of historical resources as defined in subdivision (k) of California Public Resources Code (PRC) Section 5020.1
2. A resource determined by a California lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of the PRC Section 5024.1.

In July, 2015, the City of Citrus Heights obtained a list of California Native American tribes traditionally and culturally affiliated with the project area from the NAHC. The City then sent initial letters inviting to the tribes detailed on the NAHC list to consult under AB 52 for the project. This initial letter provided a brief project description, project location, lead agency contact information, and a notification that the tribe has 30 days to request consultation. No TCRs were identified within the project area.

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

CR-1: Handling of Discovered Artifacts or Remains: Should any cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, human remains, or architectural remains be encountered during any development activities, work shall be suspended according to (A) below.

It is recommended under CEQA and Policy 41.1 of the Citrus Heights General Plan that:

1. In the event that any prehistoric, historic, or paleontological resources are discovered during construction-related earth moving activities, all work within 50 feet of the resources shall be halted and the developer shall consult with a qualified archaeologist or paleontologist to assess the significance of the find. If any find is determined to be significant by the qualified archaeologist, then
2. representatives from the City of Citrus Heights and the qualified archaeologist and/or paleontologist would meet to determine the appropriate course of action.
3. Pursuant to Section 5097.97 of the State Public Resources Code and Section 7050.5 of the State Health and Safety Code, in the event of the discovery of human remains, all work is to stop and the County Coroner shall be immediately notified. If the remains are determined to be Native American, guidelines of the Native American Heritage Commission shall be adhered to in the treatment and disposition of the remains. And that under Policy 42.3 of the Citrus Heights General Plan that planners establish thresholds by which future projects can be judged when considering historic impacts. These standards should include height and massing considerations for projects that are located in close proximity to historic resources (individual structures and districts) and define locations for potential prehistoric resources.

CR-2: Evaluation of Historic Resources Older than 45 Years. Prior to approval of projects or issuance of construction or grading permits, cultural resources that appear to be 45 years old or older on a project site need to be recorded for the purposes of inclusion in the State Office of Historic Preservation's filing system. "The 45 year criterion recognizes that there is commonly a five year lag between resource identification and the date that planning decisions are made" (California, State of 1995). Should the five year period lapse between the completion of the initial cultural resources documentation and the start date of the project, the cultural resource studies would need to be updated to include any additional properties/sites that would, by that time, meet the 45 year criteria.

Additional Project Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
VI. GEOLOGY AND SOILS - Would the project:					
a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:		X			
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.		X			
ii. Strong seismic ground shaking?		X			
iii. Seismic-related ground failure, including liquefaction?		X			
iv. Landslides?		X			
b. Result in substantial soil erosion or the loss of topsoil?		X			
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?		X			
d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?		X			
e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?		X			

Discussion

- a.i-iv. No Impact. As addressed within the 2005 ABSP FEIR, the project would not expose people or structures to potential substantial or adverse effects (ABSP 2005).
- i. California Department of Conservation (CDC) California Geological Survey does not list Sacramento County or Placer County as affected by the Alquist-Priolo Earthquake Fault Zone. According to the Fault Activity Map of California and Adjacent Areas, no active faults are located within the project area.
 - ii. The project would not expose people or structures to seismic ground shaking due to the lack of active faults within the City and the nature of the project activities.
 - iii. The project would not create ground failure or liquefaction. Given the soil types and depth to bedrock, the ground at this site is not prone to liquefaction. In addition, the project does not involve the construction of structures which would regularly be occupied by people.
 - iv. The CDC does not list Sacramento County or Placer County as an area at risk for Landslides as identified in the California Department of Transportation's (Caltrans) Highway Corridor Mapping project (CDC 2007). Therefore, the project would not expose people or structures to substantial risk of landslides.
- b. No Impact. As addressed within the 2005 ABSP FEIR, project construction would not generate soil erosion concerns or the loss of topsoil. Standard BMPs will be implemented during construction as mentioned in the Auburn Boulevard Specific Plan EIR. The project would comply with Sacramento and Placer County NPDES for discharges of urban runoff from Municipal Separate Storm Sewer Systems (MS4s). The project would comply with the Stormwater Quality Improvement Plan (SQIP), which will adequately control all erosion. Therefore, no impacts from soil erosion are anticipated.
- c. No Impact. The project is not located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse.
- d. No Impact. The project is not located on expansive soil. Special design considerations are not required for expansive soils, as they are not located within the project area.
- e. No Impact. The projects would not use a septic tank system. In addition, sewage is not required for the project; therefore, the project will have no impacts related to septic systems.

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

None.

Additional Project-Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Not applicable.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
VII. GREENHOUSE GAS EMISSIONS - Would the project:					
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?				X	
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?				X	

Setting

As part of its action in approving the City of Citrus Heights General Plan, and the City of Roseville General Plan, the City Councils certified the Master EIR that evaluated the environmental effects of development that is reasonably anticipated under the new General Plan. The General Plan EIR includes extensive discussion of the potential effects of greenhouse gas emissions. The General Plan EIR discussions regarding climate change are incorporated here by reference. See, for example:

Final EIR: Greenhouse Gas Reduction Plan

This document is available at <http://www.citrusheights.net/203/Greenhouse-Gas-Reduction-Plan> and at the offices of City of Citrus Heights at 6237 Fountain Square Drive, Citrus Heights, California.

Final Report: City of Roseville Greenhouse Gas Emissions Reduction Plan Analysis

This document is available at <https://www.roseville.ca.us/lp/supersize/ClimateActionPlan.pdf> and at the offices of City of Roseville at 311 Vernon Street, Roseville, California.

The project is consistent with the land use designations for the project site. The project would result in the generation of greenhouse gases during construction and operation, as discussed below.

Environmental Consequences

Short-Term Construction Emissions

During construction of the project, temporary GHG emissions would be emitted from the operation of construction equipment and from worker supply vendor vehicles. Emissions from construction equipment are not expected to exceed the threshold of significance.

Long-Term Construction Emissions

Because the project consists of road improvements, and does not increase capacity of the roadway, there are no long-term operational activities associated with the project. The project would not lead to changes in vehicular operations and associated emissions. While there may be maintenance visits to the project site, these visits are expected to be infrequent, and occur for emergency repair or for repaving, which occurs after the lifetime of the installed pavement has been reached. Long term operational emissions are thus expected to be negligible.

Ongoing Activities

City of Citrus Heights

The General Plan includes a Greenhouse Gas Reduction Plan for the City. The Greenhouse Gas Reduction Plan provided additional guidance, strategies and measures for the City's ongoing efforts to reduce GHG emissions.

To prevent the continued escalation of GHG emissions, the Greenhouse Gas Reduction Plan establishes a 2020 target (10 to 15 percent below 2005 levels) to reduce annual emissions levels consistent with state laws and guidelines. According to the Greenhouse Gas Reduction Plan, the actions that could be quantified along with those that could not outline a path to meet the City's 2020 reduction target, consistent with state laws and guidelines. When combined with quantified state and federal legislative reductions, primary actions contained in the Greenhouse Gas Reduction Plan offer a potential reduction of about 145,677 metric tons of carbon dioxide equivalent (CO₂e) annually. This level of reduction will meet the City's 2020 target of 10 to 15 percent and is consistent with state laws.

City of Roseville

The General Plan includes a Greenhouse Gas Emissions Reduction Plan Analysis for the City through 2015. The Greenhouse Gas Reduction Plan provided additional guidance, strategies and measures for the City's ongoing efforts to reduce GHG emissions.

To prevent the continued escalation of GHG emissions, the Greenhouse Gas Reduction Plan established a 2015 target (20 percent below 2000 levels) to reduce annual emissions levels consistent with state laws and guidelines. According to the Greenhouse Gas Reduction Plan, the actions that could be quantified along with those that could not outline a path to meet the City's 2015 reduction target, consistent with state laws and guidelines. When combined with quantified state and federal legislative reductions, primary actions contained in the Greenhouse Gas Reduction Plan offered a potential reduction of up to 16,000 metric tons of carbon dioxide equivalent (CO₂e) annually. Currently, this level of reduction has met the City's 2015 target of 20 percent and is consistent with state laws.

State and Federal Levels

In addition to the Greenhouse Gas Reduction Plan, GHG-reduction strategies continue at the state and federal level to combat climate change. In December 2009, the EPA listed GHG as harmful emissions under the Clean Air Act. This action could eventually result in regulations with a purpose of reducing such emissions.

The General Plan EIR concluded that GHG emissions that could be emitted by development that is consistent with the General Plan would be less than significant. The General Plan EIR includes a full analysis of GHG emissions and climate change, and adequately addresses these issues. As indicated in the General Plan EIR, future development within the City of Citrus Heights and the City of Roseville will be required to comply with Assembly Bill (AB) 32 and with the Sacramento Area Council of Governments (SACOG) 2035 Metropolitan transportation Plan (MTP). The 2035 MTP is anticipated to meet the AB 32 goal of reaching 1990 transportation emissions by 2020. The City is not anticipating an increase in GHG emission with the incorporation of reduction measures.

The project must comply with the General Plan policies and measures for the reduction of GHGs to comply with the 2035 MTP and AB 32. Because the traffic from the project was assumed in the 2035 MTP, and the 2035 MTP is anticipated to meet the goals of AB 32, the project would comply with the 2035 MTP. AB 32 requires an approximate 29 percent reduction from existing emissions on a statewide level in order to achieve the goal of reducing GHG emissions to 1990 levels by 2020.

The General Plan EIR for the City of Citrus Heights and the City of Roseville General Plan allows for periodic maintenance on established arterial streets, such as Auburn Boulevard, therefore the GHG emissions increase that would occur with implementation of the project has been accounted for in the General Plan. The project would not impede the City's efforts to comply with AB 32 requirements. Therefore, the projects cumulative impacts related to construction and operation of the project conflicting with applicable plans, policies, or regulations adopted for the purpose of reducing GHG emissions would be less than significant. The project would not have any significant additional environmental effects relating to GHG emissions or climate change.

Discussion

- a. Less-than-Significant Impact. Short-term, negligible GHG emissions would result from the construction equipment and worker vehicles. Worker vehicles would be limited to minimum necessary causing a less-than-significant impact to generation of GHG emissions in the region (ABSP 2005).
- b. Less than Significant. The project would not conflict with the City of Roseville Greenhouse Gas Emissions Reduction Plan or the City of Citrus Heights Greenhouse Gas Reduction Plan. The project would not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases.

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

None.

Additional Project-Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Less than significant.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
VIII. HAZARDS AND HAZARDOUS MATERIALS - Would the project:					
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?			X		
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?			X		
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?		X			
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?		X			
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?		X			
f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?		X			
g. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?		X			
h. Expose people or structures to a		X			

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
VIII. HAZARDS AND HAZARDOUS MATERIALS - Would the project:					
significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?					

Setting

Federal regulations and regulations adopted by the Sacramento Metropolitan Air Quality Management District Management District (SMAQMD) and the Placer County Air Pollution Control District (PCAPCD) apply to the identification and treatment of hazardous materials during demolition and construction activities. Failure to comply with these regulations respecting asbestos may result in a Notice of Violation being issued by the AQMD and APCD and civil penalties under state and/or federal law, in addition to possible action by U.S. EPA under federal law.

Federal law covers a number of different activities involving asbestos, including demolition and renovation of structures (40 CFR § 61.145).

SMAQMD Rule 902 and Commercial Structures, and PCAPCD Asbestos Demolition Requirements:

The work practices and administrative requirements of Rule 902 apply to all commercial renovations and demolitions where the amount of Regulated Asbestos-Containing Material (RACM) is greater than:

- 260 lineal feet of RACM on pipes, or
- 160 square feet of RACM on other facility components, or
- 35 cubic feet of RACM that could not be measured otherwise.

The administrative requirements of Rule 902 apply to any demolition of commercial structures, regardless of the amount of RACM.

Asbestos Surveys

To determine the amount of RACM in a structure, Rule 902 and PCAPCD requirements state that a survey be conducted prior to demolition or renovation unless:

- the structure is otherwise exempt from the rule, or
- any material that has a propensity to contain asbestos (so-called "suspect material") is treated as if it is RACM.

Surveys must be done by a licensed asbestos consultant and require laboratory analysis. Asbestos consultants are listed in the phone book under "Asbestos Consultants." Large industrial facilities may use non-licensed employees if those employees are trained by the U.S. EPA. Questions regarding the use of non-licensed employees should be directed to the AQMD and PCAPCD.

Removal Practices, Removal Plans/Notification and Disposal

If the survey shows that there are asbestos-containing materials present, the SMAQMD and PCAPCD recommends leaving it in place.

If it is necessary to disturb the asbestos as part of a renovation, remodel, repair or demolition, Cal OSHA and the Contractors State License Board require a licensed asbestos abatement contractor be used to remove the asbestos-containing material.

There are specific disposal requirements for friable asbestos-containing material, including disposal at a licensed landfill. If the material is non-friable asbestos, any landfill willing to accept asbestos-containing material may be used to dispose of the material.

Summary of Analysis under the City of Citrus Heights General Plan EIR, and the City of Roseville General Plan

City of Citrus Heights

This section is derived from information contained in the General Plan EIR for the City of Citrus Heights General Plan and the Initial Site Assessment for the Auburn Boulevard Complete Streets, Phase 2, Citrus Heights, California (Dokken Engineering, June 2015). Record searches conducted for the Initial Site Assessment (ISA) identified twenty-two (22) sites within one mile of the project alignment for which hazardous materials issues may be present. These sites are identified based on records that show historic soil and/or groundwater contamination; or they are existing or historic land uses commonly associated with hazardous waste contamination problems. The General Plan EIR evaluated effects of development on hazardous materials, emergency response and aircraft crash hazards. See Chapter 4.6. Implementation of the Project may result in the exposure of people to hazards and hazardous materials during construction activities. Impacts identified within the EIR and ISA related to construction activities and operations were found to be less than significant with mitigation incorporated. Policies included in the General Plan were effective in reducing the identified impacts.

City of Roseville

State and federal legislation, which address concerns regarding hazardous materials, provide much of the framework within which Roseville works to manage safety issues relating to hazardous materials. A variety of laws are now in effect that regulate hazardous materials clean-up, storage, testing procedures and financial assistance for hazardous waste reduction. Policies included in the City of Roseville General Plan were effective in reducing potential impacts relating to hazardous materials. See Chapter 8, Policies 1 through 4.

Discussion

- a-b. Less-than-Significant with Mitigation Incorporated. The Initial Site Assessment conducted by Dokken Engineering (2015) for the project identified existing and historic land uses that were associated with contamination. The most common type of sites are existing or historic service stations with possible leaks from underground storage tanks. Based on the site reconnaissance, potential REC's on properties adjacent to the project boundaries include the following:
- Potential for PCB's with existing pole-mounted electrical transformers,
 - Potential for underground fuel storage tank leaks from the four gas stations (Chevron, Shell, Towne Mart, and an abandoned gas station) in the project area located along Auburn Boulevard, and
 - Potential chemical spills from Paradise Cleaners near the intersection of Auburn Boulevard and Baird Way.

The ISA recommended additional actions to verify the presence/extent of RECs and to evaluate the potential for remediation during the Plans, Specifications & Estimate (PS&E) phase of the Auburn Boulevard Complete Streets – Phase 2 Project: Impacts related to emissions of hazardous materials are significant but can be mitigated to a less than significant level by following the precautions required for construction and demolition activities in areas where contamination may be present.

Naturally Occurring Asbestos (NOA) can occur in serpentine rock. The most common forms of NOA minerals are chrysotile, actinolite, and tremolite. A review of the “General Location Guide for Ultramafic Rocks in California – Areas likely to Contain Naturally Occurring Asbestos” (CGS Open-file Report 2000-19) indicated that NOA was not mapped on, or in the near vicinity of the project site. The nearest ultramafic rock formation which may be associated with naturally occurring asbestos is approximately 10 miles east of the project area, along the eastern banks of Folsom Lake (USGS, 2015).

The roadway improvements in the vicinity of residences adjacent to Auburn Boulevard may involve removal of yellow thermoplastic traffic striping from the existing roadway surface. Yellow traffic stripes may contain heavy metals such as lead and chromium at concentrations in excess of hazardous waste thresholds established by the CCR and may produce toxic fumes when heated. Disposal of stripes is required at a Class 1 disposal facility. See Mitigation Measure HM-1 through HM-7.

- c. Less-than-Significant Impact. The project would not emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school. The closest public school to the project area is located approximately 0.5 mile south at Mesa Verde High School. In addition, mitigation measures HM-1 will be implemented (ABSP 2005).
- d. Less-than-Significant with Mitigation Incorporated. The project is located within an area of existing and historic land uses that were associated with contamination on the Cortese List of hazardous materials sites from EnviroStor. The ISA recommended additional actions to verify the presence/extent of RECs and to evaluate the potential for remediation during the Plans, Specifications & Estimate (PS&E) phase of the Auburn Boulevard Complete Streets – Phase 2 Project: Impacts related to emissions of hazardous materials are significant but can be mitigated to a less than significant level by following the precautions required for construction and demolition activities in areas where contamination may be present. See HM-1 through HM-7.
- e-f. No Impact. The project is not located near an airport or airstrip. Since the project site is not located within two miles of an airport or an area for which an Airport Land Use Plan has been prepared, and no public or private airfields are within two miles of the project area, users of the project would not be exposed to hazards due to over flight aircraft. Thus, no significant impact would occur, and no mitigation would be necessary (ABSP 2005).
- g. No Impact. The project is not expected to impair the implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan for the City of Citrus Heights or the City of Roseville. The project would not result in an increased concentration of large numbers of persons in any at-risk location, and the project would not have a significant impact on any emergency plans. Thus, no significant impact would occur, and no mitigation would be necessary (ABSP 2005).
- h. No Impact. The project does not present conditions that are subject to wildland fires. There is no potential to expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands. Therefore, no impacts are potential or expected (ABSP 2005).

Mitigation Measures from the City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

HM-1: Pursuant to City of Citrus Heights General Plan EIR Mitigation Measure 4.15-3a, no projects shall be approved where there is substantial evidence of existing contamination on a Cortese-listed site that would pose an unacceptable risk to the health of construction workers.

HM-2: Pursuant to City of Citrus Heights General Plan EIR Mitigation Measure 4.15-3b, establish a process that identifies the steps to be taken prior to commencement of any site preparation activities on Cortese-listed sites. This may contain but not be limited to the following:

1. Retain a licensed professional to investigate the environmental status of the soils and/or groundwater contamination. Prepare a site plan that identifies and implements any remediation activities that are required to remove health risks to persons exposed to the site during construction activities.
2. Remove all contaminated soil, dispose of contaminated soil by a licensed contractor to a properly licensed facility, and replace contaminated soil with clean fill dirt.
3. Consult with appropriate regulatory agencies such as Department of Toxic Substances Control, Regional Water Quality Control Board, and Sacramento Department of Environmental Health to determine what actions are required by these agencies to be implemented (e.g., de-watering, groundwater monitoring, etc.).

Applicable ABSP EIR Mitigation Measures Incorporated

HM-3: Mitigation Measure HM-1A Handling of Asbestos Material: Control devices and fugitive emissions monitoring are required during demolition activities which will disturb, or have the possibility of disturbing, the asbestos-containing materials. All asbestos containing building material within the buildings planned for demolition should be removed prior to any demolition activity that could break up, dislodge, or similarly disturb these materials. This removal must be done using appropriate engineering controls, in compliance with all regulations, and be a contractor certified by the Contractor's State License Board and registered by the California Division of Occupational Safety and Health (Northwest Envirocon, Incorporated 1997).

HM-3B: Disposal Of The Yellow Thermoplastic Traffic Stripes: Disposal of the yellow thermoplastic traffic stripes will be at a Class 1 disposal facility. All aspects of the project associated with removal, storage, transportation, and disposal of the yellow thermoplastic traffic striping, should be in strict accordance with the appropriate regulations.

Additional Project Level Mitigation Measures

HM-4: Any leaking transformers observed during the course of the project should be considered a potential polychlorinated biphenyl (PCB) hazard. Should leaks from electrical transformers (that will either remain within the construction limits or will require removal and/or relocation) be encountered during construction, the transformer fluid should be sampled and analyzed by qualified personnel for detectable levels of PCB's. Should PCBs be detected, the transformer should be removed and disposed of in accordance with Title 22, Division 4.5 of the California Code of Regulations and any other appropriate regulatory agency. Any stained soil encountered below electrical transformers with detectable levels of PCB's should also be handled and disposed of in accordance with Title 22, Division 4.5 of the California Code of Regulations and any other appropriate regulatory agency.

HM-5: Based on preliminary plans, right-of-way acquisition may be required at the Towne Mart gas station at Sandalwood Drive and the abandoned gas station at Oak Grove Avenue. Should final plans indicate that a portion of this parcel will be acquired for new right-of-way, a preliminary environmental

screening (limited subsurface sampling and laboratory analysis) should be performed during the PS&E for potentially elevated levels of petroleum hydrocarbons and MTBE contamination within the limits of proposed construction, and/or right-of way acquisition, adjacent to the existing gas stations. Should the preliminary screening encounter elevated levels of petroleum hydrocarbons and/or MTBE a limited Phase II ISA should be performed. The Phase II ISA should consist of subsurface sampling and laboratory analysis and be of sufficient quantity to define the extent and concentration of contamination within the areal extent and depths of planned construction activities adjacent to the existing gas stations. The Phase II ISA should also provide both a Health and Safety Plan for worker safety and a Work Plan for handling and disposing contaminated soil during construction.

HM-6: The potential exists for hazardous contamination from historic chemical spills at Paradise Cleaners, which is located near the intersection of Auburn Boulevard and Baird Way. At the time of this ISA, there were no documented reports of soil/groundwater contamination related to chemical discharge from Paradise Cleaners. If a potential hazardous contamination is detected, soil samples should be gathered and tested to determine the chemical levels within the soil.

HM-7: To avoid impacts from pavement striping during construction it is recommended that removal requirements for yellow striping and pavement marking materials be performed in accordance with Caltrans Standard Special Provision 14-11.07 REMOVE YELLOW TRAFFIC STRIPE AND PAVEMENT MARKING WITH HAZARDOUS WASTE RESIDUE.

HM-8: As is the case for any project that proposes excavation, the potential exists for unknown hazardous contamination to be revealed during project construction (such as previously undetected petroleum hydrocarbon contamination from nearby gas stations). Should any previously unknown hazardous waste/material be encountered during construction, the procedures outlined in Caltrans Hazards Procedures for Construction shall be followed.

HM-9: If the project area changes (due to a change in the project design or staging area), further investigation for potential hazardous waste generators would be required to determine their impact to the revised project limits.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
VIV. HYDROLOGY AND WATER QUALITY - Would the project:					
a. Violate any water quality standards or waste discharge requirements?			X		
b. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?		X			
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?		X			
d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?		X			
e. Create or contribute runoff water which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff?		X			
f. Otherwise substantially degrade water quality?		X			
g. Place housing within a 100-year flood hazard area as mapped on a federal				X	

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
VIV. HYDROLOGY AND WATER QUALITY - Would the project:					
Flood Hazard Boundary or Flood Insurance Rate Map or other food hazard delineation map?					
h. Place structures within a 100-year flood hazard area which would impede or redirect flood flows?				X	
i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?		X			
j. Inundation by seiche, tsunami, or mudflow?		X			

Background

For the purposes of this document, the Auburn Boulevard Specific Plan EIR (2005) was referenced for this section.

The project area is located within the American River watershed hydrologic unit which feeds into Sacramento County's River Flood Control System. There are no streams or waters within the project area. The closest stream is Cripple Creek which is located 180 feet south of the project area, and flows in a westerly direction to Arcade Creek, which in turn drains to the Natomas Main Drainage Canal and ultimately into the Sacramento River (City of Citrus Heights, 2000). Drainage facilities are located at several locations along Auburn Boulevard.

Cripple Creek maintains a perennial flow south of the project area, though a significant source of this flow is urban runoff. The project is not located within one of California's four sole source aquifers. The project is located in Sacramento County, and extends into Placer County, which does not have a sole source aquifer.

Standards of Significance

For purposes of this Initial Study, impacts to hydrology and water quality may be considered significant if construction and/or implementation of the Project would result in the following impacts that remain significant after implementation of General Plan policies or mitigation from the General Plan EIR:

- Substantially degrade water quality and violate any water quality objectives set by the State Water Resources Control Board, due to increases in sediments and other contaminants generated by construction and/or development of the Specific Plan, or
- Substantially increase the exposure of people and/or property to the risk of injury and damage in the event of a 100-year flood.

Summary of Analysis under the City of Citrus Heights General Plan EIR, and the City of Roseville General Plan

City of Citrus Heights

Chapter 4.5 of the General Plan EIR evaluates the potential effects of the General Plan as they relate to surface water, groundwater, flooding, stormwater and water quality. Potential effects include water quality degradation due to construction activities and exposure of people to flood risks (Impacts 4.5-2 through 4.5-6). Policies included in the General Plan, including a directive for regional cooperation, comprehensive flood management, and construction of adequate drainage facilities with new development (Policies 34.2-49.8) were identified that reduced all impacts to a less-than-significant level.

City of Roseville

Chapters 5 and 8 of the General Plan evaluate potential effects as they relate to surface water, groundwater, flooding, stormwater and water quality. Potential effects include water quality degradation due to construction activities and exposure of people to flood risks. Policies included in the General Plan, including a directive for regional cooperation, comprehensive flood management, and construction of adequate drainage facilities with new development (Chapter 5, Implementation Measure 10, and Chapter 8, Implementation Measures 1 through 13). The City of Roseville regulates its floodplain areas through land use, zoning, and other development restrictions. This includes policies requiring the dedication of—and a prohibition on development within—the City’s Regulatory Floodplain. Certain exceptions to this policy exist primarily within the infill area and for the construction of essential services. Where encroachments may be permitted, improvements are required to be designed to minimize cumulative upstream and downstream effects.

Discussion

- a. Less than significant with mitigation incorporated. Construction would consist of activities on landscaped and hardscape environments. Construction activities would not substantially degrade water quality and would not violate any water quality objectives by the State Water Resources Control Board. Drainage facilities are located at several locations along Auburn Boulevard BMPs will be put in place to prevent sediment and other contaminants generated by construction from impacting the drainages. Mitigation measures H-1 through H-4 will be incorporated to further reduce the risk of violating any water quality or waste discharge standards.
- b. No Impact. As addressed within the 2005 ABSP FEIR, no groundwater wells would be drilled as part of the project. The project would not deplete groundwater supplies or interfere substantially with groundwater recharge that would result in a net deficit in aquifer volume or lowering of the local groundwater table level.
- c-d. No Impact. As addressed within the 2005 ABSP FEIR, the project would not substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site
- e. Less-than-Significant. As addressed within the 2005 ABSP FEIR, there will be an increase in impervious surface due to the project. This will result in more runoff water, however the project is including drainage improvements. The existing drainage facilities within the project have adequate capacity for additional runoff.
- f. No impact. As addressed within the 2005 ABSP FEIR, the project would not otherwise substantially degrade water quality.

- g-h. Less than significant. Cripple Creek has a relatively small hydrologic capacity and can be quickly overwhelmed during severe storm runoff events. In these events, stream channel banks can overflow and result in temporary inundation of adjoining low lying areas. Within the project area, the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM) shows the area immediately adjacent to Cripple Creek as being within the 100-year floodplain, Zone AE which represents areas with a 1% annual chance of flooding. The project includes construction of a raised median on Auburn Boulevard starting approximately 300 feet south of Cripple Creek Drive. The raised median encroaches approximately 50 feet on to the existing floodplain Zone AE for Cripple Creek. The rest of the project area is within Zone X, which represents areas of 0.2% annual chance flood; areas of 1% annual change flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood (Appendix D). A Summary Floodplain Encroachment Report was submitted by Dokken Engineering per Caltrans requirements on June 25, 2015 to examine if the project would increase the exposure of people and/or property to the risk of injury and damage in the event of a 100-year flood. While the proposed median would encroach 50 feet on to the existing floodplain Zone AE, the project does not include new housing or development within the floodplain and would only make improvements to the existing road which is consistent with the City of Citrus Heights and the City of Roseville General Plan. Therefore, the project would not substantially increase the exposure of people and/or property to the risk of injury and damage in the event of a 100-year flood.
- i. No Impact. As addressed within the 2005 ABSP FEIR, the project would not expose people or structures to a significant risk of loss, injury or death involving flooding as a result of the failure of levee or dam.
- j. Less than significant. As addressed within the 2005 ABSP FEIR, while the median would encroach 50 feet on to the existing floodplain Zone AE, the project does not include new housing or development within the floodplain and would only make improvements to the existing road which is consistent with the City of Citrus Heights and the City of Roseville General Plan. Therefore, the project would not substantially increase the exposure of people and/or property to flooding risks or inundation by seiche, tsunami or mudflows.

Mitigation Measures from the City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

H-1: Incorporate Development Standards for Improving Water Quality: The City shall incorporate water quality protection measures into the specific plan Development Standards: The standards may include but are not limited to the following:

1. Install and maintain landscaping that requires minimal application of chemical fertilizers, pesticides and herbicides;
2. Emphasize xeriscape landscaping that reduces the need for irrigation by minimizing the use of turf in decorative landscaping, using plant materials adapted to local conditions and efficient irrigation;
3. Minimize irrigation overspray - do not permit use of sprinkler and spray irrigation in areas less than 8 feet wide;
4. Use of drip irrigation systems where feasible;
5. Incorporate features such as filtration strips or bioswales in site design to prevent urban pollutants from entering into Cripple Creek via storm drains from parking lots and paved surfaces.

H-2: Implement Best Management Practices (BMPs). The City shall require implementation of best management practices for public and private development. Such practices may include, but are not limited to:

1. Regular inspection, maintenance and cleaning out of stormwater retention or detention structures;
2. Regular inspection, maintenance and cleaning out of oil and water separators;
3. Encourage property owners to regularly remove trash, dead vegetation and leaf litter;
4. Encourage use of landscaping and horticultural practices that minimize the need for chemical fertilizers, herbicides and pesticides.

Additional Project Level Mitigation Measures

H-3: The Project would require a NPDES General Construction Permit for Discharges of storm water associated with construction activities (Construction General Permit 2012-0006-DWQ). A SWPPP would also be developed and implemented as part of the Construction General Permit.

H-4: The construction contractor shall adhere to the SWRCB Order No. 2012-0006-DWQ NPDES Permit pursuant to Section 402 of the CWA. This permit authorizes storm water and authorized non-storm water discharges from construction activities. As part of this Permit requirement, a SWPPP shall be prepared prior to construction consistent with the requirements of the RWQCB. This SWPPP will incorporate all applicable BMPs to ensure that adequate measures are taken during construction to minimize impacts to water quality.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
IX. LAND USE AND PLANNING - Would the project:					
a. Physically divide an established community?		X			
b. Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?		X			

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
IX. LAND USE AND PLANNING - Would the project:					
c. Conflict with any applicable habitat conservation plan or natural community conservation plan?		X			

Discussion

- a. No Impact. As addressed within the 2005 ABSP FEIR, the project will take place on and adjacent to an existing facility. The project would not physically divide an established community (ABSP 2005).
- b. No Impact. As addressed within the 2005 ABSP FEIR, the project will not affect the roadway designation. The project would not change the zoning ordinance within the project area (ABSP 2005).
- c. No Impact. As addressed within the 2005 ABSP FEIR, the project will not conflict with any existing habitat conservation plan or natural community's conservation plan. No such plans currently exist within the project area (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

None.

Additional Project-Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Not applicable.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
X. MINERAL RESOURCES - Would the project:					
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?		X			
b. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?		X			

Discussion

a-b. No Impact. As addressed within the 2005 ABSP FEIR, the project site has not been identified as containing mineral resources that would be considered a significant resource. No active mining operations are present in or near the project area (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

None.

Additional Project-Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Not applicable.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XI. NOISE - Would the project result in:					
a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?			X		
b. Exposure of persons to or generation of excessive ground borne vibration or ground borne noise levels?				X	
c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?				X	
d. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above level existing without the project?			X		
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project to excessive noise levels?					X
f. For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?					X

Setting

In accordance with the Caltrans Environmental Handbook guidelines, noise is defined as unwanted sound. Sound levels usually are measured and expressed in decibels (dB), with 0 dB being the threshold of hearing. Decibel levels range from 0 to 140: 50 dB for light traffic is considered a low decibel level, whereas 120 dB for a jet takeoff at 200 feet is considered a high decibel level.

Under the Caltrans Traffic Noise Analysis Protocol (CaTNAP) 2011, projects that are not Type 1 only require an evaluation of predicted construction noise. The project is not a Type 1 project as defined in 23 CFR 772.5;

- (1) The construction of a highway on new location; or,
- (2) The physical alteration of an existing highway where there is either:

(i) Substantial Horizontal Alteration. A project that halves the distance between the traffic noise source and the closest receptor between the existing condition to the future build condition; or,
(ii) Substantial Vertical Alteration. A project that removes shielding therefore exposing the line-of-sight between the receptor and the traffic noise source. This is done by either altering the vertical alignment of the highway or by altering the topography between the highway traffic noise source and the receptor; or,

- (3) The addition of a through-traffic lane(s). This includes the addition of a through-traffic lane that functions as a HOV lane, High-Occupancy Toll (HOT) lane, bus lane, or truck climbing lane; or,
- (4) The addition of an auxiliary lane, except for when the auxiliary lane is a turn lane; or,
- (5) The addition or relocation of interchange lanes or ramps added to a quadrant to complete an existing partial interchange; or,
- (6) Restriping existing pavement for the purpose of adding a through-traffic lane or an auxiliary lane; or,
- (7) The addition of a new or substantial alteration of a weigh station, rest stop, ride-share lot or toll plaza.
- (8) If a project is determined to be a Type I project under this definition then the entire project area as defined in the environmental document is a Type I project.

The project is a reconstruction project of an existing road and is not a new highway. The project would not significantly change the horizontal or vertical alignment of the road, would not add through-traffic lanes or an auxiliary lane, would not add or relocate interchange lanes, would not restripe existing pavement for the purpose of adding through-traffic lanes, and would not add or alter a weigh station/rest stop/ride-share lot/toll plaza. Therefore, the project is not a Type 1 project and only construction noise impacts are discussed.

The project is within the City of Citrus Heights's Boulevard Plan Planning Area and the City of Roseville's Planning Area. The project area within the City of Citrus Heights is located in areas designated for General Commercial, Low Density Residential, and Medium Density Residential land uses in the Boulevard Plan (City of Citrus Heights 2005a). The area is zoned for Auburn Boulevard Commercial (ABC) Zoning District and Commercial and Village Centers (City of Citrus Heights 2005b). The project site located within the City of Roseville is located in areas designated for Community Commercial land uses and is zoned for Neighborhood Commercial, General Commercial and Community Commercial (City of Roseville, 2009).

Noise sources that contribute to ambient noise levels in and adjacent to the project site include traffic from local streets. As a way to characterize noise levels, Table 4 summarizes typical ambient noise levels based on population density.

Table 4. Population Density and Associated Ambient Noise Levels

Population Density	dBA, Ldn
Rural Suburban	40–50
Quiet suburban residential or small town	45–50
Normal suburban residential urban	50–55
Normal urban residential	60
Noisy urban residential	65
Very noisy urban residential	70
Downtown, major metropolis	75–80
Under flight path at major airport, 0.5 to 1 mile from runway	78–85
Adjoining freeway or near a major airport	80–90
Sources: Cowan 1984, Hoover and Keith 1996	

The vicinity of the project area is most similar to that of “noisy urban residential,” and “normal urban residential.” Noisy Urban Residential areas have a typical noise level of 65 dBA while Normal Urban Residential has a typical noise level of 60 dBA. The Technical Noise Supplement (Caltrans, 2009) defines a noise receiver or receptor as “any natural or artificial sensor that can perceive, register or be affected by sound, such as a human ear, or a microphone.”

In general, noise sensitive land-uses include residences, schools, hospitals, churches, and parks. The project would take place near General Commercial, Low Density Residential, and Medium Density Residential land uses (City of Citrus Heights 2005a). Open Space, which includes a community park, is south of the project footprint; however, no impacts to this noise sensitive land-use is anticipate. Additionally, no industrial development is in the immediate vicinity of the project site. Located along the project alignment are residences along with a variety of businesses, including a bowling alley, auto shops, small restaurants, retailers and convenience stores.

Standards of Significance

For purposes of this Initial Study, impacts due to noise may be considered significant if construction and/or implementation of the Project would result in the following impacts that remain significant after implementation of General Plan policies or mitigation from the General Plan EIR:

- Result in exterior noise levels in the project area that are above the upper value of the normally acceptable category for various land uses due to the project’s noise level increases;
- Result in residential interior noise levels of 45 dBA L_{dn} or greater caused by noise level increases due to the project;
- Result in construction noise levels that exceed the standards in the City of Citrus Heights or the City of Roseville’s Noise Ordinance;
- Permit existing and/or planned residential and commercial areas to be exposed to vibration-peak-particle velocities greater than 0.5 inches per second due to project construction;
- Permit adjacent residential and commercial areas to be exposed to vibration peak particle velocities greater than 0.5 inches per second due to highway traffic and rail operations; or
- Permit historic buildings and archaeological sites to be exposed to vibration-peak-particle velocities greater than 0.2 inches per second due to project construction and highway traffic.

Summary of Analysis under the City of Citrus Heights General Plan EIR and the City of Roseville General Plan

City of Citrus Heights

The General Plan EIR evaluated the potential for development under the General Plan to increase noise levels in the community. New noise sources include vehicular traffic, railways, and stationary sources. The general plan policies establish exterior and interior noise standards (Policy 51.1). A variety of policies provide standards for the types of development envisioned in the general plan. See Policy 52.2 through 52.5, which requires new mixed-use, commercial and industrial development to mitigate the effects of noise from operations on adjoining sensitive land use, and Policy 52.4, which calls for the City to limit hours of operations for parks and active recreation areas to minimize disturbance to nearby residences. Notwithstanding application of the general plan policies, noise impacts for exterior and interior noise levels, and vibration impacts, were found to be significant and unavoidable.

City of Roseville

The General Plan evaluated potential for development to increase noise levels within the community. New noise sources include vehicular traffic, vehicular traffic on highways, railways, and stationary sources. The general plan policies establish exterior and interior noise standards (Table IX-1 through Table IX-3). A

variety of policies provide standards for the types of development envisioned in the general plan. See Policies 1 through 10, which require new mixed-use, commercial and industrial development to mitigate the effects of noise from operations on adjoining sensitive land use, and call for the City to limit hours of operations for parks and active recreation areas to minimize disturbance to nearby residences.

Discussion

- a. Less-than-significant with mitigation incorporated. During construction of the project, noise from construction activities may intermittently dominate the noise environment in the immediate area of construction. Construction noise is regulated by Caltrans Standard Specifications Section 14-8.02, "Noise Control," which states that noise levels generated during construction shall comply with applicable local, state, and federal regulations, and that all equipment shall be fitted with adequate mufflers according to the manufacturers' specifications.

Generally, noise levels at construction sites can vary from 55 dBA to a maximum of nearly 96 dBA when heavy equipment is used. Construction noise of this project would be intermittent, and noise levels would vary depending on the type of construction activity. For this project, lowest construction equipment-related noise levels would be 55 dBA at a distance of 50 ft for sound from a pick-up truck. Highest noise levels would be up to 90 dBA (at a distance of 50 ft) for a concrete saw for pavement removal. A jackhammer, which would be up to 89 dBA at a distance of 50 ft, would also be utilized during the project. Rock crushing, during construction of the project, may also be necessary.

No adverse noise impacts from construction are anticipated because construction would be conducted in accordance with Caltrans Standard Specifications Section 14-8.02 and applicable local noise standards. Construction noise would be short-term and intermittent. Construction is expected to take approximately 180 days. Further, implementing the measures below would minimize the temporary noise impacts from construction.

The project is exempt from City of Citrus Heights code pertaining to noise regulation due to the Section 34-88 which states that activities involving "Noise sources associated with construction, repair, remodeling, demolition, paving or grading of any real property, provided the activities do not take place between the hours of 8:00 p.m. and 6:00 a.m. on weekdays and Friday commencing at 8:00 p.m. through and including 7:00 a.m. on Saturday, Saturdays commencing at 8:00 p.m. through and including 7:00 a.m. on the next following Sunday, and on each Sunday after the hour of 8:00 p.m. However, when an unforeseen or unavoidable condition occurs during a construction project and the nature of the project necessitates that work in process be continued until a specific phase is completed, the contractor or owner shall be allowed to continue work after 8:00 p.m. and to operate machinery and equipment necessary until completion of the specific work in progress can be brought to conclusion under conditions which will not jeopardize inspection acceptance or create undue financial hardships for the contractor or owner" are exempt from the provisions of the city code. In addition, the City of Roseville's Municipal Code specifically prohibits the overnight operation of certain noisy, construction-related equipment (i.e. between the hours of 10:00 p.m. and 7:00 a.m.). These specified equipment items include any pneumatic or air hammer, pile driver, steam shovel, derrick, steam or electric hoist, parking lot cleaning equipment or other appliance that creates a "loud or unusual" noise. Noise Element Policy P4 under Objective N-1.2 constrains all construction in the vicinity of noise sensitive land uses (such as residences, hospitals, or convalescent homes) to daylight hours of 7:00 a.m. to 7:00 p.m. Further, this Policy lays out 'best practices' strategies for reducing noise from construction equipment and operations.

The project would have a less than significant impact on noise based on: 1) the project is not anticipated to change traffic; 2) Proposed construction duration is temporary; and 3) construction of the project would use proposed minimization methods. No adverse noise impacts from construction

are anticipated because construction would be conducted in accordance with Caltrans' standard specifications and would be short term and intermittent. Mitigation measures N-1 through N-4 will be implemented to further reduce any noise specific impacts.

- b. Less than significant. The project site is level, and does not include buildings or structures that would require unusual construction techniques that would cause substantial vibration. The project would not result in additional significant environmental effects. Substantial levels of vibration are not anticipated because traffic volumes will be similar to the existing situation.

The project would generate some vibration due to construction activities, but it would not include construction activities that could generate significant ground vibration, such as pile driving. There are no historic structures within the project area that would be affected by construction-related vibration, therefore this impact would not exceed the impact disclosed in the City of Citrus Heights General Plan EIR or the City of Roseville General Plan.

- c. Less than significant. The project activities would not cause permanent increases in ambient noise levels in the project vicinity.
- d. Less-than-Significant Impact with Mitigation Incorporated. During construction activities, there would be a temporary noise increase from use of power tools, equipment, and other non-powered hand-tools. The City would require the contractor to comply with all applicable noise and occupational safety standards as defined in the construction specifications, and to protect workers and other persons from health effects of increased noise levels from the use of construction equipment. Compliance with construction specifications would reduce potential noise-related concerns at the construction site, and therefore cause a less-than-significant impact. Mitigation measures N-1 through N-4 are presented in response a. and would reduce the noise impacts to less-than-significant.
- e-f. No Impact. The project site is not located near an existing airport and is not within an area covered by an existing airport land use plan. Therefore, no impact would occur.

Mitigation Measures from the City of Citrus Heights and the City of Roseville General Plan EIR that apply to the Project

N-1: Pursuant to City of Citrus Heights General Plan EIR Mitigation Measure 4.6-1:

1. Limit hours of construction to account for more sensitive weekend hours.
2. Limit hours of construction where noise is audible at sensitive land uses beyond the boundaries of the construction site.

N-2: Pursuant to City of Roseville General Plan, Chapter 9, Mitigation Measure 8:

The City shall use the Noise Level Performance Standards contained in Table IX-3 for reviewing new development of noise-sensitive uses exposed to fixed noise sources. These standards are also to be used for evaluating potential impacts of proposed new fixed noise sources upon nearby noise-sensitive uses. Where a noise-sensitive land use is proposed near a fixed noise source, such as an industrial facility, noise measurements will be performed to determine whether existing and/or future noise levels due to that source will exceed the standards of Table IX-3 within the property line of the proposed use. Similarly, where a fixed noise-producing use such as an industrial facility is proposed near an existing or future noise-sensitive use, a noise analysis will be prepared to ensure that the noise produced by that use will not exceed the standards of Table IX-3 within the property line of the noise-sensitive use. (Policies 6, 7 and 8).

<p align="center">TABLE IX-3</p> <p align="center">PERFORMANCE STANDARDS</p> <p align="center">FOR NON-TRANSPORTATION NOISE SOURCES</p> <p align="center">OR PROJECTS AFFECTED BY NON-TRANSPORTATION NOISE SOURCES</p> <p align="center">(As Measured at the Property Line of Noise-Sensitive Uses)</p>		
Noise Level Descriptor	Daytime (7 a.m. to 10 p.m.)	Nighttime (10 p.m. to 7 a.m.)
Hourly L_{eq} , dB	50	45
Maximum level, dB	70	65
<p>¹ For municipal power plants consisting primarily of broadband, steady state noise sources, the hourly (L_{eq}) noise standard may be increased up to 10 dB(A), but not exceed 55 dB(A) Hourly L_{eq} dB.</p> <p>Each of the noise levels specified above should be lowered by five dB for simple tone noises, noises consisting primarily of speech or music, or for recurring impulsive noises. Such noises are generally considered by residents to be particularly annoying and are a primary source of noise complaints. These noise level standards do not apply to residential units established in conjunction with industrial or commercial uses (e.g., caretaker dwellings).</p> <p>No standards have been included for interior noise levels. Standard construction practices should, with exterior noise levels identified, result in acceptable interior noise levels.</p>		

Applicable ABSP EIR Mitigation Measures Incorporated

- N-3:** On-site Noise Control: To ensure mitigation of noise due to project-related loading docks and on-site traffic, development proposals should be reviewed to identify potential noise conflicts with existing or proposed noise sensitive uses. Implementation of the noise standards contained in the Noise Element of the Citrus Heights General Plan will mitigate project-related noise to an insignificant level. For development requiring installation of large groundmounted HVAC systems, development review should include an assessment of noise impacts on nearby residential areas.

Additional Project-Level Mitigation Measures

- N-4:** The Contractor shall follow City of Citrus Heights and City of Roseville noise ordinances for construction activities:
- Do not exceed 65 dBA at 50 feet from the job site activities from 8 p.m. to 7 a.m.
 - Use an alternative waiting method instead of a sound signal unless required by safety laws.
 - Equip an internal combustion engine with the manufacturer-recommended muffler.
 - Do not operate an internal combustion engine on the job site without the appropriate muffler.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XII. POPULATION AND HOUSING - Would the project:					
a. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?		X			
b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?		X			
c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?		X			

Discussion

- a-c. No Impact. As addressed within the 2005 ABSP FEIR, the project will not affect population and housing. The project is not capacity enhancing; therefore, is not growth inducing. Also, the project would not displace housing or necessitate construction of replacement housing (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

PH-1: Prior to approving a development project that would result in conversion of trailer parks to other uses; the City shall comply with Government Code Section 65863.7, a copy of which is included in Appendix J of the ABSP EIR.

PH 1-B: Relocation Assistance for Housing Displacement:

1. The City shall provide standard relocation assistance to both tenants and owner occupants in compliance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Replacement housing must be decent, safe, and sanitary (DS&S), which means it must meet all of the minimum requirements established by Federal regulations and conforms to applicable housing and occupancy codes.
2. All real property transactions shall comply with the property acquisition and relocation standards of the State of California, the Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.

PH-1C: Business Relocation. The following mitigation measures shall be required to compensate for right-of way acquisition:

1. Property owners shall be compensated in accordance with fair market values based on appraisals. Business owners shall be compensated based on an assessment of the value of the business and any loss of good will.
2. All efforts shall be made to identify relocation opportunities for affected businesses that would reduce the loss of goodwill and historic patronage. Wherever feasible, assistance shall be made available in identifying suitable relocation sites within the service area of existing businesses.

PH-1D: Property Compensation:

1. All real property transactions shall comply with the property acquisition and relocation standards of the State of California, the Caltrans Relocation Assistance Program and the Federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.
2. Property owners shall be compensated in accordance with fair market values based on appraisals. Business owners shall be compensated based on an assessment of the value of the business and any loss of good will.
3. All efforts shall be made to identify relocation opportunities for affected businesses that would reduce the loss of goodwill and historic patronage. Wherever feasible, assistance shall be made available in identifying suitable relocation sites within the service area of existing businesses.

Additional Project Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XIII. PUBLIC SERVICES					
a. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:		X			
Fire protection?		X			

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XIII. PUBLIC SERVICES					
Police protection?		X			
Schools?		X			
Parks?		X			
Other public facilities?		X			

Discussion

- a. No Impact. As addressed within the 2005 ABSP FEIR, the project will not adversely affect public services in the area. The project would not contribute to any change in population, traffic circulation, or other land use modifications that would impact local fire or police protection. There will be no road closures during construction. The project improvements will not have an effect on parks or other services. As a result of the project utility coordination has taken place and will continue throughout construction with the following utility companies: Pacific Gas and Electric (PG&E), City of Citrus Heights Water District, City of Roseville, Sacramento Area Sewer District, Sacramento Municipal Utility District (SMUD), Consolidated Communications, and Comcast. No mitigation will be required (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

None.

Additional Project-Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Not applicable.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XIV. RECREATION					
a. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?		X			
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?		X			

Discussion

- a-b. No Impact. As addressed within the 2005 ABSP FEIR, the project would not affect recreation or recreation facilities in the area. As the project consists solely of roadway improvements, the project does not propose new residential or commercial developments creating a need for construction or expansion of recreational facilities beyond what was anticipated in the City of Citrus Heights and City of Roseville General Plans therefore, the project would not result in additional significant impacts on recreation that were not addressed or considered in the City of Citrus Heights General Plan EIR or City of Roseville General Plan (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

None.

Additional Project-Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Not applicable.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XV. TRANSPORTATION/TRAFFIC - Would the project:					
a. Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?		X			
b. Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?		X			
c. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?		X			
d. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?		X			
e. Result in inadequate emergency access?		X			
f. Result in inadequate parking capacity?		X			
g. Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?		X			

Discussion

- a-b. Less-than-Significant Impact. As addressed within the 2005 ABSP FEIR, while construction of the project would generate short-term impacts through Auburn Boulevard, construction activities would be temporary, intermittent, and have a minimal impact on surrounding traffic flows, therefore short-term construction impacts are considered less than significant. Auburn Boulevard provides direct access to I-80, however the project would not adversely affect the operations of any freeway facility and will remain open during construction to maintain access to local businesses. Prior to construction signage and other construction related information for continuing traffic operations will be established through the project site (ABSP 2005).

- c. No Impact. As addressed within the 2005 ABSP FEIR, the project does not require any changes to existing regional air traffic activity, and the project site is not located near an airport. Therefore, no impact would occur.
- d. No Impact. As addressed within the 2005 ABSP FEIR, the design features associated with the project will not increase hazards. All project features will meet safety standards (ABSP 2005).
- e. Less than significant with mitigation incorporated. Per the Auburn Boulevard Specific Plan EIR, construction activities would result in temporary disruption to businesses and residents in the area as well as to some public services such as public transit and emergency vehicles (i.e. fire and police). Detours and traffic delays may occur; however interruptions to access would be minimized for all properties. These impacts would be temporary. Mitigation Measure T-1 from the ABSP EIR would reduce this impact to a less than significant level (ABSP 2005).
- f. Less than significant with mitigation incorporated. The project would require acquisition of frontage properties on Auburn Boulevard to provide for widening and construction of improvements. Property acquisition would impact parking areas within several businesses along Auburn Boulevard. Property owners must be compensated for the loss of the underlying property. Additional mitigation measures would be needed if the parking loss affects the viability of the business operations. Mitigation measure T-2 from the ABSP EIR would additional reduce this impact to a less than significant level (ABSP 2005).
- g. No Impact. As addressed within the 2005 ABSP FEIR, there are no conflicts with adopted policies, plans, or programs supporting alternative transportation (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

T-1: Ensure Adequate Parking Supply. In order to ensure that adequate parking supply is maintained in the specific plan area, the city shall establish a special permit process to allow flexibility in the number of required parking spaces when deemed appropriate.

T-1B: Compensate for Parking Impacts:

1.The determination of project-caused parking impacts shall be made in accordance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Project proponents shall compensate for acquisition of underlying property in compliance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.

2. In the event that it is determined that the project will result in a reduction of parking spaces below the number required by zoning, but would not preclude continued use of the parcel that is allowed by zoning as determined by the appropriate land use authority (City of Citrus Heights), the project proponent(s) shall:

- (a) Investigate feasibility of and compensate for cost of reconfiguring parking area or relocating parking on the same parcel to provide additional spaces; or
- (b) Investigate feasibility of and compensate for the cost of providing off-site parking; and/or

(c) The appropriate land use authority (City of Citrus Heights) shall grant a special parking permit to allow the continued use with reduced parking.

3. If it is determined during the right-of-way appraisal and acquisition process that the project would result in a reduction of parking spaces that would preclude continued use of the parcel in accordance with the existing zoning, and none of the measures under No. 2 above are feasible, the project proponent(s) shall provide compensation in accordance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.

T-6: Construction Period Traffic Management Plan. A traffic handling plan will be prepared prior to construction of any roadway improvements. The plan will address traffic management during construction periods, including but not limited to road and lane closures; detours; pedestrian and bicycle routes; and public notification. The traffic handling plan should be prepared in consultation with regional transit in order to minimize disruptions to public transit service along the corridor. Additionally, prior to commencement of construction, a Traffic Management Plan as described in A Traffic Management Plan Guide (See Appendix A of the Final EIR) will be prepared and provided to Caltrans for review in order to address strategies needed to minimize disruption of traffic at the Interstate 80/Auburn Boulevard Interchange.

Additional Project Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XVI. UTILITIES AND SERVICE SYSTEMS - Would the project:					
a. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?		X			
b. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?		X			

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XVI. UTILITIES AND SERVICE SYSTEMS - Would the project:					
c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?		X			
d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?		X			
e. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?		X			
f. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?		X			
g. Comply with federal, state, and local statutes and regulations related to solid waste?		X			

Discussion

- a. No Impact. As addressed within the 2005 ABSP FEIR, the project would not involve wastewater treatment requirements (ABSP 2005).
- b. No Impact. As addressed within the 2005 ABSP FEIR, the project would not require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities (ABSP 2005).
- c. Less-than-Significant Impact. As addressed within the 2005 ABSP FEIR, the project would not require or result in the construction of new storm water drainage facilities or expansion of existing facilities. The facility for this area has enough capacity to contain any additional runoff associated with the project during construction.
- d. No Impact. The project would not increase water supply demand (ABSP 2005).
- e. No Impact. The project would not affect wastewater treatment (ABSP 2005).

- f. No Impact. The project would not increase solid waste disposal needs (ABSP 2005).
- g. No Impact. The project would comply with federal, state, and local statutes and regulations related to solid waste (ABSP 2005).

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

U-1: Construction Management for Utilities: The construction project management team shall coordinate with utility providers during design stages of roadway projects. The construction project management team shall undertake periodic assessments of upcoming utility and service disruptions during construction. These assessments and an identification of the service area involved shall be coordinated with utility providers and the public outreach program. The public outreach program shall ensure that advance notice of any utility or service shutdowns is extended to affected businesses and residents. Through construction management and project scheduling, all available measures shall be taken to minimize the duration of utility or service shutdowns.

Additional Project Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XVII. MANDATORY FINDINGS OF SIGNIFICANCE					
a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California?			X		

	Potentially Significant Impact	Project Impact Adequately Addressed in Previous Document	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
XVII. MANDATORY FINDINGS OF SIGNIFICANCE					
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of).		X			
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?			X		

Discussion

- a. Less-than-Significant with Mitigation Incorporated. As discussed in this study, the project could result in impacts on biological resources. However, there is no potential for Federal or State threatened or endangered species to occur within the project area and no designated Critical Habitat occurs within the project vicinity. Construction of the project would also result in temporary construction noise impacts. Mitigation measures included in this study would reduce the impacts to less-than-significant levels.
- b. No Impact. As addressed within the 2005 ABSP FEIR, the project does not directly or indirectly contribute to cumulative impacts associated with increased urban development because the impacts of public parking, and traffic, have previously been evaluated by the City and considered in development of the City's General Plan (ABSP 2005).
- c. Less-than-Significant with Mitigation Incorporated. As discussed in this study, the project could result in impacts on human beings indirectly due to noise impacts. Mitigation measures included in this study would reduce impacts to less-than-significant levels.

Mitigation Measures from City of Citrus Heights General Plan EIR and the City of Roseville General Plan that apply to the Project

None.

Applicable ABSP EIR Mitigation Measures Incorporated

None applicable.

Additional Project Level Mitigation Measures

None required.

Significance Determination with Mitigation Measures

Less than significant with mitigation incorporated.

SECTION 5.0 - COMMENTS AND COORDINATION

This section summarizes the City of Citrus Heights' efforts to identify, address and resolve Project-related issues through early and continuing coordination.

Scoping Process

The approved Auburn Boulevard Specific Plan Final Environmental Impact Report document (2005) provided basis for determining potential environmental constraints within the Auburn Boulevard Complete Streets, Phase 2 project area.

Consultation and Coordination with Public Agencies

Coordination with the following agencies was initiated for the Auburn Boulevard Complete Streets, Phase 2 Project:

- City of Roseville,
- Regional Water Quality Control Board (RWQCB), and
- Native American Heritage Commission (NAHC).

Public Participation

The public comment period for the Project provides the opportunity for public comment and participation. The comment period for the project occurred October 9th, 2015, through November 8th, 2015. All public comments received are included within Appendix F of this document. Table 5 displays coordination that has occurred with developers and property owners.

Table 5. Discussions with City of Roseville

Name of Attendees	Title/Jurisdiction	Summary of Discussions
Regina Cave	Management Analyst City of Citrus Heights	<ul style="list-style-type: none">• Whyte Ave. has a proposal for a ped/bike overcrossing over 80.• There is a high incident of accidents near Whyte.• There is interconnect past the freeway overcrossing. The new bus intermodal site plans will have the location. Roseville is providing these docs.• The bus project design project will be completed in the next 2 months.• Roseville would support medians & a sidewalk in their jurisdiction.• Not sure of the R/W where the AC path is on the private property. Roseville hopes the R/W has already been dedicated for the sidewalk area.• The Keep Clear area for the bus seems to be working. This should be retained for buses accessing Whyte to the east.• Include Placer County residents in the outreach and environmental. The residential area uses Auburn.
Kevin Becker	Principal Engineer City of Citrus Heights	
Jason Shykowski	Principal CE, Engineering City of Roseville	
Mike Dour	Bike Planner City of Roseville	
Jana Cervantes	Senior Engineer/Traffic Engineer City of Roseville	

SECTION 6.0 - REFERENCES

- California Air Resources Board. 2013a. Ambient Air Quality Standards.
<http://www.arb.ca.gov/research/aaqs/aaqs2.pdf>
- California Air Resources Board. 2013b. Air Quality Standards and Area Designations.
<http://www.arb.ca.gov/desig/desig.htm>
- California Air Resources Board. 2008. Climate Change Scoping Plan.
- California Air Resources Board. 2005. Air Quality and Land Use Handbook: A Community Health Perspective.
- California Department of Transportation: Division of Environmental Analysis. May 2011. Traffic Noise Analysis Protocol: For New Highway Construction, Reconstruction, and Retrofit Barrier Projects.
- City of Citrus Heights. 2005. Final Environmental Impact Report for The Boulevard Plan: A Specific Plan for the Auburn Boulevard Corridor from Sylvan Corners to the City Limits & for the Auburn Boulevard Roadway Design Improvement Project from Sylvan Corners to Cripple Creek Road.
- City of Citrus Heights, 2011: *General Plan*.
- City of Citrus Heights, 2011: *City of Citrus Heights General Plan Update and Greenhouse Gas Reduction Plan*.
- City of Roseville, 2010. *General Plan*. Available at:
http://www.roseville.ca.us/gov/development_services/planning/general_plan_n_development_guidelines.asp
(accessed 6/26/15)
- City of Roseville, 2000. *Greenhouse Gas Reduction Plan Analysis*. Available at:
<https://www.roseville.ca.us/lp/supersize/ClimateActionPlan.pdf> (accessed 6/26/15)
- Cowan, J. P. 1984. Handbook of Environmental Acoustics. New York, NY: John Wiley & Sons.
- CNDDDB. 2015. Rarefind 5. Available at: <<http://www.dfg.ca.gov/biogeodata/cnddb/mapsanddata.asp>>
(accessed 6/26/15).
- Dokken Engineering. 2015. Hazardous Waste Initial Site Assessment Auburn Boulevard Complete Streets, Phase 2. Citrus Heights, CA.
- Fehr & Peers. 2004. Traffic Impact Study for Proposed Auburn Boulevard Specific Plan. Roseville, CA.
- FEMA. 2015. National Flood Insurance Rate Map FIRMette; Sacramento County, California and Incorporated Areas Map No. 06067C0066H. National Flood Insurance Program. Available at:
<<https://msc.fema.gov/portal>> (accessed 6/26/15).
- Hoover, R.M., and R.H. Keith. 1996. Noise Control for Buildings, Manufacturing Plants, Equipment and Products.
- Sacramento Metropolitan Air Quality Management District (SMAQMD) 2004. *Guide to Air Quality Assessment in Sacramento County*.

Sacramento Metropolitan Air Quality Management District. 2003. Road Construction Emissions Model, Version 5.1. <http://www.airquality.orgiceqa/index.shtml>.

SWRCB. 2015. USAEPA 303(d) List. State Water Resources Control Board. Available at:
< http://www.waterboards.ca.gov/water_issues/programs/tmdl/integrated2010.shtml > (accessed 6/26/15)

URS, 2007. Final Subsequent Environmental Impact Report for the City of Roseville 2020 Transportation System Capital Improvements Program Update. Available at:
<http://www.roseville.ca.us/civicax/filebank/blobdload.aspx?BlobID=26286>
(accessed 7/14/15).

USFWS. 2015. National Wetlands Inventory: Wetlands Mapper. USFWS Fisheries and Habitat Conservation. Available at: <<http://www.fws.gov/wetlands/Data/Mapper.html>> (accessed 6/26/15).

USFWS. 1973. Endangered Species Act. Section 16 of the U.S. Code (1531-1544), as amended.

USFWS. 1918. Migratory Bird Treaty Act. Section 16 of U.S. Code (703-712), as amended 1989.

Wagner, D.L, Jennings, C.W., Bedrossian, T.L., and Bortugno, E.J. 1981. California Geological Survey, Regional Geologic Map No. 1A, 1:250,000 scale: Geological Map of the Sacramento Quadrangle California. California Department of Conservation. Assessed 6/26/15.
<http://www.quake.ca.gov/gmaps/RGM/sacramento/sacramento.html>.

Western Regional Climate Center. 2015. Station: (047630) Sacramento FAA Arpt—General Climate Summary Tables. <http://www.wrcc.dri.edu/cgi-bin/cliMAIN.pl?ca7630>

APPENDIX A: Representative Site Photos



Photograph 1: Representative existing Auburn Boulevard, facing south.



Photograph 2: Representative existing narrow sidewalks and road shoulder; looking north.



Photograph 3: Representative commercial and residential land use; looking west at the proposed road improvements area south of I-80.

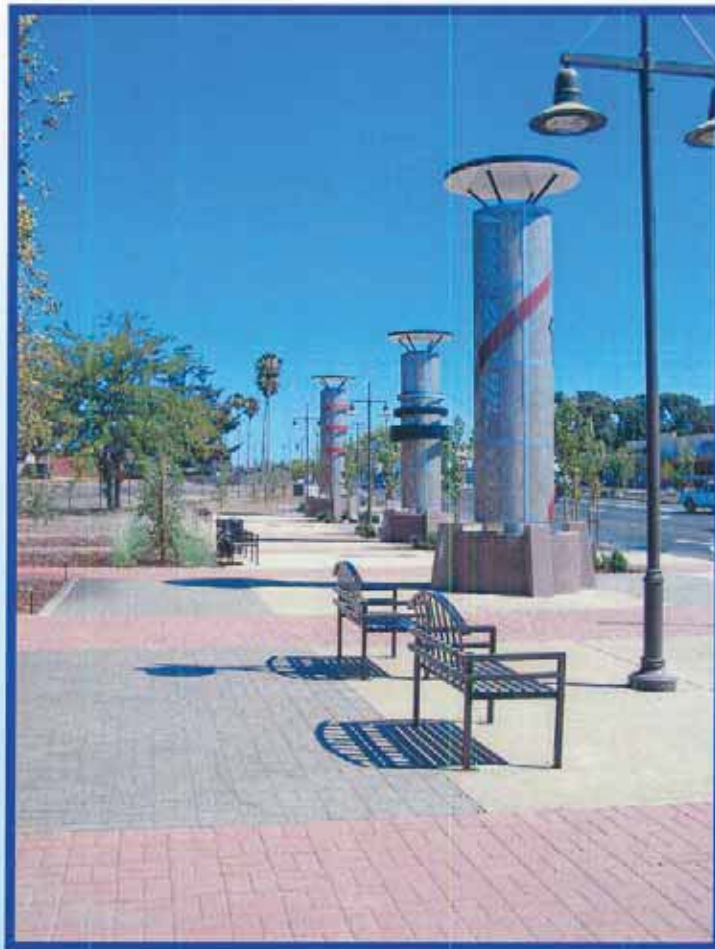


Photograph 4: Representative highly disturbed ruderal vegetation; west of Auburn Boulevard.

APPENDIX B: Auburn Boulevard Specific Plan Final Environmental Impact Report

FINAL ENVIRONMENTAL IMPACT REPORT
FOR
THE BOULEVARD PLAN

A Specific Plan
for the Auburn Boulevard Corridor from Sylvan Corners to the City Limits
&
for the Auburn Boulevard Roadway Design Improvement Project from
Sylvan Corners to Cripple Creek Road
City of Citrus Heights,
Sacramento County, California



SCH No. 2003-062165

January 2005

FINAL ENVIRONMENTAL IMPACT REPORT
FOR

THE
BOULEVARD PLAN

*A Specific Plan
for
the Auburn Boulevard Corridor from Sylvan Corners to the City Limits
&
for the Auburn Boulevard Roadway Design Improvement Project from
Sylvan Corners to Cripple Creek Road
City of Citrus Heights,
Sacramento County, California*

SCH No. 2003-062165

January 2005

**FINAL ENVIRONMENTAL IMPACT REPORT
FOR
THE BOULEVARD PLAN
A SPECIFIC PLAN
FOR THE AUBURN BOULEVARD CORRIDOR
FROM SYLVAN CORNERS TO THE CITY LIMITS
&
FOR THE
AUBURN BOULEVARD
ROADWAY DESIGN IMPROVEMENT PROJECT
FROM SYLVAN CORNERS TO CRIPPLE CREEK ROAD
CITY OF CITRUS HEIGHTS,
SACRAMENTO COUNTY,
CALIFORNIA**

Prepared for:
City of Citrus Heights
6237 Fountain Square Drive
Citrus Heights, CA 95621

Prepared by:
PAR ENVIRONMENTAL SERVICES, INC.
P.O. Box 160756
Sacramento, CA 95816

SCH No. 2003-062165

January 31, 2005

TABLE OF CONTENTS

1.0 INTRODUCTION	1-1
1.1 Overview.....	1-1
1.2 Organization of the Document.....	1-2
1.3 Recirculation of the Draft EIR Not Required	1-2
2.0 EXECUTIVE SUMMARY	2-1
2.1 Introduction.....	2-1
2.2 Environmental Process and Issues	2-4
2.3 Alternatives Considered.....	2-5
2.4 Summary of Environmental Impacts	2-5
3.0 WRITTEN COMMENTS RECEIVED	3-1
3.1 List of Commentors	3-1
4.0 RESPONSES TO WRITTEN COMMENTS	4-1
5.0 ORAL COMMENTS AND RESPONSES	5-1
5.1 Introduction.....	5-1
6.0 REVISIONS TO THE DRAFT SPECIFIC PLAN.....	6-1
7.0 MINOR CHANGES TO THE AUBURN BOULEVARD PLAN LINE	7-1
8.0 MINOR CHANGES AND ERRATA TO THE DRAFT EIR.....	8-1
8.1 Overview.....	8-1
8.2 Revisions and Editorial Changes to the Draft EIR	8-1

LIST OF TABLES

2-1 Summary Table.....	2-6
------------------------	-----

LIST OF FIGURES

Specific Plan Figure 1.2 Planning Area.....	follows 6-19
Specific Plan Figure 3.3 Zoning Districts Map	follows 6-19
Auburn Boulevard Plan Line Study	follows 7-1
EIR Figure 4.11-1 in Chapter 8.0.....	8-4

APPENDICES

- A Notice of Completion for the DEIR
- B *A Traffic Management Plan Guide* (Caltrans)

CHAPTER 1.0 INTRODUCTION

1.1 Overview

The Draft Environmental Impact Report (Draft EIR) for The Boulevard Plan (Specific Plan) and the Auburn Boulevard Roadway Design Improvement Project (SCH# 2003-062165) was submitted to the State Clearinghouse and released for public review for a 45-day review and comment period on October 12, 2004. The public review and comment period closed on November 29, 2004. This document provides a record of comments received on the Draft EIR and the City's responses to those comments and comprises the Final EIR for the project. In accordance with the California Environmental Quality Act (CEQA) Guidelines Section 15090, the City of Citrus Heights City Council as the Lead Agency decision-making body must review and consider the information contained in the Final EIR before it approves or rejects the project.

In accordance with CEQA Guidelines (Section 15132), this final EIR consists of the following:

1. Revisions to the Draft EIR;
2. Comments and recommendations received on the Draft EIR either verbatim or in summary;
3. A list of persons, organizations, and public agencies commenting on the Draft EIR;
4. The responses of the Lead Agency to significant environmental points raised in the review and consultation process;
5. Other information added by the Lead Agency.

In addition to considering the Final EIR, the Lead Agency is required to make findings of fact regarding the significant environmental impacts identified in the Final EIR and the alternatives to the project that were considered. If the project would result in significant environmental impacts which could not be mitigated, then the Lead Agency, in accordance with CEQA Guidelines (Section 15093), is required to make a statement of overriding considerations which explains in writing the specific reasons to support the Lead Agency's action based on the Final EIR and/or other information in the record.

The findings of fact and any statement of overriding consideration are made after the Lead Agency considers the Final EIR and are part of the public record.

Additionally, in accordance with CEQA Guidelines (Section 15097) a mitigation monitoring or reporting program (MMRP) must be adopted by the Lead Agency in order to ensure that the mitigation measures and project revisions identified in the Final EIR are implemented.

Both the findings of fact, statement of overriding considerations along with the MMRP are separate documents adopted after the Lead Agency has considered the Final EIR.

1.2 Organization Of This Document

The Final EIR is organized into seven chapters. Chapter 2.0 is an Executive Summary providing a brief project description and summary table of the project's environmental impacts and mitigation measures. Chapter 3.0, Written Comments Received, provides a list of commenting agencies, organizations and individuals and copies of the written comments (coded for reference). Chapter 4.0, Responses to Written Comments, provides the Lead Agency responses to the comments in Chapter 3.0 and briefly notes any revisions to the Draft EIR that are needed in order to respond to the comments. Chapter 5.0, Oral Comments and Responses, presents the oral comments made at the Planning Commission Hearings of October 28, 2004 and November 18, 2004 and responds to those comments related to environmental issues. Chapter 6.0, Revisions to the Draft Specific Plan, presents revisions made to the draft specific plan as a result of public and Planning Commission input at the previously noted hearings. Any changes to the Draft EIR required due to these revisions are summarized. Chapter 7.0 Minor Changes to the Auburn Boulevard Plan Line presents minor revisions that were made to the Plan Line Study. Chapter 8.0, Minor Changes and Errata to the Draft EIR, includes corrections and additions to the Draft EIR text as a result of comments made on the Draft EIR or changes made to the Draft Specific Plan. Changes to the Draft EIR are indicated by underline for additions and ~~strikeout~~ for deletions to the text.

1.3 Recirculation Of The Draft EIR Not Required

Comments received on the Draft EIR do not indicate that new significant impacts and/or significant new information have been identified, nor have changes in the project been made that would require recirculation of the Draft EIR pursuant to CEQA Guidelines (Section 15088.5).

CHAPTER 2.0 EXECUTIVE SUMMARY

2.1 Introduction

Project Location

Auburn Boulevard is located in the City of Citrus Heights in Sacramento County. The City is located near the center of the Sacramento Metropolitan Area in northeastern Sacramento County, on the south side of the Interstate 80 (I-80) corridor. The City of Roseville is located to the north and immediately across the Placer County line.

The Boulevard Plan (the Specific Plan) is comprised of the Auburn Boulevard corridor between the City of Citrus Heights city limits on the north and the Sylvan Road/Auburn Boulevard/Old Auburn Road intersection (Sylvan Corners) on the south, and extending approximately 1000 feet east and west of Auburn Boulevard, an area which totals approximately 460 acres. The Specific Plan focuses primarily on the parcels and commercial and residential uses (encompassing 112 acres) that front on Auburn Boulevard.

The Auburn Boulevard Roadway Design Improvement Project (Roadway Design Project), is located within the boundaries of the Boulevard Plan, and begins in the vicinity of Cripple Creek Road (a private road) on the north and ends at Sylvan Corners on the south.

Project Description

Summary of the Boulevard Plan

The Boulevard Plan is a Specific Plan in accordance with State Planning Law. It provides for the systematic implementation of the City's general plan for a defined area of the community. The Boulevard Plan includes the following:

- Concepts, Goals and Principles¹ to establish formal policies regarding land use, circulation and community design and presents, in diagrammatic form, key concepts of the plan;
- Development Standards that present specific guidance for public and private development along the Boulevard;

¹ Concepts, Goals and Principles

- Concept: A concept is an organizational idea. The concepts derive from the objectives established by the community and stakeholders in workshops and meetings. They inform the standards and guidelines.
- Goal: A goal is a general direction-setter. It is an ideal future end related to public health, safety, or general welfare. A goal is a general expression of community values and, may be abstract in nature.
- Principle: A principle is an assumption, fundamental rule, or doctrine guiding-tenet.

- Guidelines to provide guidance for making design changes along the corridor in each of four districts; and
- An Implementation Section that describes how the Plan's policies and guidelines are to be put into place.

Boulevard Plan Districts

The Boulevard Plan divides the Auburn Boulevard corridor into the following four districts:

1. Gateway District;
2. Rusch Park District;
3. Lincoln 40 District; and
4. Sylvan Corners Village Square District.

Land Use, Circulation and Community Design Concepts

The Plan contains Goals and Principles to support the Land Use, Circulation and Community Design Concepts for the Districts. These Goals and Principles are listed in Section 2 of The Boulevard Plan, which is incorporated by reference.

Land Use Designations

The Boulevard Plan uses six residential, commercial, open space, and public land use designations to depict the proposed land uses within The Boulevard Plan planning area. Each land use designation is defined in terms of allowable uses and density and intensity standards. The Boulevard Plan focuses primarily on the parcels and commercial uses that front Auburn Boulevard. Most of these parcels along the corridor are designated as General Commercial, which allows for a variety of land uses including mixed-use, multi-family residential, and service and retail commercial uses. The General Commercial designation is implemented through a new zoning district, the Auburn Boulevard Commercial District (ABC District).

Transportation and Circulation

The Plan seeks to improve pedestrian/motorist safety by limiting left-turn movements, consolidating and reducing the number of driveways, and creating a better balance among transportation modes (i.e., automobiles, transit, bicyclists, and pedestrians), while allowing for future growth in the area.

The Specific Plan also proposes to amend the General Plan Circulation Element to change Rollingwood Boulevard from a local street to a collector street.

General Plan Amendments

As part of Implementation of The Boulevard Plan, four amendments to the City of Citrus Heights General Plan Land Use Element are proposed. Each General Plan Amendment is described below.

- A. General Plan Amendment from General Commercial (GC) to Medium Density Residential affecting 3.63 acres located between the west end of Auburn Oaks Court and Interstate 80.
- B. General Plan Amendment from Medium Density Residential (MDR) to General Commercial (GC) affecting 3.61 acres located at the intersection of Auburn Boulevard and Auburn Oaks Court.
- C. General Plan Amendment from Business Professional (BP) to General Commercial (GC) affecting 1.72 acres located west of Auburn Boulevard between Rusch Park and Antelope Road.
- D. General Plan Amendment from Medium Density Residential (MDR) to General Commercial (GC) affecting 1.99 acres located on the south side of Antelope Road, west of Auburn Boulevard.

The Auburn Boulevard Plan Line

The Plan Line for Auburn Boulevard will cover the limits as described earlier for the Boulevard Plan, from Sylvan Corners on the south to the northerly City limits. The Plan Line will be adopted by the City Council as a document identifying the ultimate alignment of Auburn Boulevard and the required right of way acquisitions to implement the Specific Plan. The plan line will use the cross sections identified within the specific plan to develop the right of way requirements.

Utility Undergrounding

There are existing overhead utility lines located along the east side of Auburn Boulevard. The utilities involved include electric (SMUD), telephone (Surewest), cable television (Comcast), and fiber optic lines (Sprint). The project proposes to underground these lines along the east side of the roadway either below the sidewalk or adjacent to the easterly curb line. Trenching depths will be up to six (6) feet. Private easements may be required for additional utility company facilities.

Auburn Boulevard Roadway Design Improvement Project (Sylvan Corners to Cripple Creek Road)

The Draft EIR also addressed the project specific impacts that would result from the proposed Auburn Boulevard Roadway Design Improvement Project (Roadway Design Project), located within the boundaries of The Boulevard Plan, beginning in the vicinity of Cripple Creek Road (a private road) on the north and ending at Sylvan Corners on the south. The Roadway Design Project located in the portion of Auburn Boulevard that is within the Lincoln 40 District and the very southern edge of the Rusch Park District.

2.2 Environmental Process And Issues

In accordance with Section 15082 of the California Environmental Quality Act (CEQA) Guidelines, the City of Citrus Heights circulated a Notice of Preparation for the EIR on June 24, 2003 for a thirty day review period. The Notice of Preparation is provided in Appendix A of the Draft EIR. The notice was circulated through the State Clearinghouse to state agencies; the City of Citrus Heights circulated the notice to local and federal agencies and other interested parties in order to solicit comments on significant effects that could potentially occur due to the project. Concerns raised in response to the Notice of Preparation were taken into consideration when preparing the Draft EIR. Comments received on the Notice of Preparation are included in Appendix B of the Draft EIR.

An Environmental Checklist Form was attached to the Notice of Preparation, which identified environmental issues to be addressed in the Draft EIR. The following issues were found to be of concern due to the project's potential to have significant effects on the environment.

- Aesthetics (visual resources)
- Air Quality
- Biological Resources
- Cultural Resources (archaeology/historic architecture)
- Hazardous Materials
- Hydrology/Water Quality
- Land Use Planning
- Noise
- Population and Housing
- Public Services
- Recreation
- Transportation/Traffic
- Utilities

An Environmental Scoping Meeting was held in accordance with CEQA Section 21083.9 on July 24, 2003 at the City Council Hearing Room at 6237 Fountain Square Drive in Citrus Heights. The Scoping meeting was convened at 4 p.m., and at 7 p.m as part of the regularly scheduled Planning Commission Meeting. Notice of the meeting was provided in the Notice of Preparation and was provided in the City's advertisement of the City Council meeting.

The City of Citrus Heights held formal hearings before the City Planning Commission during the Draft EIR review period on October 28 and November 18, 2004. The majority of comments made at those hearings addressed concerns regarding the Specific Plan. Comments made at the hearings requiring a formal response are included in Chapter 5.0 of this Final EIR.

2.3 Alternatives To The Proposed Project

The CEQA Guidelines, Section 15126.6, state that “An EIR shall describe a range of reasonable alternatives to the project, or the location of the project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project, and evaluate the comparative merits of the alternatives. An EIR need not consider every conceivable alternative to a project. Rather it must consider a reasonable range of potentially feasible alternatives that will foster informed decision-making and public participation. An EIR is not required to consider alternatives which are infeasible.”

The following alternatives were addressed in the Draft EIR.

No Project Alternative

As required by the CEQA Guidelines the Draft EIR considered the No Project Alternative in order to allow decision-makers to compare the impacts of approving the proposed project with the impacts of not approving the proposed project [CEQA Guidelines Section 15126.6(e)(1)]. With the No Project Alternative there would be no adoption of a specific plan that would guide the implementation of comprehensive land use, circulation and community design concepts within the plan area. The roadway design improvements would not be constructed. Land uses would continue to be governed by the City of Citrus Heights General Plan and Zoning Ordinance and any proposed changes would be addressed on a case-by-case basis. Existing General Plan and Zoning designations would be retained.

Plan Alternatives

No other plans or development alternatives were addressed in the Draft EIR. Through a lengthy process of plan development, which included significant community and stakeholder involvement, the City developed a plan that addresses the needs and major problems related to land use and circulation in the specific plan area. The land use concepts, design guidelines and development standards were tailored to address specific problems within the specific plan area related to land use conflicts; under-utilization of the commercial corridor; the appearance of the commercial corridor; and transportation deficiencies – both automobile and non-motorized. No other approaches appeared to offer substantial environmental advantages over the proposed specific plan in addressing these issues. A description of the process used to develop The Boulevard Plan and plan scenarios evaluated are in Chapter 6.0 of the Draft EIR.

2.4 Summary of Environmental Impacts And Mitigation Measures

Table 2-1 provides a summary of impacts associated with the adoption and implementation of The Boulevard Plan, the General Plan Amendments, the Plan Line for Auburn Boulevard, future roadway improvements on Auburn Boulevard and construction of the Auburn Boulevard Roadway Design Improvement Project from Sylvan Corners to Cripple Creek Road.

Table 2-1 Summary Of Impacts And Mitigation Measures

Environmental Impact	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
4.2 AESTHETICS/VISUAL RESOURCES			
<i>A-1 Impacts to scenic resources, visual character and/or quality</i>			
A. Specific Plan Impacts: The potential exists for significant visual changes to take place as a result of tree removal for development on private properties along Cripple Creek in the vicinity of Rusch Park.	S	Implementation of the GP Policy 37.1, and compliance with the Tree Preservation Ordinance will encourage the retention of native oaks in the landscape of the specific plan area and will mitigate for the visual impacts resulting from the removal of native oak trees. See Mitigation Measures for biological resources, B-1, B-2A and B-2B and B-2C which address impacts to the oak woodland and riparian habitat adjacent to Cripple Creek.	LTS
B. Plan Line Adoption and Future Roadway Design Improvement Projects: Adoption of the Plan Line for Auburn Boulevard would assure that new development improvements such as landscaping are located outside of the future road right of way. Future roadway improvement projects constructed in accordance with The Boulevard Plan's Design Guidelines, for the most part would be beneficial, however increased right of way and widening would require removal of any existing trees and landscape improvements on properties that have not been redeveloped.	S	See A-1 above	LTS

Table 2-1 Summary Of Impacts And Mitigation Measures (continued)

Environmental Impact	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>C. General Plan Amendments: Site D (s/w quadrant of Auburn Blvd. and Antelope Rd.) Proposed commercial development constructed in conformance with proposed design guidelines would have positive visual impacts; however, two large oak trees on the site may be lost.</p>	S	See A-1 above	LTS
<p>D. Near-term Roadway Design Improvement Project Roadway improvements will require removal of some existing trees and landscaping, most notably, ten native oak trees with trunk diameters larger than six inches. One blue oak with 27-inch trunk diameters, and one valley oak with a 40-inch trunk diameter are included in this count. Construction of the on-street bicycle lane and a wider sidewalk at the southeast corner of Rusch Park will require installation of a retaining wall to avoid encroachment into the Creek. The construction will temporarily encroach into the Creek and removal of some trees may be necessary.</p>	S	Refer to Mitigation Measures B-1C, B-2A, B-2B and B-2C in the Biological Resources section. These measures require restoration of riparian vegetation and minimizing impacts to existing oaks and require replacement of removed oaks.	LTS

Table 2-1 Summary Of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
Impact A-2: Creation of a new source of light or glare that would adversely affect day or nighttime views in the area.			
A. Impacts of the Specific Plan, Plan Line Adoption and Future Roadway Improvement Projects, General Plan Amendments and Near-term Roadway	LTS	Implementation of The Boulevard Plan's Principles and Design Guidelines and enforcement of the City's Zoning Code Landscaping and Lighting standards will avoid impacts associated with light and glare.	LTS
Design Improvement Project. Spill over of lighting from the commercial corridor may impact adjacent residential neighborhoods, especially the rural neighborhoods on the east side of Auburn Boulevard.			
4.3 AIR QUALITY			
Impact AQ-1 - Construction-Related Impacts			
A. Construction-Related Impacts of the Specific Plan, Future Road Improvements, General Plan Amendments and Zone Change Construction activity associated with implementation of the proposed specific plan (including general plan amendments and zone change) would result in the temporary generation of ROG, NO _x and PM ₁₀ emissions. These emissions would result from construction equipment exhaust, and fugitive dust from land clearing, earthmoving, and wind erosion of exposed soil. It is possible some individual project components would be large enough to result in construction-related emissions greater than the significance thresholds.	S	<p><u>Mitigation Measure AQ-1A Inhalable Particulate Matter</u> The following mitigation measures shall be incorporated into the project to minimize the generation of PM₁₀ dust during construction.</p> <ul style="list-style-type: none"> ▪ enclose, cover, or water twice daily all soil piles; ▪ water exposed soil with adequate frequency for continued moist soil; ▪ water all haul roads twice daily; and ▪ cover loads of all haul/dump trucks securely. <p><u>Mitigation Measure AQ-1B Asbestos Mitigation Measures</u> The demolition or renovation of asbestos-containing building material is subject to the limitations of the National Emissions Standards for Hazardous Air Pollutants (NESHAP) regulations as listed in the Code of Federal Regulations (40CFR Part 61, Subpart M) requiring notification and inspection. Most demolitions and many renovations are subject to a CAL-OSHA Certified asbestos inspection prior to the start of activity. SMAQMD Rule 902, which requires District consultation and permit, applies to demolition, renovation or removal of asbestos-containing material. Compliance with these regulations is considered to reduce this impact to a less-than-significant level.</p>	

Table 2-1 Summary Of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
Implementation of the specific plan would result in the demolition of structures. Structures to be demolished may include building materials containing asbestos. Asbestos present in building materials may become airborne during demolition activities			
<p><u>B. Construction Impacts of the Roadway Design Improvement Project (Sylvan Corners to Cripple Creek Road)</u></p> <p>Construction activity associated with implementation of the proposed roadway design improvement project from Sylvan Corners to Cripple Creek Road would result in the temporary generation of ROG, NO_x and PM₁₀ emissions. These emissions would result from construction equipment exhaust, and fugitive dust from land clearing, earthmoving, and wind erosion of exposed soil. These increases in emissions were determined to be less than the SMAQMD thresholds.</p>	LTS	No mitigation measures required	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
Impact AQ-2 Operational Ozone Precursor, Inhalable Particulate Matter and Carbon Monoxide Impacts			
<p>A. Specific Plan, General Plan Amendments, and Zone Change</p> <p>Operational Impacts</p> <p>Ozone Precursor Impacts</p> <p>Operational motor vehicle activity associated with implementation of the proposed specific plan (including general plan amendments and zone change) would result in the long-term generation of ROG, NO_x and PM₁₀ emissions. The motor vehicle activity would result from trips generated by specific plan land uses. Implementation of the Specific plan would result in vehicle trips associated with existing land use designations being replaced with vehicle trips associated with proposed land use designations. The increases in emissions are less than the SMAQMD thresholds.</p> <p>Operational Carbon Monoxide Impacts</p> <p>Operational motor vehicle activity associated with implementation of the proposed specific plan would result in the long-term generation of CO emissions. The motor vehicle activity would result from trips generated by specific plan land uses. Implementation of the specific plan would result in vehicle trips associated with existing land use designations being replaced with vehicle trips associated with proposed land use</p>	LTS	No mitigation measures required	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>designations. The resulting change in vehicle trips would contribute to traffic volumes and vehicle congestion at the intersection of Antelope Road and Auburn Boulevard. Estimates were made of CO concentrations in the vicinity of this intersection under future year Cumulative No Project conditions, and Cumulative Plus Project conditions.</p> <p>Both one-hour average and eight-hour average CO concentrations near to the intersection of Antelope Road and Auburn Boulevard would be below ambient air quality standards for CO under both Cumulative No Project and Cumulative Plus Project conditions.</p>	LTS	No mitigation measures required	LTS
<p><u>B. Roadway Design Improvement Project Operational Impacts</u></p> <p>Implementation of the proposed Roadway Design Improvement Project would not change the number of vehicle trips generated in the project vicinity, would not change the capacity of the roadway system, and would not have a substantial effect on traffic operations. Therefore, the Roadway Design Improvement Project is not considered to have a significant operational impact on air quality.</p>	LTS		

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>C. Impacts of the Proposed Plan Line Adoption and Future Road Improvements</p> <p>Adoption of the proposed Plan Line by itself would have no effect on air quality. However, implementation of future road improvements that may result from adoption of the Plan Line would have an impact on air quality.</p> <p>The nature and magnitude of future road improvements are not known at this time. Therefore, quantification of the air quality impacts of the improvements is not possible. However, it is expected that, in general, the impacts would be similar to the impacts of the Roadway Design Improvement Project. These impacts are presented above.</p> <p>The impacts of the Roadway Design Improvement Project were found to be less-than-significant.</p>	LTS	No mitigation measures required	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
4.4 BIOLOGICAL RESOURCES			
<i>Impact B-1: Impacts to Waters of the United States Including Water Quality</i>			
A. Impacts of the Specific Plan, Plan Line Adoption and Future Roadway Improvement Projects and General Plan Amendments. Water quality in Cripple Creek, a tributary of Arcade Creek, could be adversely affected if untreated runoff from roadway and parking lots flows into the stream either directly or via the storm drain system. Runoff from development of land adjacent to Cripple Creek could result in increased sediment loads, turbidity, and siltation in the creek adversely affecting fish and other aquatic resources.	S	<p>The following measures shall be made conditions of approval for all projects within the specific plan area.</p> <p><u>Mitigation Measure B-1A Protect Cripple Creek's Aquatic Life:</u> To protect Cripple Creek's terrestrial and aquatic wildlife and special status species, and to avoid encroachments within the creek's floodplain, a Floodplain/Habitat Buffer (Buffer) should be established on both sides of Cripple Creek for projects adjacent to the Creek. Ownership and management of the Buffer should be consistent with implementing plans that fulfill the goals and policies of the City of Citrus Heights General Plan and shall be consistent with the City's administrative policies and procedures for drainage and development, and objectives of the Arcade Creek Watershed Plan, so that individual property owners are not free to undertake vegetation clearing, bank protection, soil disturbance, or creation of fences or structures within the Buffer. The width of the buffer shall be the 100-year floodplain boundary, as defined by best available data (County hydraulic studies, FIRM and other flood data).</p> <p><u>Mitigation Measure B-1B Protect Cripple Creek's Water Quality:</u> To prevent urban pollutants from entering into Cripple Creek from The specific plan area parking lots and paved surfaces, site design shall incorporate features that will intercept runoff before it can enter the storm drain system or directly enter Cripple Creek. Use of features such as filtration strips or bioswales is recommended.</p>	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>D. Impacts of the Near-term Roadway Design Improvement Project. Impacts to Waters of the U.S. as a result of construction and operation of the Roadway Design Improvement Project are limited to the area at Cripple Creek. Construction of the on-street bicycle lane and a wider sidewalk at the southeast corner of Rusch Park will require installation of a retaining wall to avoid encroachment into the Creek. The construction will temporarily encroach into the Creek however, and some temporary fill may be necessary. Earthmoving and construction activities within the immediate watershed of the creek could result in increased sediment loads, turbidity, and siltation in the creek, as well as the potential for other pollutants to enter the creek.</p>	S	<p>Refer to Mitigation Measures B-1A and B-1B. The full text of the following measures are presented in Chapter 4.0 of the Draft EIR and in the mitigation monitoring programs.</p> <p><u>Mitigation Measure B-1C Minimize Impacts to Riparian Habitat and Waters of the United States:</u></p> <p><u>Establish Environmentally Sensitive Areas (ESAs)</u> to limit work areas near Cripple Creek riparian habitat and stream channel to the minimum possible area.</p> <p><u>Mitigation Measure B-1D Prepare and Implement Riparian and Wetland Restoration Plan:</u> To restore disturbed habitat at the site retaining wall site, a riparian/wetland restoration plan should be prepared by a qualified restoration ecologist prior to construction.</p> <p><u>Mitigation Measure B-1E Implement Water Quality Protection Measures:</u> Potential instream impacts to Cripple Creek and the Arcade Creek Watershed aquatic resources and fisheries shall be minimized by adherence to State Standard Specifications for avoidance of water pollution (Section 7-1.01G) and by implementing Best Management Practices.</p> <p><u>Mitigation Measure B-1F Obtain Clean Water Act Permit and Streambed Alteration Agreement:</u> Prior completion of project final design, consult with the USACOE to determine permitting requirements under the Clean Water Act Section 404. Obtain Streambed Alteration Agreement from California Department of Fish and Game (CDFG).</p>	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>Impact B-2: Impacts to Native Oaks, Oak Woodland and Associated Wildlife</p> <p>A. Impacts of the Specific Plan, Plan Line Adoption and Future Roadway Improvement Projects and General Plan Amendments. Future development and redevelopment activities within The specific plan area have the potential to directly and indirectly impact native oak trees and remaining oak woodlands, including the area adjacent Cripple Creek.</p>	S	<p>Mitigation Measure B-2A Minimize Impacts to Oaks: To ensure consistency with the City of Citrus Heights' Policy 37.1, which requires incorporation of existing trees into development projects, building envelopes for future development projects should be configured to minimize impacts to trees to the extent feasible. The following measures shall be implemented:</p> <ol style="list-style-type: none"> 1. Building envelopes should be established on plans and specifications for the future development projects to designate the area needed for construction of roads, driveways, and building pads. 2. These building envelopes should be large enough to include not only the proposed improvements, but also work areas for heavy equipment, staging areas, and equipment and material lay down areas. 3. To protect trees elsewhere on construction sites, no construction activities or use of heavy equipment should occur outside of the building envelopes. 4. Oaks that fall within the building envelope but which are not slated for removal should be protected by the following measures, which should be implemented during all construction phases of the project: <ol style="list-style-type: none"> a. Plans and specifications should clearly state protection procedures for oaks to be preserved on the project site. The specifications should also require contractors to stay within designated work areas and should include a provision for penalties if oak trees are damaged; b. No vehicles, construction equipment, mobile offices, or materials should be parked or located within the driplines of oaks and other trees that are to be preserved; c. Soil surface removal should not occur within the driplines of oaks to be preserved. No cuts or trenching should occur within the dripline. If this area cannot be avoided, then the tree should be added to the list of oaks to be replaced through an on-site planting; 	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
		<p>d. Earthen fill deep should not be placed within the driplines of oak trees to be retained, and no fill should be placed within five feet of their trunks, except</p> <p>e. Paving should not be placed within the dripline of oaks to be retained;</p> <p>f. Underground utility line trenching should not occur within the driplines of oaks to be retained. If it is absolutely necessary to install underground utilities within the driplines of oak trees, the trench should either be bored or drilled but not within five feet of the trunk and a certified arborist should be retained to monitor this construction and repair or wrap any damaged roots.</p> <p>g. <u>Living Among the Oaks: A Management Guide for Landowners</u> (UC Cooperative Extension, Berkeley) in Appendix H should be used by the City as a guide in reviewing landscape plans. The information should be distributed to landowners and developers to provide information and guidelines for preparing landscape plans and for protecting oaks after construction is complete.</p> <p>h. <u>Mitigation Measure B-2B Prepare and Implement Oak Replacement and Management Plan (Oak Woodland Replacement)</u>: In order to compensate for impacts due to removal of native oak trees found within oak woodland and/or riparian habitats (as opposed to isolated landscape or street trees), the following measures shall be implemented:</p> <ol style="list-style-type: none"> 1. Oak trees shall be planted on project sites or off-site in numbers and species composition similar to those impacted. 2. Prior to approval of development or redevelopment projects, a qualified biologist or arborist should make an accurate count of the number, diameter, and species of trees that would be removed within each building envelope or area subject to disturbance. Based on the estimate, an Oak Replacement and Management Plan (Oak Plan) should be prepared in accordance with the City of Citrus Heights Tree Ordinance, and consistent with the City of Citrus Heights General Plan biological resource goals and policies. 	

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
		<p>a. The goals of the Oak Plan should be to replace trees lost by the project to create healthy, self-sustaining habitats that are not dependent on maintenance or irrigation following the minimum maintenance period. The functions and values of the created habitat should approximate those of the affected habitats, i.e., the functions and values of oak woodland rather than an ornamental landscape planting.</p> <p>b. At a minimum, the Oak Plan should include clear success criteria, monitoring and reporting requirements, and a contingency plan should the responsible parties fail to meet the success criteria that ensure that mitigation goals and ratios are met. The Oak Plan should also include details for the species, size of plants and quantities, planting techniques, techniques for protecting the trees from herbivory, and irrigation, weed control and maintenance plan, and monitoring requirements.</p> <p><u>Mitigation Measure B-2C Prepare and Implement Oak Replacement and Maintenance Plan (Landscape Tree Replacement):</u> In order to compensate for impacts due to removal of native oak trees found within landscape settings (i.e. isolated landscape or street trees), the following measures shall be implemented:</p> <p>1. Oaks trees shall be planted on project sites or off-site in numbers and species composition similar to those impacted.</p> <p>a. Prior to approval of development or redevelopment projects, a qualified biologist or arborist should make an accurate count of the number, diameter, and species of trees that would be removed within each building envelope or</p>	

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
		<p>area subject to disturbance. Based on the estimate, an Oak Replacement and Maintenance Plan (Oak Plan) should be prepared in accordance with the City of Citrus Heights Tree Ordinance, and consistent with the City of Citrus Heights General Plan biological resource goals and policies.</p> <p>The goals of the Oak Plan, at a minimum, should be to replace trees lost by the project in an appropriate landscape setting that will allow trees to thrive and be self-sustaining and not dependent on maintenance or irrigation following the minimum maintenance period. Replacement within the specific plan area's planned landscape areas as street trees, trees for public space landscape or roadway medians, should be emphasized when identifying replanting sites.</p> <p>Replacement in a natural habitat setting as described in Measure B-2B would also accomplish these oak tree replacement goals.</p>	
<p>B. Impacts of the Near-term Roadway Design Improvement Project. The proposed Roadway Design Improvement Project, from Sylvan Corners to Cripple Creek Road would potentially require removal of ten native oak trees larger than six inches in diameter (dbh)</p>	S	<p>The following measures shall be made conditions of approval for all projects within The Boulevard Plan Area.</p> <p>Mitigation Measure B-2D Preconstruction Tree Survey: Prior to construction, a qualified biologist or arborist should make an accurate count of the number, diameter, condition and species of trees that would be removed by the roadway improvement project. An Oak Tree Replacement and Management Plan shall be prepared in accordance with Mitigation Measures B-2A, B-2B and B-2C described above.</p>	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (*continued*)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
Impact B-3: Impacts to Nesting/Migratory Birds			
A. Impacts of, Plan Line Adoption and Future and Near-Term Roadway Improvement Projects and General Plan Amendments. Tree removal for construction could harm nesting migratory birds if it occurs during the nesting season (February 1– July 31). Cooper’s hawks and red-shouldered hawks are likely nesters in The Specific Plan area, as are many other species.	S	<p>The following measures shall be made conditions of approval for all projects within the specific plan area.</p> <p><u>Mitigation Measure B-3 Avoid Impacts to Nesting Birds</u></p> <ol style="list-style-type: none"> 1. If tree removal for construction will occur during the nesting season (February through July), a minimum of two pre-construction surveys should be conducted in construction areas for nesting birds. Surveys shall be conducted by a qualified wildlife biologist. 2. Surveys should be conducted no more than 14 days prior to the initiation of tree-removal activities during the early part of the breeding season (February through April) and no more than 30 days prior to the initiation of these activities during the late part of the breeding season (May through July). 3. If the surveyor deems that an active bird nest is close enough to the construction area to be disturbed, he or she should (in consultation with CDFG) determine the extent of the construction-free buffer zone to be established around the nest. 4. Trees should be removed outside the nesting season (February through July), or after a qualified wildlife biologist verifies that the nest is empty and the nest tree is no longer used by the adults and young birds. 	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
Impact B-4: Introduction of Noxious Weeds-Significant and Avoidable Impact			
A. Impacts of the Specific Plan, Plan Line Adoption and Future and Near-term Roadway Improvement Projects and General Plan Amendments. construction activities and soil disturbance from the construction activities could result in the introduction and spread of noxious weeds and other invasive plants, as could inappropriate erosion control measures. This is of particular concern in the vicinity of Cripple Creek. Erosion control measures such as use of straw bales and seed can also result in the inadvertent introduction of invasive plants to the area	S	Mitigation Measure B-4 Avoid Introduction and Spread of New Noxious Weeds. In the vicinity of Cripple Creek, during construction only certified weed-free straw will be used and all disturbed soils will be thoroughly covered with straw (or mulch or chips created on-site during tree removal) upon completion of grading. No seed mixes should be used unless consisting of locally native grasses and forbs.	LTS
4.5 CULTURAL RESOURCES			
Impact CR-1: Potential Impacts To Undiscovered Prehistoric, Archaeologic, And Historic Resources			
A. Impacts of Specific Plan, Plan Line Adoption, and Future Roadway Improvements. All ground disturbing activities associated with implementation of the specific plan, general plan amendments and roadway design improvements have the potential to adversely affect undiscovered prehistoric, archaeological, and historic resources.	S	Mitigation Measure CR-1: Handling of Discovered Artifacts or Remains: Should any cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, human remains, or architectural remains be encountered during any development activities, work shall be suspended according to (A) below. It is recommended under CEQA and Policy 41.1 of the Citrus Heights General Plan that: 1. In the event that any prehistoric, historic, or paleontological resources are discovered during construction-related earth moving activities, all work within 50 feet of the resources shall be halted and the developer shall consult with a qualified archaeologist or paleontologist to assess the significance of the find. If any find is determined to be significant by the qualified archaeologist, then	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
		<p>2. representatives from the City of Citrus Heights and the qualified archaeologist and/or paleontologist would meet to determine the appropriate course of action.</p> <p>3. Pursuant to Section 5097.97 of the State Public Resources Code and Section 7050.5 of the State Health and Safety Code, in the event of the discovery of human remains, all work is to stop and the County Coroner shall be immediately notified. If the remains are determined to be Native American, guidelines of the Native American Heritage Commission shall be adhered to in the treatment and disposition of the remains.</p> <p>And that under Policy 42.3 of the Citrus Heights General Plan that planners: Establish thresholds by which future projects can be judged when considering historic impacts. These standards should include height and massing considerations for proposed projects that are located in close proximity to historic resources (individual structures and districts) and define locations for potential prehistoric resources.</p>	
Impact CR-2: Potential Impacts To Existing Prehistoric, Archaeological, And Historic Resources A. Impacts of Specific Plan, Plan Line Adoption, and Future Roadway Design Improvement Projects. There are a number of properties within the Specific Plan area that appear to be 45 ears old or older. These buildings could be affected by future redevelopment projects. As an historic	S	Mitigation Measure CR-2: Evaluation of Historic Resources Older than 45 Years: Prior to approval of projects or issuance of construction or grading permits, cultural resources that appear to be 45 years old or older on a proposed project site need to be recorded for the purposes of inclusion in the State Office of Historic Preservation's filing system. "The 45 year criterion recognizes that there is commonly a five year lag between resource identification and the date that planning decisions are made" (California, State of 1995). Should the five year	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
cemetery, there is the potential for burials to be present outside of the current marked boundaries of Sylvan Cemetery. These unmarked burials could be disturbed by construction activity.		period lapse between the completion of the initial cultural resources documentation and the start date of the project, the cultural resource studies would need to be updated to include any additional properties/sites that would, by that time, meet the 45 year criteria.	
B. Impacts of the General Plan Amendments. All of the sites that are subject to proposed general plan amendment are currently developed sites. One site is a trailer park located on the southwest portion of the intersection of Antelope Road and Auburn Boulevard. This park is more than 50 years old but lacks significance and integrity necessary to be considered potentially eligible for the National Register of Historic Places or CEQA.	LTS	See CR-1 for measures related to potential impacts to buried remains. No additional mitigation measures required	LTS
4.6 HAZARDOUS MATERIALS			
Impact HM-1: Impacts related to hazardous emissions or handling of hazardous materials within one-quarter mile of an existing or proposed school.			
A. Impacts of the Specific Plan The proposed Specific Plan Area includes two schools, located at the northwest corner of the intersection of Auburn Boulevard/Sylvan Road/Old Auburn Road. Implementation of the specific plan may involve demolition or significant remodeling of existing structures in the vicinity. Redevelopment activities in the vicinity of the intersection could involve structures with asbestos or other hazardous materials.	S	Mitigation Measure HM-1A Handling of Asbestos Material: Control devices and fugitive emissions monitoring are required during demolition activities which will disturb, or have the possibility of disturbing, the asbestos-containing materials. All asbestos containing building material within the buildings planned for demolition should be removed prior to any demolition activity that could break up, dislodge, or similarly disturb these materials. This removal must be done using appropriate engineering controls, in compliance with all regulations, and be a contractor certified by the Contractor's State License Board and registered by the California Division of Occupational Safety and Health (Northwest Envirocon, Incorporated 1997).	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>B. Impacts Related to Plan Line Adoption and Near-term and Future Roadway Design Improvement Projects. Implementation of future roadway improvement projects in the vicinity of the schools adjacent to Auburn Boulevard may involve removal of yellow thermoplastic traffic striping from the existing roadway surface. Yellow traffic stripes may contain heavy metals such as lead and chromium at concentrations in excess of hazardous waste thresholds established by the CCR and may produce toxic fumes when heated. Disposal of stripes is required at a Class 1 disposal facility.</p>	S	<p><u>Mitigation Measure HM-1B Disposal Of The Yellow Thermoplastic Traffic Stripes:</u> Disposal of the yellow thermoplastic traffic stripes will be at a Class 1 disposal facility. All aspects of the project associated with removal, storage, transportation, and disposal of the yellow thermoplastic traffic striping, should be in strict accordance with the appropriate regulations.</p>	LTS
<p>4.6.5.2 Impact HM- 2: Impacts related to presence of listed hazardous materials sites that would create a significant hazard to the public or the environment; or impacts related to the creation of a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment.</p>			
<p>A. Impacts of the Specific Plan and the General Plan Amendments Implementation of the specific plan may involve demolition or significant remodeling of existing structures. Redevelopment activities could involve structures with asbestos or other hazardous materials, or could disturb soils that have been contaminated by past land uses and result in exposure of workers and the general public to hazardous waste due to soil disturbance of soils or exposure to groundwater.</p>	S	<p>See HM 1A and 1B above, plus the following measures. <u>HM-2A Service Station Sites:</u> At a minimum, prior to redevelopment activities or acquisition of property for road improvements, additional review of site specific data shall be conducted regarding the status of investigation/remediation and how it might impact the planned improvements prior to right of way acquisition and/or redevelopment activities. <u>HM-2B Additional Investigations:</u> Prior to redevelopment or acquisition of property frontage for roadway improvements, an investigation to further define past site use at 7800 Auburn Boulevard (northeast corner of Auburn Boulevard and Watson Way) shall be conducted. If the site investigation confirms that a service station was present, a preliminary investigation shall be conducted. This investigation shall focus on detecting hydrocarbon contamination in the soil and groundwater within the property boundaries.</p>	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>B. Impacts Related to Plan Line Adoption and Future Roadway Improvement Projects. Roadway improvement projects will involve right of way acquisition and ground disturbance on properties adjacent to Auburn Boulevard. Record searches conducted for the ISA identified 19 properties bordering Auburn Boulevard for which hazardous materials issues are present. Additional investigations are required to determine whether these sites have existing soils and/or water contamination problems. Future roadway improvement projects may involve removal of yellow thermoplastic traffic striping from the existing roadway surface. Disposal of stripes is required at a Class 1 disposal facility.</p>	S	Implement Mitigation Measures HM-1A, 1B, 2A and 2B.	LTS
<p>C. Auburn Boulevard Roadway Design Improvement Project. There are twelve sites adjacent to Auburn Boulevard in the area of the Roadway Design Improvement Project (between Sylvan Corners and Cripple Creek Road) that may have existing or historic land uses that require further study due to possible soil or ground water contamination. Additional investigations are required to determine whether these sites have existing soils and/or water contamination problems.</p>	S	Implement Mitigation Measures HM-1A, 1B, 2A and 2B.	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
4.7 HYDROLOGY AND WATER QUALITY			
<i>Impact H-1. Impacts to Water Quality due to Substantial Additional Sources of Polluted Runoff.</i>			
A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Future and Near Term Roadway Design Improvements. the proposed Specific Plan's emphasis on landscaping (landscaped street medians, street trees, parking lot landscape borders and public spaces) could lead to increased runoff from irrigation overspray. This is of concern because of the role that irrigation runoff plays in introducing pollutants into streams and the stated need to improve and monitor water quality of Arcade Creek and its tributaries, with special emphasis on pesticide levels and other toxicants contained in runoff.	S	<p><u>Mitigation Measure H-1: Incorporate Development Standards for Improving Water Quality:</u> The City shall incorporate water quality protection measures into the specific plan Development Standards. The standards may include but are not limited to the following:</p> <ol style="list-style-type: none"> 1. Install and maintain landscaping that requires minimal application of chemical fertilizers, pesticides and herbicides; 2. Emphasize xeriscape landscaping that reduces the need for irrigation by minimizing the use of turf in decorative landscaping, using plant materials adapted to local conditions and efficient irrigation; 3. Minimize irrigation overspray - do not permit use of sprinkler and spray irrigation in areas less than 8 feet wide; 4. Use of drip irrigation systems where feasible; 5. Incorporate features such as filtration strips or bioswales in site design to prevent urban pollutants from entering into Cripple Creek via storm drains from parking lots and paved surfaces. <p><u>Mitigation Measure H-2: Implement Best Management Practices (BMPs):</u> The City shall require implementation of best management practices for public and private development. Such practices may include, but are not limited to:</p> <ol style="list-style-type: none"> 1. Regular inspection, maintenance and cleaning out of stormwater retention or detention structures; 2. Regular inspection, maintenance and cleaning out of oil and water separators; 3. Encourage property owners to regularly remove trash, dead vegetation and leaf litter; 4. Encourage use of landscaping and horticultural practices that minimize the need for chemical fertilizers, herbicides and pesticides. 	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
Impact H-2: Impacts due to substantial alterations to the existing drainage patterns of the site or area, including through the alteration of the course of a stream or river, resulting in erosion or runoff and flooding. Create runoff water which would exceed the capacity of existing or planned stormwater drainage systems.			
A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Future and Near Term Auburn Boulevard Roadway Design Improvements. The proposed Specific Plan and its roadway improvement projects would not substantially alter topography or change drainage patterns. Impacts are less than significant since the area is substantially built out with an existing storm drain system, no impacts related to capacity of the system are anticipated, no substantial increase in stormwater runoff is anticipated.	LTS	No mitigation measure required.	LTS
Impact H-3: Impacts due to Placement of Housing Within a 100-year Flood Hazard Area Or Placement Within A 100-Year Flood Hazard Area Structures Which Would Impede Or Redirect Flood Flows.			
A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Future and Near-term Roadway Design Improvements. Areas adjacent to Cripple Creek are within the 100-year floodplain. While The Boulevard Plan does not specifically address development adjacent to the Creek, the City of Citrus Heights administrative policies and procedures for drainage and development on property affected by the 100-year floodplain.	LTS	No mitigation measures required.	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
4.8 LAND USE AND PLANNING			
Impact LU-1: Land uses that are incompatible with existing or planned land uses on or surrounding the project site.			
A. Impacts of the Proposed Specific Plan and Proposed General Plan Amendments Overall, there would be little change in the general categories of land uses that already exist in the Commercial Corridor, and no changes to existing residential land uses outside of the Commercial Corridor are proposed. The amount of commercial floor area would be reduced along the Corridor by 22 percent for retail and 20 percent for office. With the implementation of the proposed Urban Design Guidelines and the City of Citrus Heights Zoning Code sections addressing parking lot lighting, and landscaping adjacent to residential areas, conflicts between the commercial and residential uses would be less than significant.	LTS	No mitigation measures required.	LTS
Impact LU-2: Would the proposed project physically divide an established community?			
A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption. The intent of the Boulevard Plan is to improve connectivity within the specific plan area by improving accessibility, mobility and improving the streetscape for pedestrians,	No Impact	None required.	No Impact

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
bicyclists and transit users. With the implementation of land use concepts, circulation concepts and design concepts, The Boulevard Plan, including the proposed general plan amendments, Plan Line adoption and future roadway improvements will increase connectivity.			
B. Impacts of the Auburn Boulevard Roadway Design Improvement Project. Construction activities would result in temporary disruption to connectivity by requiring detours for pedestrians and bicyclists, and traffic congestion.	LTS	No mitigation measures required.	LTS
4.9 NOISE			
Impact N-1: Transportation Noise - Exposure of persons to, or generation of, noise levels in excess of standards established in the local General Plan or noise ordinance, or applicable standards of other agencies			
A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Roadway Design Improvement Projects. Traffic associated with the project would reduce traffic noise levels along Auburn Boulevard by about 1 dB as compared to the No Project condition, the result of the project-related reduction in the speed limit.	LTS	No mitigation measures required.	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
Impact N-2: Non-Transportation Noise - A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project.			
A. Impacts of Specific Plan and General Plan Amendments. Commercial HVAC system noise is potentially significant, depending upon the size of the systems and their locations relative to noise sensitive uses. Loading docks are potentially significant noise sources, depending upon their locations and the hours of use. Offices and supporting retail uses would also have the potential to affect neighboring noise sensitive uses, primarily due to noise from on-site traffic and service activities.	S	Mitigation Measure N-2 On-site Noise Control: To ensure mitigation of noise due to project-related loading docks and on-site traffic, development proposals should be reviewed to identify potential noise conflicts with existing or proposed noise sensitive uses. Implementation of the noise standards contained in the Noise Element of the Citrus Heights General Plan will mitigate project-related noise to an insignificant level. For development requiring installation of large ground-mounted HVAC systems, development review should include an assessment of noise impacts on nearby residential areas.	LTS
Impact N-3: Construction Impacts - A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project.			
A. Impacts of Specific Plan, General Plan Amendments and Roadway Design Improvement Projects. During the construction phases of the project, noise from construction would dominate the noise environment in the immediate area. Construction noise levels could result in annoyance or sleep disruption for nearby residences. Road construction noise is regulated by Caltrans standard specifications Section 7-1.01I "Sound Control Requirement" and the City noise ordinance applies to the road construction and to other construction activities involved in redevelopment of the commercial corridor.	LTS	No mitigation measures required.	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
4.10 POPULATION AND HOUSING			
<i>Impact PH-1: Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure.</i>			
A. Impacts of the Specific Plan, General Plan Amendments Plan Line Adoption and Future Roadway Design Improvement Projects. The specific plan area is built out, with the exception of a few vacant properties. Under the proposed Specific Plan, redevelopment activities would result in the addition of approximately 541 residential units that would accommodate a population of approximately 1,352 people. would result in an overall decrease in the amount of commercial floor space. The increase in population is not considered substantial (1.6 percent of the projected 2007 population). No significant environmental impact is anticipated from the additional housing since it is occurring in an area with existing services, and would be in conjunction with mixed use development and transportation improvements (both automobile and non automobile) serving the area.	LTS	No mitigation measures required.	LTS
B. Auburn Boulevard Roadway Design Improvement Project (Sylvan Corners to Cripple Creek Road) The Roadway Design Improvement project would not induce growth or create additional demand for housing. No impact.	LTS	No mitigation measures required.	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<i>Impact PH-2: Impacts from displacement of substantial numbers of people, impacts due to acquisition of property.</i>			
<p>A. Impacts of the Specific Plan and General Plan Amendments The proposed Specific Plan primarily focuses on redevelopment and rehabilitation of commercial properties along the Auburn Boulevard Corridor. Impacts to residential uses would occur as a result of the redevelopment on two sites that are existing trailer parks. GPA Site D is an existing trailer park with approximately 27 mobile home units on 1.99 acres. The trailer park at Pratt Avenue has approximately 16 units.</p>	S	<p><u>Mitigation Measure PH-1A: Disclosure Requirements:</u> Prior to approving a development project that would result in conversion of trailer parks to other uses, the City shall comply with Government Code Section 65863.7, a copy of which is included in Appendix J of this EIR.</p>	LTS
<p>B. Plan Line Adoption and Future Roadway Design Improvement Projects. Future roadway design improvement projects would require removal of approximately 3 residences for the realignment of the Linden Avenue intersection. The realignment of Linden Avenue and Walnut Drive (private) would also require removal of two commercial structures. Installation of raised medians will limit left-turn movements to median breaks. Direct left-turn access to some businesses will be eliminated; however u-turn movements will be possible and access will be maintained to all properties along Auburn Boulevard.</p>	S	<p>The following mitigation measures shall be implemented in conjunction with the design and environmental review process for future roadway design projects.</p> <p><u>Mitigation Measure PH-1B: Relocation Assistance for Housing Displacement:</u></p> <ol style="list-style-type: none"> 1. The City shall provide standard relocation assistance to both tenants and owner occupants in compliance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Replacement housing must be decent, safe, and sanitary (DS&S), which means it must meet all of the minimum requirements established by Federal regulations and conforms to applicable housing and occupancy codes. 2. All real property transactions shall comply with the property acquisition and relocation standards of the State of California, the Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. 	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
		<p><u>Mitigation Measure PH-1C Business Relocation:</u> The following mitigation measures shall be required to compensate for right-of-way acquisition.</p> <ol style="list-style-type: none"> 1. Property owners shall be compensated in accordance with fair market values based on appraisals. Business owners shall be compensated based on an assessment of the value of the business and any loss of good will. 2. All efforts shall be made to identify relocation opportunities for affected businesses that would reduce the loss of goodwill and historic patronage. Wherever feasible, assistance shall be made available in identifying suitable relocation sites within the service area of existing businesses. 	
<p>C. Near-term Roadway Design Improvement Project. The Roadway Design Improvement project would not displace housing or persons but would require acquisition of property frontages for additional right-of-way. Parking may be affected by the widening of the right of way (Parking is discussed in Section 4.11 Transportation). <i>This is a significant impact which can be mitigated.</i></p>	S	<p><u>Mitigation Measure PH-1D Property Compensation:</u></p> <ol style="list-style-type: none"> 1. All real property transactions shall comply with the property acquisition and relocation standards of the State of California, the Caltrans Relocation Assistance Program and the Federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. 2. Property owners shall be compensated in accordance with fair market values based on appraisals. Business owners shall be compensated based on an assessment of the value of the business and any loss of good will. 3. All efforts shall be made to identify relocation opportunities for affected businesses that would reduce the loss of goodwill and historic patronage. Wherever feasible, assistance shall be made available in identifying suitable relocation sites within the service area of existing businesses. 	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>4.11 Transportation and Circulation</p> <p>Impact T-1: Impacts related to increase in traffic which would be substantial in relation to the existing traffic load and capacity of the system.</p> <p>The proposed Specific Plan will be implemented over a period of approximately 20 years; buildout conditions would not occur until the year 2025. The near term and future roadway design improvement projects are not capacity enhancing projects and would not generate additional vehicle trips in the corridor. Therefore, the existing plus project condition was not analyzed for the specific plan or the roadway design improvement projects. Development of roadway improvements identified in the Plan would not be constructed under existing conditions.</p>	LTS	None required.	LTS
<p>Impact T-2: Impacts related to exceeding, either individually or cumulative, a level of service standard established by the county congestion management agency for designated road or highways.</p>			
<p>A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Future Roadway Design Improvement Projects.</p> <p>The traffic study conducted for the project indicate that with implementation of project, study roadway segments and intersections would operate at acceptable LOS according to the City of Citrus Heights LOS thresholds contained in the General Plan policies.</p>	LTS	None required.	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
Impact T-3: Impacts that would substantially increase hazards due to a design feature.			
A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Future and Near-term Roadway Improvement Projects. The Plan includes a thorough analysis of existing conflicts due to the design of the roadway and identifies specific locations where bus turnouts and crosswalks should be located. The roadway standards identified in the Plan are intended to develop a corridor that is easy to maneuver, pedestrian-friendly, and safe to use. The Roadway design improvement projects would implement these standards.	No Impact	None required.	No Impact
4.11.6.4 Impact T-4: Impacts that would result in inadequate emergency parking.			
A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Future and Near-term Roadway Improvement Project The proposed Specific Plan and its components would have no effect on parking for emergency vehicles.	No Impact	None required.	NA

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
4.11.6.5 Impact T-5: Impacts that would result in inadequate parking capacity.			
<p>A. Impacts of the Specific Plan and General Plan Amendments.</p> <p>One objective of the specific plan is to change the relationship between transportation patterns and development standards along the Auburn Boulevard corridor. The parking standards of the City would apply within the specific plan area; however, the parking standards in the specific plan allow flexibility for the provision of parking in some instances. Where businesses are accessible to pedestrians and bicycles, it may be feasible to reduce parking requirements.</p>	S	<p>Mitigation Measure T-5A: Ensure Adequate Parking Supply</p> <p>In order to ensure that adequate parking supply is maintained in the specific plan area, the city shall establish a special permit process to allow flexibility in the number of required parking spaces when deemed appropriate.</p>	NA
<p>B. Impacts of the Plan Line Adoption and Future Roadway Design Improvement Projects. Property acquisition for future roadway projects may impact parking areas of existing businesses. By adopting the Plan Line, the City will ensure that impacts to parking facilities for new commercial uses will be avoided since parking areas for new businesses will be located outside of the Plan Line. The determination of parking impacts will need to be made for each roadway design improvement project as it is funded and approved.</p>	LTS	No mitigation measures required.	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
<p>C. Impacts of the Near-term Roadway Design Improvement Project. The near-term roadway design improvement project will require acquisition of frontage properties on Auburn Boulevard to provide for widening and construction of improvements. Property acquisition will impact parking areas at approximately nine existing businesses. Property owners must be compensated for the loss of the underlying real property. Additional mitigation measures would be needed if the parking loss affects the viability of the business operations.</p>	S	<p><u>Mitigation Measure T-5B: Compensate for Parking Impacts.</u></p> <p>1. The determination of project-caused parking impacts shall be made in accordance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Project proponents shall compensate for acquisition of underlying property in compliance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.</p> <p>2. In the event that it is determined that the project will result in a reduction of parking spaces below the number required by zoning, but would not preclude continued use of the parcel that is allowed by zoning as determined by the appropriate land use authority (City of Citrus Heights), the project proponent(s) shall:</p> <ul style="list-style-type: none"> (a) Investigate feasibility of and compensate for cost of reconfiguring parking area or relocating parking on the same parcel to provide additional spaces; or (b) Investigate feasibility of and compensate for the cost of providing off-site parking; and/or (c) The appropriate land use authority (City of Citrus Heights) shall grant a special parking permit to allow the continued use with reduced parking. 	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (continued)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
		<p>3. If it is determined during the right-of-way appraisal and acquisition process that the project would result in a reduction of parking spaces that would preclude continued use of the parcel in accordance with the existing zoning, and none of the measures under No. 2 above are feasible, the project proponent(s) shall provide compensation in accordance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.</p>	
<p>4.11.6.6 Impact T-6: Impacts to the circulation network during construction activities</p> <p>A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Future and Near Term Roadway Design Improvement Projects</p> <p>Construction activities would result in temporary disruption to businesses and residents in the area as well as to some public services such as public transit and emergency vehicles (i.e. fire and police). Detours and traffic delays may occur; however interruptions to access would be minimized for all properties. These impacts would be temporary.</p>	S	<p>Mitigation Measure T-6 Construction Period Traffic Management Plan. . . A traffic handling plan will be prepared prior to construction of any roadway improvements. The plan will address traffic management during construction periods, including but not limited to road and lane closures; detours; pedestrian and bicycle routes; and public notification. The traffic handling plan should be prepared in consultation with regional transit in order to minimize disruptions to public transit service along the corridor. Additionally, prior to commencement of construction, a Traffic Management Plan as described in <i>A Traffic Management Plan Guide</i> (See Appendix A of the Final EIR) will be prepared and provided to Caltrans for review in order to address strategies needed to minimize disruption of traffic at the Interstate 80/Auburn Boulevard Interchange.</p>	LTS

Table 2-1 Summary of Impacts And Mitigation Measures (concluded)

Environmental Impacts	Level of Significance Before Mitigation	Mitigation Measures	Level of Significance After Mitigation
4.12 UTILITIES AND SERVICE SYSTEMS			
<i>Impact U-1: Impacts to Water Supply, Water Treatment, Wastewater Treatment and Storm Drain Facilities.</i>			
A. Impacts of the Specific Plan and General Plan Amendments. The proposed Specific Plan is largely urbanized with existing connections to water supply, water treatment and wastewater treatment facilities. According to the DEIR for the Citrus Heights General Plan, capacity for water supply, water treatment facilities and wastewater treatment facilities are expected to be adequate to serve the projected development buildout of the General Plan. Development proposed by the specific plan is consistent with the Citrus Heights General Plan.	LTS	No mitigation measures are required.	LTS
<i>Impact U-2: Impacts due to disruption of utilities and services during construction.</i>			
A. Impacts of the Roadway Design Improvement Projects Near-term and Future Projects. Public and private utilities are located adjacent to the roadway throughout the project corridor. Temporary impacts to utilities will occur during construction since utilities located in the existing and proposed right-of-way areas must be relocated. Effects may include interruption of services due to accidental damage or during relocation activities. Service interruption can be minimized through preconstruction and construction period coordination efforts between the project proponents and utility providers.	S	Mitigation Measure U-2 Construction Management for Utilities: The construction project management team shall coordinate with utility providers during design stages of roadway projects. The construction project management team shall undertake periodic assessments of upcoming utility and service disruptions during construction. These assessments and an identification of the service area involved shall be coordinated with utility providers and the public outreach program. The public outreach program shall ensure that advance notice of any utility or service shutdowns is extended to affected businesses and residents. Through construction management and project scheduling, all available measures shall be taken to minimize the duration of utility or service shutdowns.	LTS

CHAPTER 3.0 WRITTEN COMMENT LETTERS

Six comment letters were received on the Draft EIR during the public review period (October 12 through November 29, 2004). One additional letter was received after the public review period ended and is included here. The letters received are listed below. A copy of each letter received is provided in this chapter. Each letter has a reference number which corresponds to the response provided in Chapter 4.0.

List of Written Comments Received

Letter Number	Individual or Signatory	Affiliation	Date
Public Agencies			
1	George Booth	County of Sacramento, Department of Water Resources-Drainage & Hydrology	October 27, 2004 (email)
2	Katherine Eastham	Department of Transportation – District 3	November 22, 2004
3	Mark Morse	City of Roseville	November 23, 2004
4	Taiwo Jaiyeoba	Regional Transit	November 23, 2004
5	Peter Christensen	Sacramento Metropolitan Air Quality Management District	November 29, 2004
6	Terry Roberts	Office of Planning and Research, State Clearinghouse	November 30, 2004
7	Wendy Haggard	County Sanitation District – 1	January 14, 2005

Comment Letter 1

Ruggiero, Janet

From: Booth, George (MSA) [boothg@SacCounty.NET]
Sent: Wednesday, October 27, 2004 12:53 PM
To: Fredrickson, Craig (MSA)
Cc: Becker, Kevin; Ruggiero, Janet; Becker, Kevin
Subject: Auburn Blvd redevelopment

Craig,
The City of Citrus Heights is proposing to redevelop Auburn Blvd, from Sylvan to I-80. Please provide your comments to Kevin.

Janet,
My office has no specific comments.

George H. Booth, PE
Drainage Development and Hydrology Section
Sacramento County Department of Water Resources
(916)874-6484

This email and any attachments thereto may contain private, confidential, and privileged material for the sole use of the intended recipient. Any review, copying, or distribution of this email (or any attachments thereto) by other than the County of Sacramento or the intended recipient is strictly prohibited.

If you are not the intended recipient, please contact the sender immediately and permanently delete the original and any copies of this email and any attachments thereto.

10/27/2004

DEPARTMENT OF TRANSPORTATION

DISTRICT 3 – SACRAMENTO AREA OFFICE

VENTURE OAKS, MS 15

P. O. BOX 942874

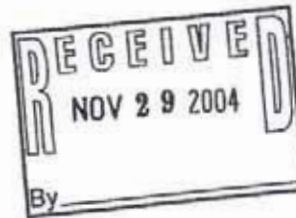
SACRAMENTO, CA 94274-0001

PHONE (916) 274-0614

FAX (916) 274-0648

TTY (530) 741-4509

Comment Letter 2

*Flex your power!
Be energy efficient!*

November 22, 2004

04SAC0148

03- SAC-80 P.M. 17.910

The Boulevard Plan/Auburn Blvd. Specific Plan

DEIR

SCH#2003062165

Ms. Janet Ruggiero
City of Citrus Heights
6237 Fountain Square Drive
Citrus Heights, CA 95621

Dear Ms. Ruggiero:

Thank you for the opportunity to review and comment on the Auburn Boulevard Corridor Specific Plan project. Our comments are as follows:

- Although there may be no direct impacts to Interstate 80 as a result of the implementation of this project, Caltrans should be provided with project specifics regarding the construction period and traffic mitigation strategies (ie. night operations, lane closures during off-peak traffic periods, etc.) to avert any potential traffic disruption problems or possible ramp queuing congestion at the Interstate 80/Auburn Boulevard Interchange. Please address Caltrans' interchange traffic concerns when drafting Mitigation Measure T-6 to include Interstate 80/Auburn Boulevard Interchange mitigation, as well as City street and road closures and property access mitigation along the Auburn Boulevard corridor. A Traffic Management Plan Guide is provided for reference. Please provide a copy of the Traffic Management Plan for our review.

2-1

If you have any questions regarding these comments, please contact Ken Champion at (916) 274-0615.

Sincerely,

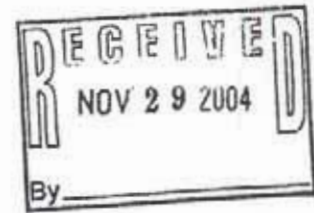
KATHERINE EASTHAM, Chief
Office of Transportation Planning - Southwest

c: Scott Morgan, State Clearinghouse

"Caltrans improves mobility across California"



Comment Letter 3



Community Development

311 Vernon Street
Roseville, California 95678-2649

November 23, 2004

City of Citrus Heights
6237 Fountain Square Drive
Citrus Heights, CA 95621-5577
Attn: Janet M. Ruggiero, CDD Director

Via: Fax and Regular Mail

Subject: Draft EIR for the Auburn Boulevard Specific Plan (ABSP) and Road Design Project

Dear Ms. Ruggiero:

I have reviewed the Draft EIR for The Boulevard Plan (Auburn Boulevard Corridor) and have the following comments.

The EIR should analyze the impacts to the intersections of Auburn Boulevard/Orlando Avenue and Cirby Way/Riverside Avenue using the City of Roseville Buildout/2020 Travel Demand Forecast Model.

3-1

On page 4.11-4, the DEIR refers to Figure 4.11-1 showing traffic volumes along Auburn Boulevard but there is no Figure 4.11-1. Please provide a copy of Figure 4.11-1 for our review.

3-2

Thank you for your consideration of these comments. If you have any questions regarding this letter please contact Scott Gandler, in the Engineering Department, at 746-1300.

Sincerely,

Mark Morse
Environmental Coordinator

cc: Rob Jensen, Roseville Public Works Director



Sacramento Regional
Transit District
A Public Transit Agency
and Equal Opportunity Employer

Mailing Address:
P.O. Box 2110
Sacramento, CA 95812-2110

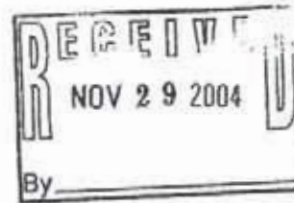
Administrative Office:
1400 29th Street
Sacramento, CA 95816
(916) 321-2800
29th St. Light Rail Station/
Bus 36, 38, 50, 57, 68

Light Rail Office:
2700 Academy Way
Sacramento, CA 95815
(916) 648-8400

Public Transit Since 1973

www.sacrt.com

Comment Letter 4



November 23, 2004

Janet Ruggiero
Director of Community Development
City of Citrus Heights,
6237 Fountain Square Drive
Citrus Heights, CA 95621

NAME OF DEVELOPMENT: The Boulevard Plan Project

TYPE OF DOCUMENT: Draft Environmental Impact Report (DEIR)

Regional Transit (RT) staff has reviewed the DEIR for re-inventing the Auburn Boulevard Corridor (The Boulevard Plan) and would like to provide the following comments/recommendations:

The proposed corridor improvement covers a portion of Auburn Boulevard between Sylvan Corner and I-80. The project will improve an area along the Auburn Boulevard Corridor that totals approximately 460 acres focusing on parcels, commercial and residential uses that front on Auburn Boulevard. The goal is to improve the image of the corridor, and improve its function as a transportation facility serving adjacent land uses and better connectivity with other areas in the region.

RT supports the proposed improvements to the Auburn Boulevard corridor. Improvements to this corridor will provide an enhancement to this major transit corridor within the City of Citrus Heights. RT provides services along Auburn Boulevard with bus routes 91, 93, 95 and 103 traveling at regular frequencies within this portion of the corridor.

In order to further enhance the transit supportiveness of this Plan and to minimize future impacts on transit, regardless of the roadway alternatives considered, the following recommendations are proposed for further analysis in the DEIR:

1. Evaluate the potential impact on transit movements along Auburn Boulevard and develop appropriate mitigation plan for the impacts. The DEIR indicates that "construction activities would result in temporary disruption to connectivity by requiring detours for pedestrians and bicyclists, and traffic congestion" (Page 2-28). RT is concerned about impact of construction activities on existing transit services along this corridor.

November 23, 2004

A significant impact on transit operations occurs if the project construction would result in lane closure (even temporary) or cause a delay to bus transit services within the affected area.

RT's goal is to provide dependable and on-time service for our patrons and delays caused by construction will make it difficult to adhere to a timed schedule. Please contact Al Schweim, RT's Director of Transportation or Dennis Jones, Transportation Superintendent at 321-2800 for further information regarding the short-term disruption of traffic and delays.

2. Although the DEIR did not identify specific "noise barriers" to mitigate the impact of noise on nearby residents, RT would like to recommend against soundwalls as a type of noise barrier should the discussion arise in future. Physical barriers such as soundwalls impede access to transit and may increase the distance pedestrians have to walk in order to access transit services.

Thank you for the opportunity to review this project. If you have further questions regarding these recommendations, please contact me at (916) 321-2870 or tjaiyeoba@sacrt.com.

Sincerely,



Taiwo Jaiyeoba
Real Estate Administrator/Transit Oriented Development

c: Fred Arnold, Director of Real Estate, RT
Al Schweim, Director of Transportation, RT

Comment Letter 5

Larry Greene
AIR POLLUTION CONTROL OFFICER

29 November 2004

Janet M. Riggiero
City of Citrus Heights
Planning Division
6237 Fountain Square Dr
Citrus Heights CA 95621

**RE: The Boulevard Plan, Reinventing the Auburn Boulevard Corridor**

Dear Ms. Ruggiero:

Thank you for the opportunity to review the draft Environmental Impact Report for "The Boulevard Plan, Reinventing the Auburn Boulevard Corridor". District staff is encouraged by the focus on creating districts that enhance quality environments for all modes of travel, especially bicycling and walking. By combining better travel options with new opportunities for mixed infill development with a residential component, the City is contributing to the regional effort to accommodate growth in a manner that can help to minimize emissions.

We offer the following specific comment on the draft EIR:

Section 4.3.6.1 (Page 4.3-18) describes impact AQ-1 related to construction impacts of the Specific Plan. Construction of the Specific Plan is identified as a significant impact, although quantified emission impacts are not identified because the "amount and timing of construction activities associated with the Specific Plan are not known." In order to mitigate the significant impact, the SMAQMD standard NOx construction mitigation is included.

SMAQMD recommends construction NOx mitigation only in cases where quantified impacts exceed the adopted threshold of significance (85 lbs/day). Applying the mitigation to smaller construction activities is not intended because of the limited amount of off-road equipment associated with smaller projects. As described in the draft EIR, the Specific Plan provides a framework for redevelopment of the corridor over time. It is likely that most construction activities will be relatively small, such as construction of a parcel or group of parcels on an individual basis. Therefore, we recommend that the construction NOx mitigation be removed as required mitigation. We would recommend that any contractor with off-road equipment working on Specific Plan projects contact SMAQMD to investigate financial incentive opportunities for reducing emissions from construction equipment, but specific emission reduction actions should not

5-1

be mandated. The mitigation related to particulate matter and asbestos is appropriate and should be retained.

Thank you for the opportunity to review The Boulevard Plan documents. If you have any questions regarding these comments, please contact me at 916.874.4886.

Sincerely,



Peter Christensen
Mobile Source Division

SAC200400263



Arnold
Schwarzenegger
Governor

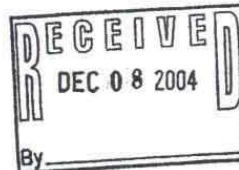
Comment Letter 6

STATE OF CALIFORNIA
Governor's Office of Planning and Research
State Clearinghouse and Planning Unit



Jan Boel
Acting Director

November 30, 2004



Janet M. Ruggiero
City of Citrus Heights
6237 Fountain Square Drive
Citrus Heights, CA 95621-5577

Subject: Boulevard Plan, A Specific Plan for the Auburn Boulevard Corridor from Sylvan Corners to the City Limits, Citrus Heights
SCH#: 2003062165

Dear Janet M. Ruggiero:

The State Clearinghouse submitted the above named Draft EIR to selected state agencies for review. On the enclosed Document Details Report please note that the Clearinghouse has listed the state agencies that reviewed your document. The review period closed on November 29, 2004, and the comments from the responding agency (ies) is (are) enclosed. If this comment package is not in order, please notify the State Clearinghouse immediately. Please refer to the project's ten-digit State Clearinghouse number in future correspondence so that we may respond promptly.

Please note that Section 21104(c) of the California Public Resources Code states that:

"A responsible or other public agency shall only make substantive comments regarding those activities involved in a project which are within an area of expertise of the agency or which are required to be carried out or approved by the agency. Those comments shall be supported by specific documentation."

These comments are forwarded for use in preparing your final environmental document. Should you need more information or clarification of the enclosed comments, we recommend that you contact the commenting agency directly.

This letter acknowledges that you have complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to the California Environmental Quality Act. Please contact the State Clearinghouse at (916) 445-0613 if you have any questions regarding the environmental review process.

Sincerely,

Terry Roberts

Terry Roberts
Director, State Clearinghouse

Enclosures
cc: Resources Agency

1400 TENTH STREET P.O. BOX 3044 SACRAMENTO, CALIFORNIA 95812-3044
TEL (916) 445-0613 FAX (916) 323-3018 www.opr.ca.gov



Comment Letter 7

January 14, 2005
E225.000

10545 Armstrong Avenue

Mather

California

95655

Tele: [916] 876-6000

Fax: [916] 876-6160

www.csd-1.com

Board of Directors

County of Sacramento

Roger Dickinson

Illa Collin

Muriel E Johnson

Roger Niello

Don Nottoli

City of Citrus Heights

Jeannie Bruins

City of Elk Grove

Sophia Scherman

City of Folsom

Kerri Howell

City of Rancho Cordova

Dan Skoglund

City of Sacramento

Heather Fargo

Cheryl Creson

Agency Administrator

Robert E Shanks

District Engineer

Marcia Maurer

Chief Financial Officer

Wendell H. Kido

District Manager

Mary K. Snyder

Collection Systems Manager

Janet M. Ruggiero
City of Citrus Heights
6237 Fountain Square Drive
Citrus Heights, CA 95621-5577

Dear Ms. Ruggiero:

Subject: The Boulevard Plan-General Planning Document EIR

Both the County Sanitation District 1 (CSD-1) and the Sacramento Regional County Sanitation District (SRCSD) reviewed the subject documents and have the following comments.

CSD-1 and SRCSD have made some changes, clarifications and additions to some of our comments since our last response letter concerning the Notice Of Preparation dated July 11, 2003. These changes are reflected below and serve as amendments to comments sent on said date. (A copy is attached for your ready reference). Please refer to that letter regarding the following comments.

The two trunk-line relief projects discussed in paragraph one on page two are no longer included in our current planning for capacity increases, due to cost or design complexity issues. This will not impede the ability of the district to serve the area.

7-1

The District is still concerned that installation of new surface infrastructure such as roadway medians and channelizations, streetscape elements, street trees etc. could impact our ability to maintain sub-surface sewer line and related facilities, as discussed in paragraphs three and four on page 2.

7-2

The future construction should be carefully co-ordinated with the District to eliminate conflicts with our facilities. City designers are asked to contact the District for our standards including a list of compatible street trees and to follow these in their designs wherever possible. Wherever possible, surface elements should be kept free of our subsurface lines in case of future excavation-maintenance needs.

7-3

The District still plans on the installation of the Northwest Interceptor Line along Old Auburn Road and Auburn Road west of Sylvan Corner. This could entail extensive Right-of-Way acquisition needs. Accordingly, for this and other reasons, the District would appreciate co-ordination with the City during the formulation and establishment of the official plan line for Auburn Boulevard, as well as over all design and construction involving right-of-way facets.

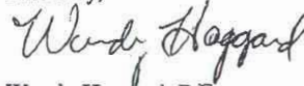
7-4

The above and our July 11, 2003 letter would cover any comments on the project at this time.

Janet M. Ruggiero
January 14, 2005
Page 2

If you have any questions regarding these comments, please call Joycc Ferguson at (916) 876-6098 or myself at (916) 876-6094.

Sincerely,



Wendy Haggard, P.E.
Department of Water Quality
Development Services

Attachment

WH/JRO: cc

cc: Maria Cablaio

ruggiero011405.ltr.doc



0545 Armstrong Avenue

ather

alifornia

15655

ele: [916] 876-6000

ax: [916] 876-6160

Website: www.srscsd.com

Board of Directors

County of Sacramento

oger Dickinson

lla Collin

aniel R. Johnson

oger Niello

Jon Nottoli

City of Sacramento

leather Fargo

City of Folsom

erri Howell

City of Citrus Heights

cannie Bruins

City of Elk Grove

lophia Scherman

Warren H. Harada

gency Administrator

Robert F. Shanks

istrict Engineer

Garcia Maurer

Chief Financial Officer

Nendell H. Kido

istrict Manager

Aary K. Snyder

ollection Systems Manager

July 11 2003

E225.000

Janet M. Ruggiero
City of Citrus Heights
6237 Fountain Square Drive
Citrus Heights, CA 95621-5577

Dear Ms. Ruggiero:

**Subject: Notice of Preparation of Environmental Impact Report
Auburn Boulevard Specific Plan (ABSP) and Road
Design Project**

County Sanitation District 1 (CSD-1) and Sacramento Regional County Sanitation District (SRCSO) reviewed the subject document and have the following comments, which should be fully studied and discussed in the Environmental Impact Report (EIR).

The project description indicates that *"commercial services, housing and mixed use infill, and/or regional commercial"* land use would be encouraged. The draft EIR will need to completely describe the land use changes that are proposed and what, if any, increase of development intensity is contemplated.

The master plans of both Districts provide for increased density and development in the project area and do expect that the proposed land use plan proposed by the City would be compatible with District programs. District staff expects that after the review of the EIR that they will be able to support the land use plan

Unfortunately, many of the needed master plan facilities are not yet in place and the sewer system is currently capacity constrained. Increases in density will aggravate the existing constrained sewer facilities serving the project area. Impacts from infill projects could be significant if they are served prior to completion of facilities that will increase capacity. Current development within the plan area will continue to be served by the existing system.

SRCSO constructs and maintains most sanitary sewer facilities for flows that exceed 10 mgd. Currently, the existing facilities are nearing capacity and additional pipe capacity must be developed. SRCSO is currently underway with construction and design of the Lower Northwest and Upper Northwest Interceptors. All sections of these interceptors are funded and completion is currently scheduled for 2007, although a variety of unforeseeable factors may cause the date to be revised. The completion of Lower and Upper Northwest Interceptors will provide the additional interceptor capacity needed to serve the City of Citrus Heights in general and this project specifically.

Janet M. Ruggiero
July 11, 2003
Page 2

CSD-1 is responsible to collect flows from individual customers and collect them for transfer to SRCSD. To this end, CSD-1 maintains a trunk system for flows of over 1 mgd. Two major trunk sewer lines serve the subject project area. The southern trunk is a 27" diameter line draining westerly from Sylvan Road through the Stock Ranch Development. The northern trunk ranges from 24" to 36" in diameter and flows south on Auburn Boulevard for 1000 feet before turning westerly at Watson Way and then later heading in a southwesterly direction. The southern 27" trunk has been identified as capacity constrained and additional connections cannot be permitted at this time. However, discussions are currently underway to provide additional capacity to this trunk and such capacity would be available to serve new customers in the southern portion of the project area. If this project proceeds as is expected, capacity would be available in late 2004. The northern trunk has not been as carefully examined and it will be necessary to prepare a current study to quantify the extent of any capacity constraints that may exist on the pipe. If the northern trunk is capacity constrained, it will be necessary to develop projects that will create additional capacity prior to permitting additional connections.

CSD-1 and SRCSD believe that all impacts could be mitigated to a less than significant impact. To facilitate insignificant impacts the EIR should established mitigation measures requiring new development to analyze sewer capacity when significant flows or modifications will affect the sewer system. Mitigation measures should also require interim sewer service facilities when determined necessary by CSD-1.

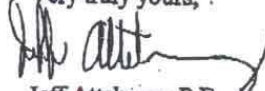
Reconstruction of Auburn Boulevard could impact existing sewer facilities in the roadway. Many of these lines are older, only 6 inches in diameter and may not be in conformance with current standards. Upsizing portions of the collector system may be warranted and/or required as redevelopment occurs in the area. Upsizing the lines will disturb sections of the newly improved road unless the line sizes are increased prior to or during construction of the subject project. The EIR should address these issues in relation to project road design improvements.

Construction of the subject project proposals may also impact sewer facilities that will remain. Mitigation measures that insure protection of the existing facilities during construction will reduce impacts to less than significant.

Finally, SRCSD plans to construct the Upper Northwest Interceptor 9 (UNWI9) section down Old Auburn Road and Auburn Boulevard through the Intersection with Sylvan Road (at Sylvan Corners). The UNWI9 will be around 36" in diameter and 23 feet deep through the Sylvan Corners intersection. Construction, anticipated in the summer of 2007, may require massive removal of street improvements in the area due to excavation and will affect the near-term portion of the subject project between Sylvan Corners and Watson Way. The EIR should explore mitigation measures that would reduce these impacts to less than significant.

If you have any questions regarding these comments, please call Joyce Ferguson at 876-6098 or myself at 876-6094.

Very truly yours,



Jeff Atteberry, P.E.
Local Sewer Engineering

JA/JF:ds

cc: Christoph Dobson
Neal B. Allen

CHAPTER 4.0 RESPONSES TO WRITTEN COMMENT LETTERS

LETTER 1: George Booth, County of Sacramento, Department of Water Resources

Comment states that this office has no comments. No response necessary.

LETTER 2: Katherine Eastham, Department of Transportation – District 3

Comment 2-1: Comment states that Caltrans should be provided with project specifics regarding construction period and traffic mitigation strategies to avert potential traffic disruption problems or possible ramp queuing congestion at the Interstate 80/Auburn Boulevard Interchange. Caltrans notes that these concerns should be addressed in Mitigation Measure T-6 of the Draft EIR. Caltrans further provides a copy of *A Traffic Management Plan Guide* and requests that Caltrans be provided with a copy of the Traffic Management Plan for review.

Response 2-1: Mitigation measure T-6 is intended to ensure that these potential problems are addressed in a “traffic handling plan”. The mitigation measure is revised to clarify that Caltrans should be provided a traffic management plan for review prior to road construction projects. *A Traffic Management Plan Guide* is included in Appendix B of the Final EIR and referenced in the revised mitigation measure. Specific details regarding specific projects’ effects on traffic congestion are not available at this time. Project specific environmental reviews for development and redevelopment projects that may have an effect on traffic on the corridor should address these potential impacts. The mitigation measure is revised in this Final EIR to clarify the need for additional coordination and review with Caltrans.

LETTER 3: Mark Morse, City of Roseville

Comment 3-1: The City of Roseville staff requested that the Draft EIR should analyze the project’s impacts to the intersections of Auburn Boulevard/Orlando Avenue and Cirby Way/Riverside Avenue using the City of Roseville’s Buildout/2020 travel demand forecasting model.

Response 3-1:

The project’s impacts were not analyzed at any City of Roseville intersections because the trip generation of the project showed only a slight increase in traffic compared to “no project” conditions. In fact, it was estimated that the project would result in a decrease in trips (compared to “no project” conditions) on the segment of Auburn Boulevard between the City of Roseville/Citrus Heights border and Antelope Road due to the lower-intensity land uses proposed for this area. Overall, the increase in trips due to the project was estimated to be approximately 4,700 daily trips, 290 AM peak hour trips, and 370 PM peak hour trips over the entire project study segment between the north city limits and Sylvan Road.

The City of Roseville is currently conducting a study to determine if Cirby Way should be widened to six lanes or remain four lanes. Various alternatives, such as providing triple left-turn lanes from northbound Riverside Avenue onto Cirby Way, are being studied with the use and development of a visual simulation model. The Specific Plan project was based on a less sophisticated, yet adequate, analysis and did not employ a visual simulation analysis. The inconsistency of analysis techniques between these two projects and the uncertainty of the improvements at the Cirby Way/Riverside Avenue intersection make the traffic analysis of this intersection for the Specific Plan project impractical. Given the project's low trip generation numbers, the level of effort required in using the more sophisticated model for the Specific Plan's traffic impacts at these intersections was not justified.

Comment 3-2: Comment notes that Figure 4.11-1 showing existing conditions traffic volumes along Auburn Boulevard is missing from the Draft EIR.

Response 3-2: The Figure, which shows traffic volumes along Auburn Boulevard under the existing conditions scenario, is provided in Chapter 7.0 of this Final EIR.

LETTER 4: Taiwo Jaiyeoba, Regional Transit

Comment 4-1: Comment notes that RT supports the proposed improvements that will provide an enhancement to the corridor, which is a major transit corridor in Citrus Heights.

Response 4-1: No response is necessary.

Comment 4-2: Comment notes that RT is concerned about impacts to transit services due to construction activities on Auburn Boulevard. The potential impact on transit should be addressed in the DEIR and appropriate mitigation measures developed.

Response 4-2: The EIR notes that construction activities would have an affect on traffic congestion and that a traffic handling plan should be put in place prior to construction. The intent of the measure is to minimize traffic congestion that would affect all forms of transportation along the corridor, including public transit. The impacts discussion and Mitigation Measure T-6 are revised to clarify this point.

Comment 4-3: The comment notes that noise barriers impede access to transit and may increase the distance pedestrians have to walk in order to reach transit services.

Response 4-3: As noted in the comment, neither the Specific Plan nor the roadway design improvement project proposes construction of sound walls at any specific location; soundwalls may be proposed as part of future development/redevelopment projects in the Specific Plan area. Additionally, the Specific Plan promotes use of landscaping, walls and fences to screen commercial parking lots and unsightly uses in commercial areas. One of the main priorities of the Specific Plan it to promote transit use and pedestrian friendly site design. The Specific Plan contains design guidelines that address pedestrian

access to transit, pedestrian safety and comfort and connectivity between commercial areas and adjacent neighborhoods. These guidelines include the following:

- Gateway District Transit Design Guidelines – Pedestrian connections to transit facilities shall be easy to navigate, safe, comfortable and friendly.
- Rusch Park District Connections to the Community –
 - New projects shall provide connections between existing and new streets
 - Concrete block sound walls are not allowed as a means to separate commercial uses from new residential areas
- Lincoln 40 District Integrating Transit –
 - New and renovated commercial projects shall be clearly connected to transit services. Sidewalks shall provide direct access to transit stops. Special considerations shall be taken into account, such as shopping cart storage near bus stops.
 - Transit stops shall be conveniently and centrally located. They shall be easy to find and co-located with commercial services and amenities.
 - Transit stops and connecting pedestrian routes shall be well lit and visible.

Future development and redevelopment projects would be evaluated for compliance with these guidelines on a case by case basis to ensure that plan priorities are being met.

LETTER 5: Peter Christensen, Sacramento Metropolitan Air Quality Management District

Comment 5-1: Sacramento Metropolitan Air Quality Management District (SMAQMD) recommends construction NOx mitigation only in cases where quantified impacts exceed the adopted threshold of significance of 85 lbs/day. It is likely that most construction activities within the Specific Plan will be relatively small; therefore it is recommended that the construction NOx mitigation be removed as required mitigation. Applying mitigation to smaller construction activities is not intended because of the limited amount of off-road equipment associated with smaller projects. The mitigation related to particulate matter and asbestos is appropriate and should be retained.

Response 5-1: The air quality mitigation measure is revised in the Final EIR to reflect this comment.

LETTER 6: Terry Roberts, Governor's Office of Planning and Research, State Clearinghouse and Planning Unit

This letter is a transmittal letter with the comment letter from the Department of Transportation (Caltrans) attached. Refer to Letter No. 2 comment and response. No further response is necessary.

**LETTER 7: Wendy Haggard, Department of Water Quality Development Services,
County Sanitation District -1**

Comment 7-1: This comment provides an update on the status of the trunk-line relief projects described in the District's response to the Notice of Preparation (see CSD-1 letter dated July 11, 2003). CSD-1 notes that these projects are no longer included in their current planning for capacity increases. CSD-1 also notes that this will not impede the ability of the district to serve the Specific Plan area.

Response 7-1: Comment is noted and this information is included in the errata section of this Final EIR.

Comment 7-2: The District is concerned that installation of new surface infrastructure such as medians, streets trees, etc. could impact its ability to maintain sub-surface sewer line and related facilities.

Response 7-2: The Draft EIR notes that temporary impacts to utilities may include interruption of services due to accidental damage or during relocation activities. Mitigation measure U-2 is provided to minimize disruptions through preconstruction and construction period coordination efforts between the project proponents and utility providers.

Comment 7-3: The District asks that future construction be coordinated with the District to eliminate conflicts with their facilities. City designers are asked to contact the District for their standards including a list of compatible street trees.

Response 7-3: Mitigation measure U-2 is modified to clarify the need for design period coordination with the district.

Comment 7-4: CSD-1 notes that the District's plans for installation of the Northwest Interceptor Line along Old Auburn Road and Auburn Road west of Sylvan Corner are still current. The District would appreciate coordination with the City on the planning for the adoption of the Auburn Boulevard Plan Line.

Response 7-4: The Plan Line proposed as part of The Boulevard Plan, Plan Line Study and Auburn Boulevard Roadway Design Improvement project only affects that portion of Auburn Boulevard north of the Sylvan Corners intersection. The Interceptor Project is located in a different segment of the Auburn Boulevard (the segment to the west of Sylvan Corners).

CHAPTER 5.0 ORAL COMMENTS AND RESPONSES

At the Planning Commission Hearing of October 28, 2004 the following members of the public spoke.

Olita J. Platt Resident - Speaker focused her comments on the Specific Plan and did not address the Draft EIR.

Response to Comments of Dave Fisher, Resident of Oak Forest Street - Speaker expressed concerns that adoption of the Specific Plan would exacerbate existing sewer problems in his neighborhood (Oak Forest Street) and concerns about traffic congestion.

Response: Sacramento Regional County Sanitation District (SRCSD) and County Sanitation District 1 (CSD-1) has provided an update on their planning for improvements to sewer infrastructure in the area (see Letter #7 and attached response to the Notice of Preparation dated July 11, 2003). SRCSD noted that the sewer interceptor system is currently capacity constrained; however, design and construction activities are underway for the Lower Northwest and Upper Northwest Interceptors. All sections of these interceptors are funded and completion is expected in 2007. The completion of these interceptors will provide the additional interceptor capacity needed to serve the City of Citrus Heights generally, including the Specific Plan project.

According to CSD-1 (personal communication Dobson 12/14/04), the sewer collector lines serving particular neighborhoods sometimes have problems due to the condition of the line, rather than capacity. This is the case with the line serving the neighborhood on Oak Forest Street, which is located in a backyard easement. Any development as a result of the approval of the Auburn Boulevard Specific Plan would most likely connect to a line in Auburn Boulevard and therefore would have no effect on the sewer line serving Oak Forest Street. In the unlikely event that development was allowed to connect to the sewer line serving Oak Forest Street, CSD-1 would require the project proponents to either demonstrate that adequate sewer capacity was available, or construct improvements to the sewer system to ensure necessary capacity.

With respect to traffic congestion, one of the main priorities of the project is to improve traffic safety. The Specific Plan would provide roadway improvements and site design that would increase safety and the capacity of the roadway.

Bob Croom Resident - Speaker focused his comments on the Specific Plan and did not address the Draft EIR.

At the Planning Commission Hearing of November 18, 2004 the following members of the public spoke.

Donald Patch Resident Mr. Patch's comments addressed the Specific Plan and did not address the Draft EIR.

Van Ford Resident

Mr. Ford expressed concern regarding traffic issues.

Response: One of the main priorities of the project is to improve traffic safety. The Draft EIR noted that the Specific Plan would provide roadway improvements and site design that would increase safety and the capacity of the roadway.

CHAPTER 6.0 REVISIONS TO THE DRAFT SPECIFIC PLAN

6.1 Introduction

This section presents revisions (Addendum and Errata) to the public review draft of The Specific Plan (July 2004) as a result of recommendations in the EIR, or recommendations that resulted from public review and Planning Commission review. It has been determined that the revisions are minor and do not constitute a significant change in the proposed project, do not present additional information that would result in the identification of new significant impacts, or a change in significance of previously identified impacts, nor in the need for additional mitigation measures. Incorporation of the Addendum and Errata into the Specific Plan would not require additional environmental analysis or recirculation of the Draft EIR.

6.2 Addendum and Errata

The following are changes to July 2004 Public Hearing Draft Boulevard Plan. Changes are noted by ~~strikeout~~ for deleted language and underline for new or revised language. The Draft Plan will undergo changes to correct non-substantive typographical errors, revise numbering sequences and figure and page numbering changes in addition to the specific changes noted below.

Section 2 Concepts, Goals, and Principles

Page 2-4.

Insert New Land Use Principle

LUP 5 Gateway District Uses

Approve uses, such as a hotel or, alternatively, mixed use office-residential or commercial-residential development (e.g., grocery store with condominiums) for the K-mart site that promotes the Gateway District as a distinctive, mixed use business address. Restrict uses such as big-box retail as well as other similar retail commercial uses that do not achieve this objective.

LUP 6. Lincoln 40 District: ~~Reinventing~~ Continuing Highway Commercial
Reinforce business opportunities for smaller parcels and related land use.

Insert New Land Use Principle

LUP 8 Grocery Store

Recruit a grocery store for the southwest corner of Auburn Boulevard and Antelope Road in the Lincoln 40 District, on the “main street” of the Rusch Park Village Center, or as part of a mixed-use development at the K-mart site in the Gateway District Commercial Center.

LUP 10. Long-Term Vision

Require new uses to reflect the City's long-term vision for the Boulevard, while ensuring that existing uses are not displaced in the near term by planning or regulatory activities, including allowing upgrading and minor remodeling of existing non-conforming uses.

Page 2-5.

Insert New Land Use Principle

LUP 15 Mitigation Measures

Implement the environmental mitigation measures identified in Appendix 9.

2.3.1 Circulation Concepts

Figure 2.2 shows the major circulation concepts for the Auburn Boulevard corridor, including the location of permitted turning movements, enhanced transit stops, enhancement and repair of local side streets, existing and future traffic signals for both automobiles and pedestrians, potential roadway realignments, parcels that would likely generate considerable pedestrian activity, and private streets. Figure 2.3 addresses the broader circulation context by showing the connectivity of the Boulevard to Interstate 80, major thoroughfares, and local school sites. The following summarizes the circulation concepts for the four districts.

Page 2-8.

~~CP 7. Bus Turn-Outs~~ Enhanced Transit Stops

Install bus turn outs and/or shelters at every bus stop along Auburn Boulevard with the configuration determined by the General Services Department based on site constraints.

Insert New Circulation Principle

CP 8 Sidestreet Improvements

Enhance existing local side streets through street repairs, connecting sidewalks, grading, and landscaping.

Page 2-13

CDP 9. Undergrounding Utilities

Remove overhead transmission lines, light poles, and other utilities along the entire Boulevard ~~that are unsightly and limit the possibility of adding trees and landscaping.~~

Page 2-14.

CDP 11. Signs and Billboards

Remove unsightly commercial signs such as ~~billboards~~, illegal A-frames and signs in disrepair that contribute to the visual clutter along the Boulevard. Limit A-frame signs to one for each parcel.

Insert New Community Design Principle

CDP 12. Billboards

Develop a process for the removal or relocation of billboards along the Boulevard.

CDP 14. Facade Improvements

Upgrade the facades of aging commercial buildings that are ~~outdated and~~ showing signs of blight.

Page 2-18

Insert New Streetscape Principle

SP4 Sound Walls

Plant climbing vines at the base of all existing sound walls and new sound walls to soften their visual impact.

Section 3 Development Standards

Table 3.5 Building Setbacks

Setbacks	Gateway District		Rusch Park District		Lincoln 40 District	Sylvan Corners Village Square District
	Gateway Commercial Center	Outside Gateway Commercial Center	Rusch Park Village Center	Outside Rusch Park Village Center		
Auburn Boulevard frontage	5' setback for commercial and residential mixed-use (required) ; 20' setback for residential uses (required)	5' setback for commercial and residential mixed-use (required) ; 20' setback for residential uses (required)	5' setback for commercial and residential mixed-use (required) ; 20' setback for residential uses (required)	5' setback for commercial and residential mixed-use (required) ; 20' setback for residential uses (required)	5' -15' setback for commercial (required) ;	5' setback for commercial (required)
Side street frontage	5' setback for commercial and residential mixed-use (required) ; 20' setback for residential uses (required)	5' setback for commercial and residential mixed-use (required) ; 20' setback for residential uses (required)	5' setback for commercial and residential mixed-use (required) ; 20' setback for residential uses (required)	5' setback for commercial and residential mixed-use (required) ; 20' setback for residential uses (required)	5'-15' setback for commercial (required) ;	5' setback for commercial (required) ;
Transition to existing residential	20' setback minimum for commercial and residential uses	1 story-10' 2 story-15' setback minimum for commercial and residential uses 20' setback minimum (1)	20' setback minimum for commercial and residential uses	1 story-10' 2 story-15' setback minimum for commercial and residential uses 20' setback minimum (1)	20' setback minimum	20' setback minimum

Table 3.6 Allowable Heights and FAR

Setbacks	Gateway District		Rusch Park District		Lincoln 40 District	Sylvan Corners Village Square District
	Gateway Commercial Center	Outside Gateway Commercial Center	Rusch Park Village Center	Outside Rusch Park Village Center		
Auburn Boulevard frontage	4 stories 50'	3 stories 40'	4 3 stories 40' 50'	3 stories 40'	2 stories 30'	2 stories 30'
Side street frontage	3 stories 40'	2 stories 30'	3 stories 40'	2 stories 30'	2 stories 30'	2 stories 30'
Transition to existing residential	3 stories 40'	2 stories 30'	2 stories 30'	2 stories 30'	2 stories 30'	2 stories 30'
Maximum Floor Area Ratio (FAR)	Commercial 0.60 Residential 0.50	Commercial 0.60 Residential 0.50	Commercial 0.60 Residential 0.50	Commercial 0.60 Residential 0.50	Commercial 0.60 Residential 0.50	Commercial 0.60 Residential 0.50

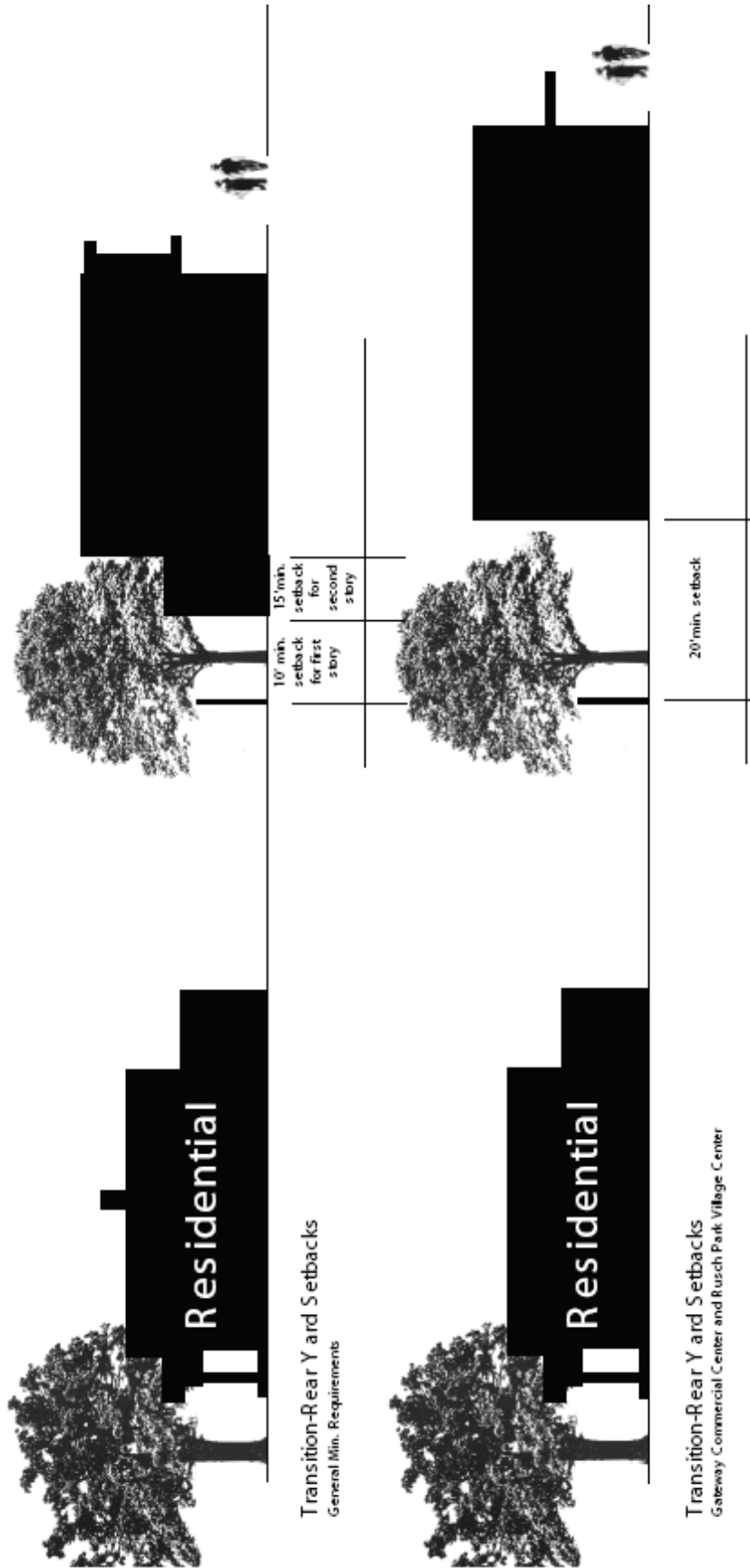
Parking

One of the objectives of The Boulevard Plan is to change the relationship between transportation patterns and development standards along the Auburn Boulevard corridor. Requiring high amounts of on-site parking can result in excessive paving, increase housing and development costs, and produce an oversupply of parking spaces that can go unused for much of the year. The parking standards in this Plan allow for greater flexibility in the provision of parking and reduce the negative impacts of excessive parking. Where this Plan is silent, the parking standards in the Citrus Heights Zoning Ordinance shall apply.

- Provide opportunities for shared use parking agreements between adjacent parcels when cross access easements are used and driveways are closed or when office buildings are located adjacent to entertainment-oriented uses (e.g., cinema, restaurants). Allow, with a minor parking permit, for a parking reduction of 25 percent to reflect uses that have complementary demand patterns.
- Provide opportunities for on-street parking on interior commercial streets in the Rusch Park Village Center and Gateway Commercial Center.
- Prohibit on-street parking on Auburn Boulevard.
- Require wheel stops to prevent overhang of parked vehicles that may encroach into the sidewalk or planting strip.
- Require employee parking to the rear of businesses where appropriate to provide adequate parking for patrons and visitors.

- Require pedestrian walkways within parking lots (down middle of parking block) plus crosswalks for larger commercial lots (e.g., within Rusch Park Village Center or Gateway Commercial Center).
- Encourage parking layouts that facilitate egress onto side streets where appropriate instead of on to Auburn Boulevard.
- Allow a up to a 15 percent reduction in parking standards for uses that provide transit support facilities such as bike lockers, shower facilities, etc.
- Allow on-street guest parking for multi-family residential areas in the Rusch Park Village Center.
- For small lot parcels, allow the following exceptions to the parking standards in the Zoning Ordinance:
 - Allow up to 50 percent of all parking to consist of compact parking stalls (8 foot width and 16 foot length).
 - Utilize angled parking stalls with one-way drive aisles in lieu of 90-degree spaces where appropriate.
 - Encourage cross parcel access easements to minimize driveway openings and increase parking on-site.
 - Reduce the sidewalk width from 5 feet to 4 feet and planter width from 6 feet to 4 feet to increase on-site parking.
 - Require cross parcel access easements, where appropriate, to minimize driveway openings, allowing for increased parking on-site.
 - Require a minimum parking aisle width of 18 feet for turn-around space for small lots.
 - Allow landscape to count for a portion of the required on-site landscaping.

**FOLLOWING PAGES: FIGURES ILLUSTRATING BUILDING SETBACKS
AND HEIGHT STANDARDS TO BE INSERTED FOLLOWING FIGURES 3.5
AND 3.6 (DRAFT SPECIFIC PLAN PAGES 3-10 AND 3-11).**



**Section 4 Design Guidelines
Signage Standards**
Pages 4-10, 4-20, 4-32, 4-42

Signage Standards					
<u>District</u>	<u>Objective</u>	<u>District Signage</u>	<u>Site Signage</u>	<u>Building Signage</u>	<u>Tenant Signage</u>
<u>Gateway District</u>	Signage concepts for a <u>mixed-use</u> storefront district	Citrus Heights gateway sign, streetlight banners (1)	NA	AREA: 1 SF per 5 LF of building appear on one side	One window sign 4 SF max. One blade sign 4 SF max.
<u>Rusch Park District</u>	Signage concepts for multi-tenant commercial buildings	District entry signs, streetlight banners, wayfinding signage (1)	NA	AREA: 1 SF per 1 LF of storefront HEIGHT: -36" for major tenants (3) -18" for minor tenants	Blades signs 6 SF max. Window signs 4 SF max. (2)
<u>Lincoln 40 District</u>	Signage concepts for single and multi-tenant commercial buildings	District banners and directional signage (1)	MULTI-TENANT Monument signs allowed (2) (3) SINGLE TENANT BUILDINGS Monument signs allowed (2) (3)	MULTI-TENANT AREA: 1 SF per 1 LF of storefront HEIGHT: -36" for major tenants (3) -18" for minor tenants SINGLE TENANT BUILDINGS AREA: 1 SF per 1 LF of storefront HEIGHT: -36" max. -24" max. for buildings under 3,500 SF	MULTI-TENANT Blades signs 6 SF max. Window signs 4 SF max. (2) SINGLE TENANT BUILDINGS (2)

<i>Signage Standards</i>					
<u>District</u>	<u>Objective</u>	<u>District Signage</u>	<u>Site Signage</u>	<u>Building Signage</u>	<u>Tenant Signage</u>
<u>Sylvan Corners Village District</u>	Signage concepts that support uniform standard for Village Square identity	Public art monuments, entry drive signs, banners (1)	NA	Pads over 3,000 SF AREA: 30 SF HEIGHT: 36" max. Pads under 3,000 SF AREA: 16 SF HEIGHT: 24" max.	Anchors over 20,000 SF (3) AREA: 48 SF HEIGHT: 48" Shops over 6,000 SF AREA: 30 SF HEIGHT: 36" Shops under 6,000 SF AREA: 30" HEIGHT: 24 SF Shops under 3,000 SF AREA: 24" HEIGHT: 16 SF

Notes:

- (1) Coordinated with City of Citrus Heights
- (2) Signage counts against cumulative allowable building signage area
- (3) Reviewed on case by case basis

Section 4 Design Guidelines

Page 4-10

Gateway District Screen Walls and Security Fences

When walls or fences are required, they shall be designed as an extension of architectural and landscape design concepts.

- Screen walls shall be architecturally treated as an extension of the building. They shall be architectural concrete block, use a cement plaster finish, or otherwise reflect the design and materials of the building. Vertical and horizontal reveals, accents, and other details shall be included.
- Screen walls along pedestrian routes or sidewalks shall be set back to allow for landscaping.
- Chain link fencing is not allowed. Razor wire or barbed wire is not allowed.
- Service and loading dock areas shall not be placed in visually prominent locations. They shall be screened from view.
- Planting of climbing vines or ivy on fences and soundwalls is encouraged.
 - Soundwalls and fences shall be limited to 8 feet in height.

Page 4-14

Connections to the Community in the Rusch Park District

New projects shall be developed as an integral part of the surrounding community.

- Gateways and edges of the district shall promote landscape and street improvements as common amenities that link adjacent neighborhoods. No part of the district shall be gated or distinguished as an enclave.
- New projects shall provide connections between existing and new streets.
- New commercial projects and retrofit of existing buildings shall be designed so that service vehicle access maintains the pedestrian friendliness of the street.
- Concrete block sound walls are not allowed as a means to separate commercial uses from new residential uses.

Page 4-20

Rusch Park District Walls and Fences

When walls or fences are needed, they shall be designed as an extension of architectural and landscape design concepts.

- Screen walls shall be architecturally treated as an extension of the building. They shall be architectural concrete block, use a cement plaster finish, or otherwise reflect the design and materials of the building. Vertical and horizontal reveals, accents, and other details shall be included.
- Screen walls along pedestrian routes or sidewalks shall be set back to allow for landscaping.
- Chain link fencing is not allowed. Razor wire or barbed wire is not allowed.
- Planting of climbing vines or ivy on fences and soundwalls is encouraged.
- Soundwalls and fences shall be limited to 8 feet in height.

Page 4-27

Lincoln 40 District Transition to Residential Areas

New and renovated projects shall be designed to enhance adjacent residential neighborhoods. Projects shall be designed to reduce the visual, noise and use impacts on adjacent residential areas. ~~Screen walls are generally regarded as mitigation for poor site planning.~~

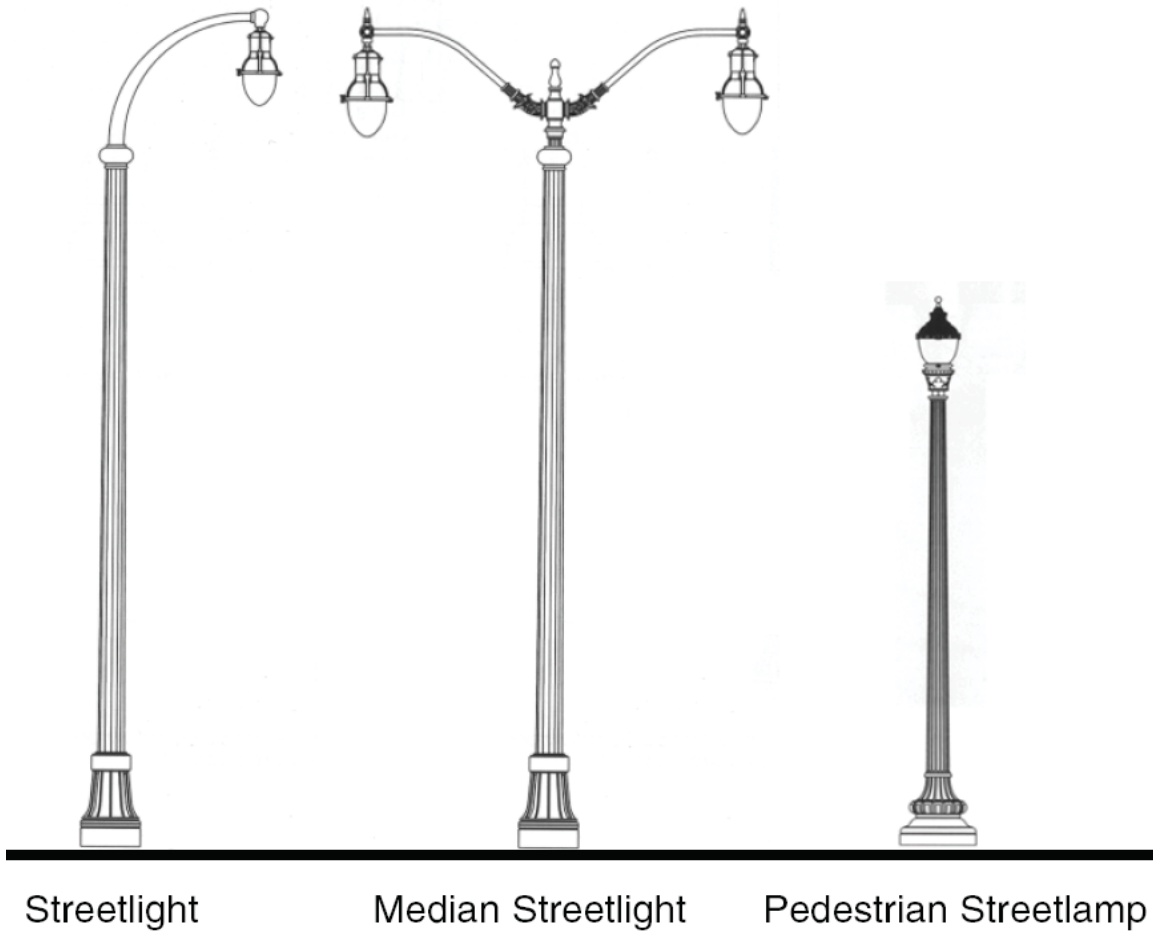
- New and renovated commercial projects shall enhance the connections to Auburn Boulevard. They shall provide streetscape, sidewalks, building setback and storefront design that link residential streets to Auburn Boulevard. Residents should be able to walk a direct route from their homes to commercial center stores without traversing parking lots.
- New and renovated commercial projects shall provide a site plan that supports the design and pedestrian access objectives for contiguous residential streets.
- New projects shall respect the scale and proximity of adjacent residential neighborhoods by reducing building height, increasing setbacks, and providing a more friendly building orientation.
- Placing loading and service areas adjacent to residential areas is discouraged.

Site circulation routes and loading areas shall be screened and set back from residential areas.

Where screening walls are required, they shall:

- be designed as a natural extension of the architectural and landscaping concepts for the project. They shall be architectural concrete block, use a cement plaster finish, or otherwise reflect the design and materials of the surrounding buildings. Vertical and horizontal reveals, accents, and other details shall be included.
- have climbing vines or ivy planted along the wall base;
- be constructed of permanent, durable, and high quality building materials;
- restricted to 8 feet in height;

Section 4 Design Guidelines



Section 5 Implementation

Page 5-5

5.4.1 Development Standards and Project Review

The City of Citrus Heights will use the development standards described in Section 3 of this Plan and the project review process as a primary tool to implement The Boulevard Plan. The development standards and the project review process are designed to ensure that individual projects comply with the directives of the Citrus Heights General Plan and this Plan. This is accomplished through determination of compliance with City standards and guidelines and conditions of approval. The Boulevard Plan will be implemented through development standards that are unique to Auburn Boulevard. In addition, the City will require that the mitigation measures identified in Appendix 9 be made conditions of approval for all projects within The Boulevard Plan Planning Area.

The area within the Auburn Boulevard corridor designated General Commercial in the General Plan Land Use Diagram will be zoned Auburn Boulevard Commercial (ABC). The development standards (allowable uses, heights, and setbacks) for the ABC District are set out in Section 3 of this Specific Plan and are referenced but not repeated in the Citrus Height's Zoning Ordinance.

Page 5-7

5.4.6 Undergrounding

The City will apply for federal funding and work with local private and public utility companies to pay for a portion or all of the undergrounding costs. If the City cannot obtain all of the necessary funding, the City will establish an undergrounding district. The City will require property owners to sign an undergrounding district agreement where each property owner is required to pay their pro-rata share of the costs. The property owners share will depend on the financial resources provided by utility companies and/or the federal government. To reduce the overall undergrounding costs, the City will attempt to coordinate the undergrounding efforts with those of the roadway construction.

Page 5-20

5.8 Follow-up Studies and Plans

There are several sites along Auburn Boulevard that will require further study to determine their feasibility for future development. The two catalyst sites (Gateway Commercial Center and Rusch Park Village Center) will require a pro forma analysis to assist in developing a financing strategy for the sites. The pro forma shall be carried out during Phase 1. Should there be development interest along Cripple Creek or at southwest corner of Auburn Boulevard and Antelope Road, the City should consider preparing a development feasibility study for both sites. The City will also need to prepare a detailed implementation plan for each district to identify staffing

levels for technical assistance and coordination, implementation priorities, and allocating funds to finance improvements and development projects.

The City shall prepare the following studies/plans:

- Pro Forma Analysis for Catalyst Sites (Gateway Commercial Center and Rusch Park Village Center)
- Development Feasibility Study of Parcels along Cripple Creek
- Development Feasibility Study for Southwest Corner of Auburn Boulevard and Antelope Road
- Implementation plan for each district.

Appendix 9

New Appendix 9

1. Biology – Protection of Cripple Creek Aquatic Habitat.

The following measures shall be made conditions of approval for all projects within The Specific Plan Area.

Mitigation Measure B-1A Protect Cripple Creek's Aquatic Life: To protect Cripple Creek's terrestrial and aquatic wildlife and special status species, and to avoid encroachments within the creek's floodplain, a Floodplain/Habitat Buffer (Buffer) should be established on both sides of Cripple Creek. Ownership and management of the Buffer should be consistent with implementing plans that fulfill the goals and policies of the City of Citrus Heights General Plan. Development in the vicinity of Cripple Creek shall be consistent with the City's administrative policies and procedures for drainage and development, and objectives of the Arcade Creek Watershed Plan, so that individual property owners are not free to undertake vegetation clearing, bank protection, soil disturbance, or creation of fences or structures within the Buffer. The width of the buffer shall be the 100-year floodplain boundary, as defined by best available data (County hydraulic studies, FIRM and other flood data).

2. Biology – Protection of Native Oak Trees and Consistency with Citrus Heights General Plan.

Mitigation Measure B-2A Minimize Impacts to Oaks:

To ensure consistency with the City of Citrus Heights' Policy 37.1, which requires incorporation of existing trees into development projects, building envelopes for future development projects should be configured to minimize impacts to trees to the extent feasible. The following measures shall be implemented:

1. Building envelopes should be established on plans and specifications for the future development projects to designate the area needed for construction of roads, driveways, and building pads.

2. These building envelopes should be large enough to include not only the proposed improvements, but also work areas for heavy equipment, staging areas, and equipment and material lay down areas.

3. To protect trees elsewhere on construction sites, no construction activities or use of heavy equipment should occur outside of the building envelopes.

4. Oaks that fall within the building envelope but which are not slated for removal should be protected by the following measures, which should be implemented during all construction phases of the project:

a) Plans and specifications should clearly state protection procedures for oaks to be preserved on the project site. The specifications should also require contractors to stay within designated work areas and should include a provision for penalties if oak trees are damaged;

b) No vehicles, construction equipment, mobile offices, or materials should be parked or located within the driplines of oaks and other trees that are to be preserved;

c) Soil surface removal should not occur within the driplines of oaks to be preserved. No cuts or trenching should occur within the dripline. If this area cannot be avoided, then the tree should be added to the list of oaks to be replaced through an on-site planting;

d) Earthen fill should not be placed within the driplines of oak trees to be retained, and no fill should be placed within five feet of their trunks, except for those trees marked for mitigation;

e) Paving should not be placed within the dripline of oaks to be retained;

f) Underground utility line trenching should not occur within the driplines of oaks to be retained. If it is absolutely necessary to install underground utilities within the driplines of oak trees, the trench should either be bored or drilled but not within five feet of the trunk and a certified arborist should be retained to monitor this construction and repair or wrap any damaged roots.

g) Living Among the Oaks: A Management Guide for Landowners (UC Cooperative Extension, Berkeley) should be used by the City as a guide in reviewing landscape plans. The information should be distributed to landowners and developers to provide information and guidelines for preparing landscape plans and for protecting oaks after construction is complete.

Mitigation Measure B-2B Prepare and Implement Oak Replacement and Management Plan (Oak Woodland Replacement): In order to compensate for impacts due to removal of native oak trees found within oak woodland and/or riparian habitats (as opposed to isolated landscape or street trees), the following measures shall be implemented:

1. Oak trees shall be planted on project sites or off-site in numbers and species composition similar to those impacted.
2. Prior to approval of development or redevelopment projects, a qualified biologist or arborist should make an accurate count of the number, diameter, and species of trees that would be removed within each building envelope or area subject to disturbance. Based on the estimate, an Oak Replacement and Management Plan (Oak Plan) should be prepared in accordance with the City of Citrus Heights Tree Ordinance, and consistent with the City of Citrus Heights General Plan biological resource goals and policies.
 - a. The goals of the Oak Plan should be to replace trees lost by the project to create healthy, self-sustaining habitats that are not dependent on maintenance or irrigation following the minimum maintenance period. The functions and values of the created habitat should approximate those of the affected habitats, i.e., the functions and values of oak woodland rather than an ornamental landscape planting.
 - b. At a minimum, the Oak Plan should include clear success criteria, monitoring and reporting requirements, and a contingency plan should the responsible parties fail to meet the success criteria that ensure that mitigation goals and ratios are met. The Oak Plan should also include details for the species, size of plants and quantities, planting techniques, techniques for protecting the trees from herbivory, and irrigation, weed control and maintenance plan, and monitoring requirements.

Mitigation Measure B-2C Prepare and Implement Oak Replacement and Maintenance Plan (Landscape Tree Replacement): In order to compensate for impacts due to removal of native oak trees found within landscape settings (i.e. isolated landscape or street trees), the following measures shall be implemented:

1. Oaks trees shall be planted on project sites or off-site in numbers and species composition similar to those impacted; or fees may be payed in lieu into the City's oak tree preservation fund. Payment in lieu of replacement should only be accepted if it is infeasible to replace trees.
2. Prior to approval of development or redevelopment projects, a qualified biologist or arborist should make an accurate count of the number, diameter, and species of trees that would be removed within each building envelope or area subject to disturbance. Based on the estimate, an Oak Replacement and Maintenance Plan (Oak Plan) should be prepared

in accordance with the City of Citrus Heights Tree Ordinance, and consistent with the City of Citrus Heights General Plan biological resource goals and policies.

a. The goals of the Oak Plan, at a minimum, should be to replace trees lost by the project in an appropriate landscape setting that will allow trees to thrive and be self-sustaining and not dependent on maintenance or irrigation following the minimum maintenance period. Replacement within the specific plan area's planned landscape areas as street trees, trees for public space landscape or roadway medians, should be emphasized when identifying replanting sites.

b. Replacement in a natural habitat setting as described in Measure B-2B would also accomplish these oak tree replacement goals.

3. Hydrology and Water Quality - These should be included in development standards.

Mitigation Measure H-1: Incorporate Development Standards for Improving Water Quality: The City shall incorporate water quality protection measures into The Boulevard Plan Development Standards: The standards may include but are not limited to the following:

1. Install and maintain landscaping that requires minimal application of chemical fertilizers, pesticides and herbicides;
2. Emphasize xeriscape landscaping that reduces the need for irrigation by minimizing the use of turf in decorative landscaping, using plant materials adapted to local conditions and efficient irrigation;
3. Minimize irrigation overspray - do not permit use of sprinkler and spray irrigation in areas less than 8 feet wide;
4. Use of drip irrigation systems where feasible;
5. Incorporate features such as filtration strips or bioswales in site design to prevent urban pollutants from entering into Cripple Creek via storm drains from parking lots and paved surfaces.

4. Noise – this measure should be implemented during site plan review.

Mitigation Measure N-2 On-site Noise Control: To ensure mitigation of noise due to project-related systems, loading docks and on-site traffic, development proposals shall be reviewed to identify potential noise conflicts with existing or proposed noise sensitive uses. Implementation of the noise standards contained in the Noise Element of the Citrus Heights General Plan will mitigate project-related noise to an insignificant level. For developments requiring installation of large, ground-mounted HVAC systems, development review shall include an assessment of noise impacts on nearby residential areas.

Revised Maps

Figure 1.2 Planning Areas and Figure 3.3 Zoning Districts Map have been revised. These maps follow this page.

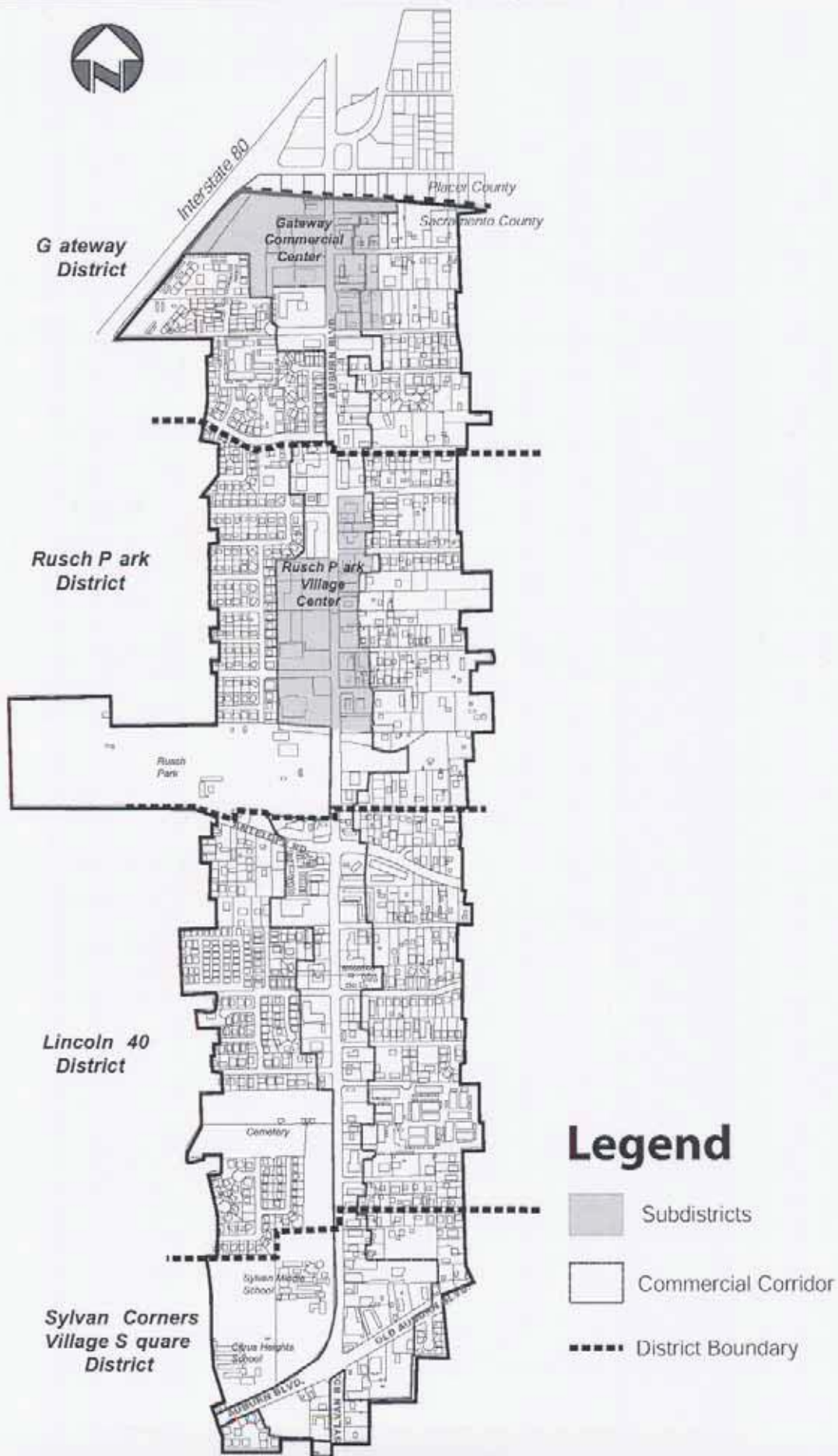
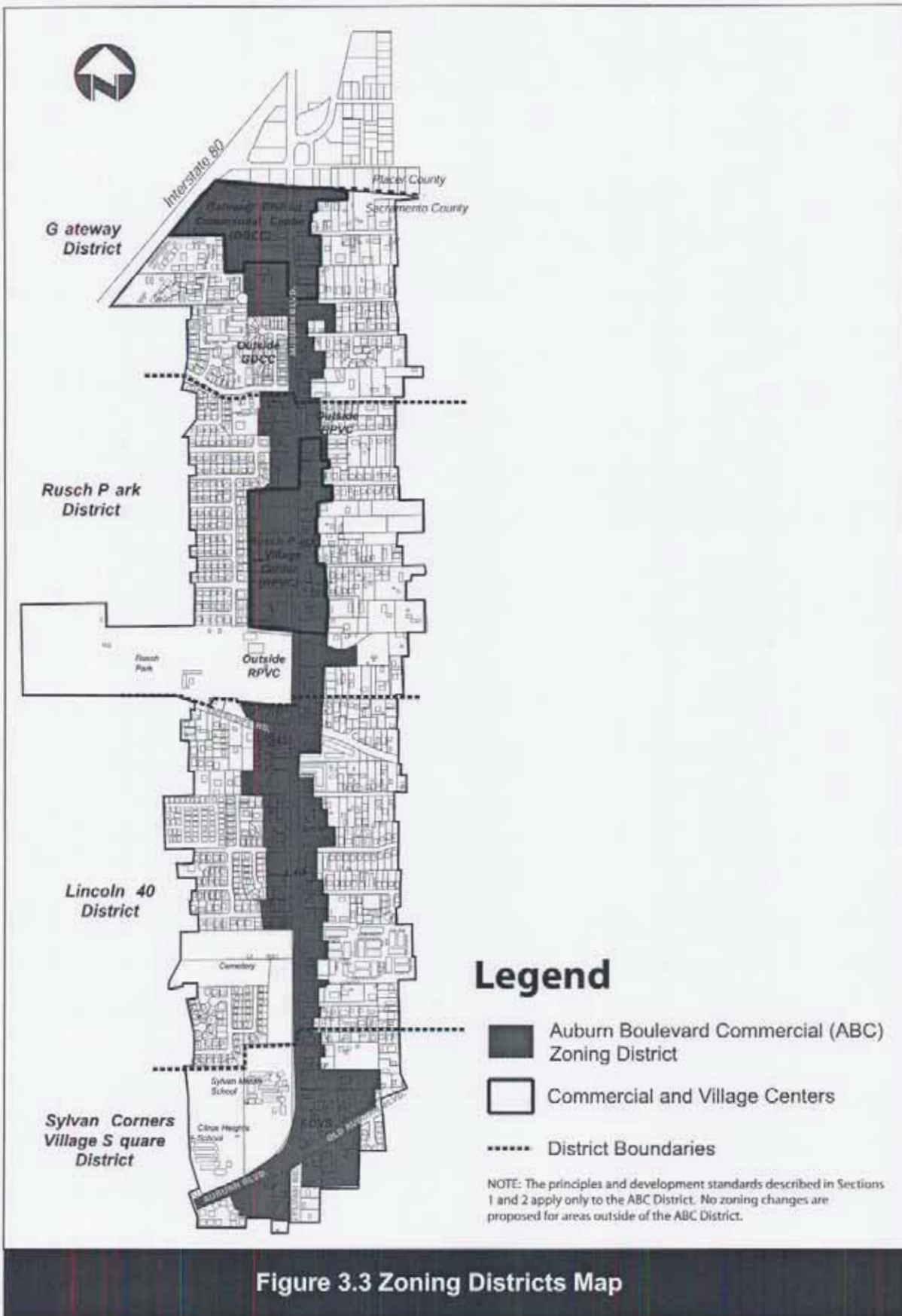


Figure 1.2 Planning Area



Above: This map identifies the boundary of the new Auburn Boulevard (ABC) district.

CHAPTER 7.0 MINOR CHANGES TO THE AUBURN BOULEVARD PLAN LINE STUDY

Minor changes have been made to the proposed plan line for Auburn Boulevard. The sidewalk width is being increased to six feet and bike lanes are decreased to six feet. These revisions do not change the analysis or conclusions contained in the Draft Environmental Impact Report. The revised Plan Line Study is provided on the following pages.

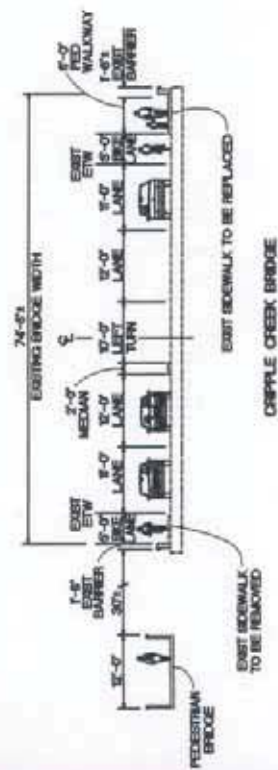
NOTES:

1. ADDITIONAL RIGHT OF WAY WILL BE REQUIRED TO ACCOMMODATE U-TURNS WHERE SHOWN ON THE PLANS. MINIMUM OUTSIDE PAVEMENT CLEAR PATH WIDTH FOR U-TURNING VEHICLES IS 44 FEET. (SEE DETAIL ON SHEET C-4)
2. EXISTING AND PROPOSED RIGHT OF WAY LINES SHOWN WERE TAKEN FROM RECORD INFORMATION ONLY AND ARE APPROXIMATE.
3. BUS TURNOUTS ARE DESIGNED FOR COUNTY OF SACRAMENTO STANDARD DRAWINGS 4-21 AND 4-22. LANDSCAPE STRIP TO BE LOCATED BEHIND SIDEWALK ADJACENT TO BUS TURNOUTS.

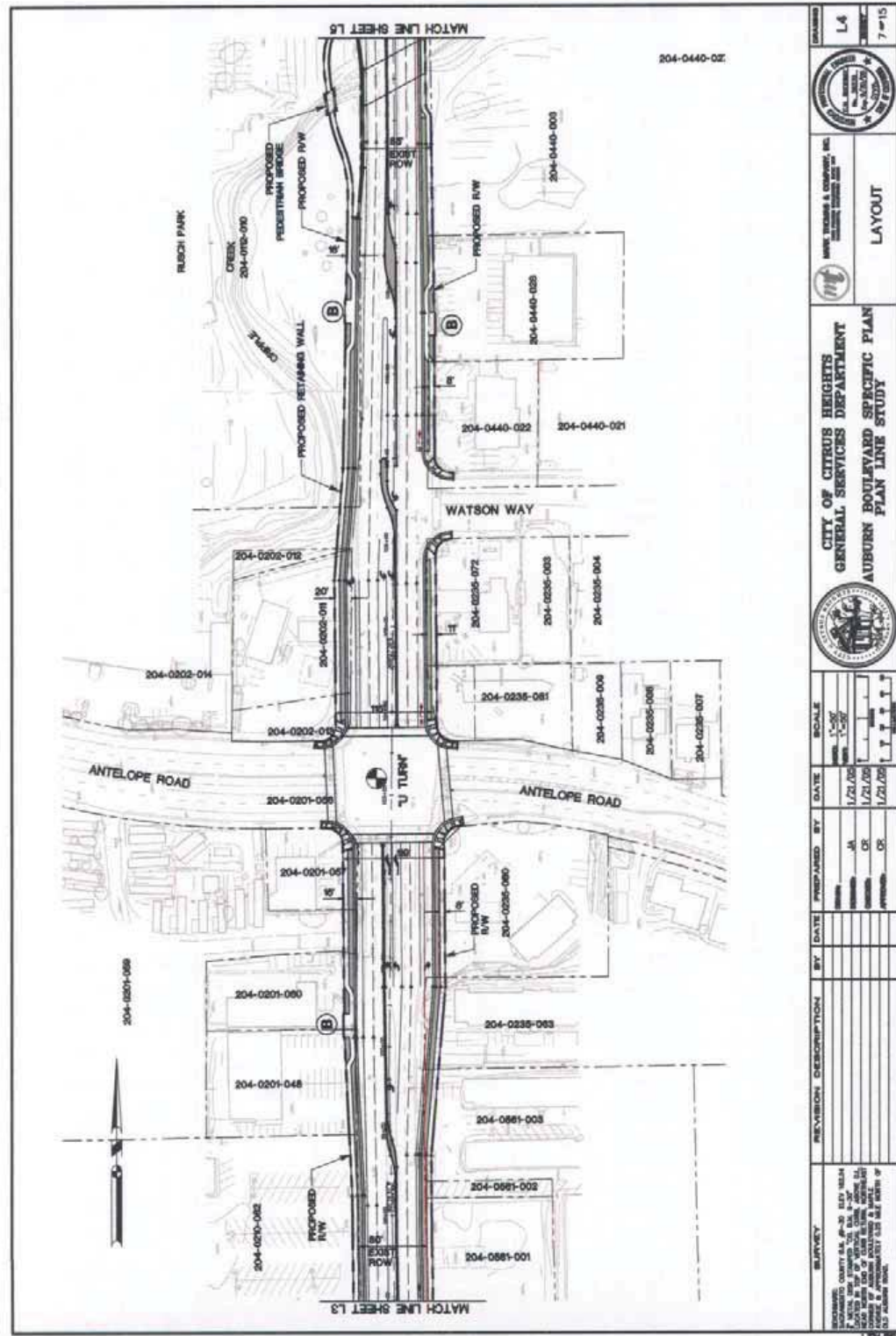
LEGEND:

-  RAISED MEDIAN ISLAND
-  LANDSCAPE AREAS
-  PROPOSED RIGHT OF WAY
-  EXISTING RIGHT OF WAY AND PROPERTY LINES
-  INDICATES U-TURN ALLOWED (SEE NOTE 1)
-  EXISTING STRUCTURE
-  TRAFFIC SIGNAL
-  ADDRESSED PARCEL NUMBER
-  BUS TURNOUT (SEE NOTE 3)
-  BUS BEACH ONLY (NO TURNOUT)

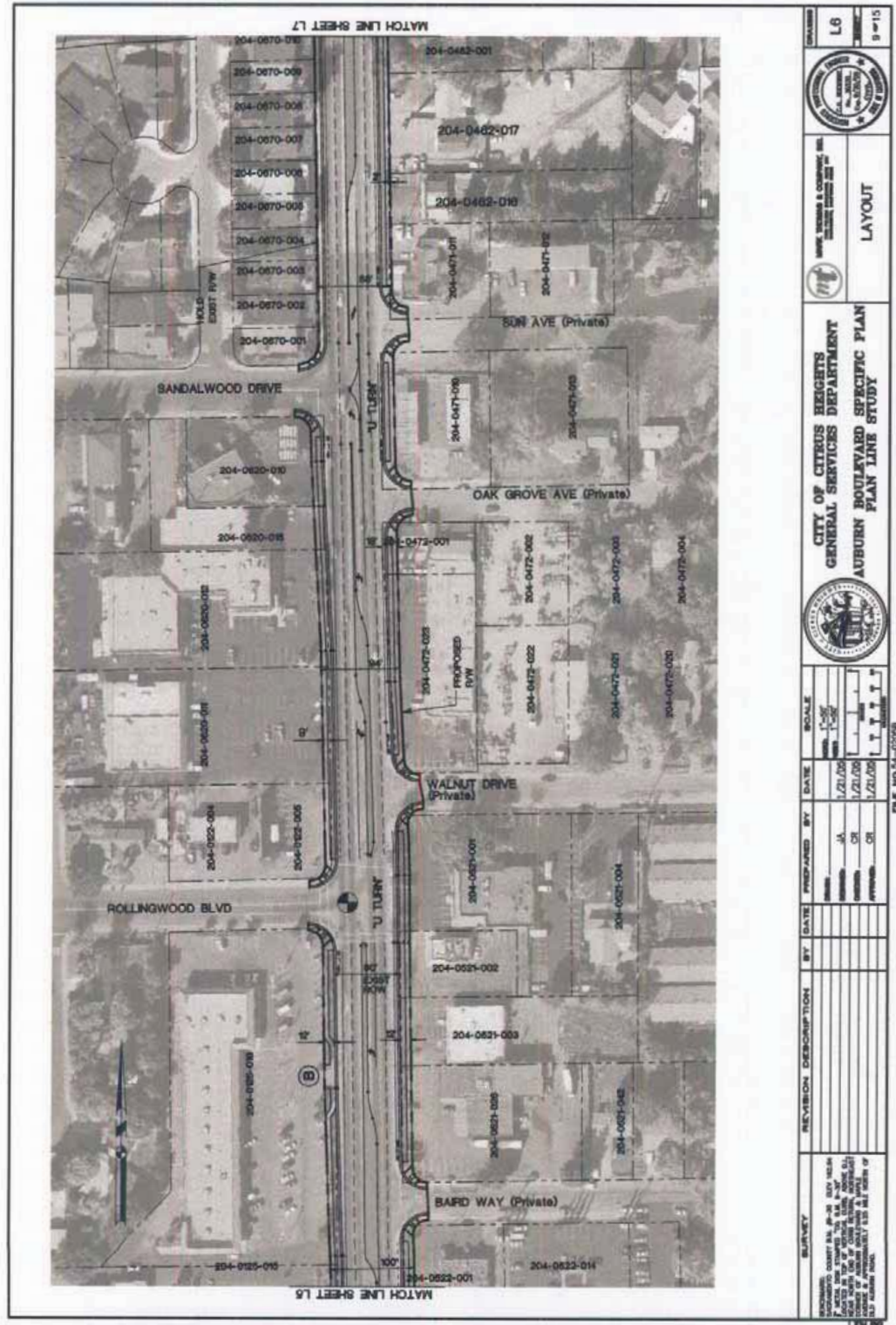
		CITY OF CITRUS HEIGHTS GENERAL SERVICES DEPARTMENT AUBURN BOULEVARD SPECIFIC PLAN PLAN LINE STUDY						GNH 2 of 15					
SURVEY RECORDING COUNTY 041 89-30 BUY 1024 2' METAL ROD STAKED TO E.M. 8'-30" LOCATED IN TOP OF EXISTING CURB ABOVE OLD COURSE OF AUBURN BOULEVARD & 1/2 MILE NORTH OF OLD PARKWAY ROAD.		PREVISION DESCRIPTION		BY		DATE		PREPARED BY		DATE		SCALE	
								DESIGNER: JA CHECKER: CR APPROVED: CR		1/21/05 1/21/05 1/21/05			
FILE NO. 54-12058													
GENERAL NOTES													

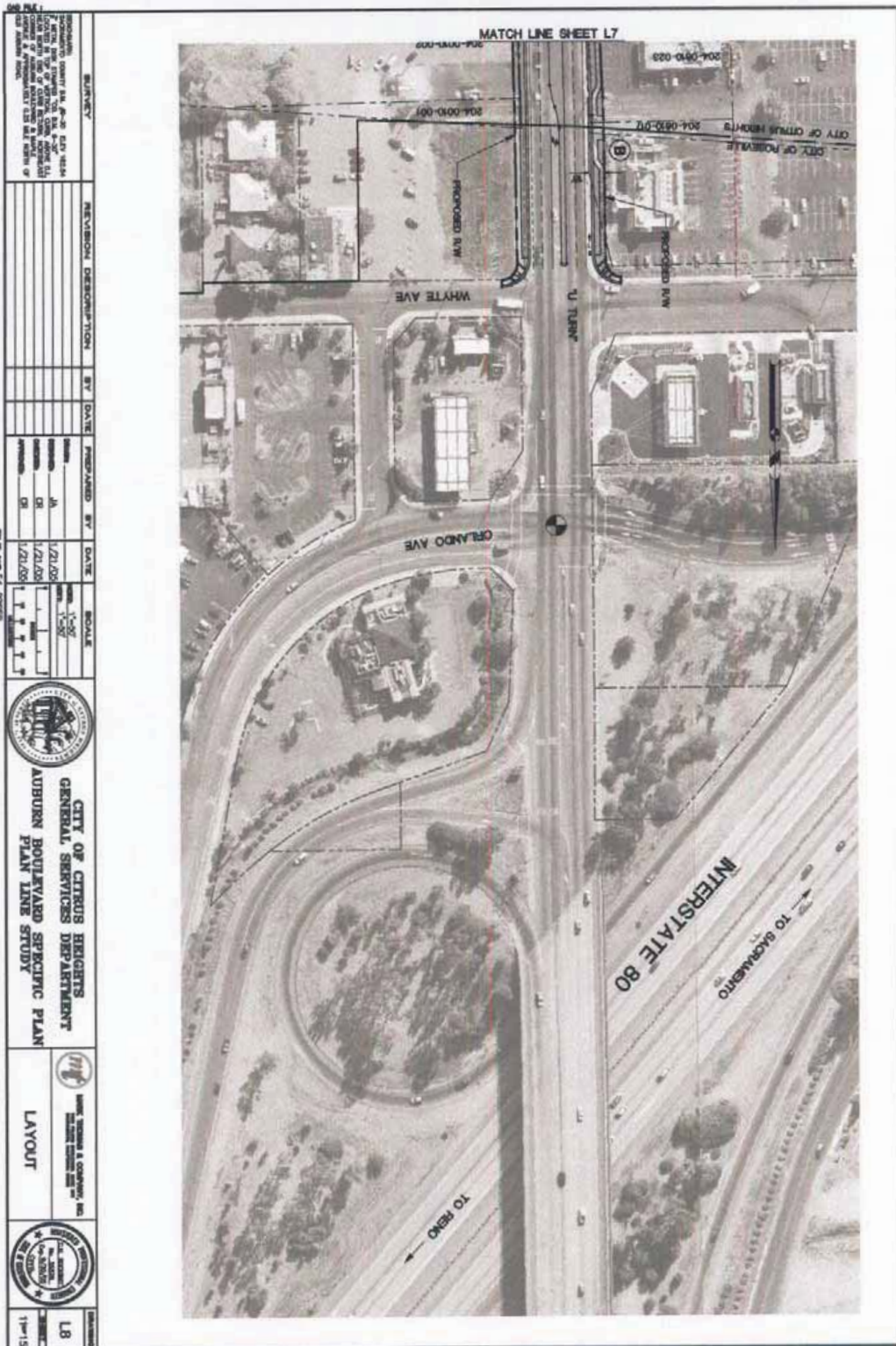
[illegible]



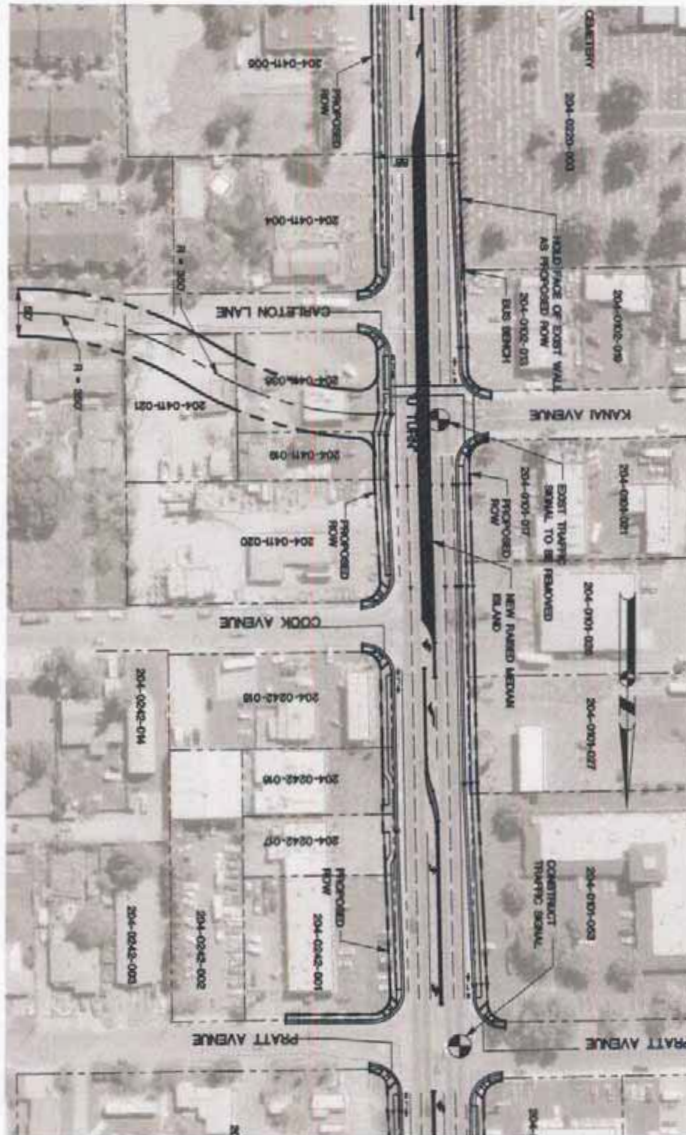


CITY OF CITRUS HEIGHTS GENERAL SERVICES DEPARTMENT AUBURN BOULEVARD SPECIFIC PLAN PLAN LINE STUDY		LAYOUT		7 of 15	
SURVEY EXHIBIT COUNTY OF CALIFORNIA PLAT NO. 100-100-000		REVISION DESCRIPTION 1. 1/21/05 2. 1/21/05 3. 1/21/05		SCALE 1" = 50' 1" = 100' 1" = 200'	
BY DATE PREPARED BY DATE CHECKED BY DATE APPROVED BY DATE		DATE 1/21/05 1/21/05 1/21/05		FILE NO. 100-100-000	





PROJECT: AUBURN BOULEVARD SPECIFIC PLAN SHEET: L8 DATE: 1/21/05		SCALE: 1" = 100' DATE: 1/21/05	
PREPARED BY: JH CHECKED BY: JH APPROVED BY: JH		DATE: 1/21/05 DATE: 1/21/05 DATE: 1/21/05	
CITY OF CITRUS HEIGHTS GENERAL SERVICES DEPARTMENT AUBURN BOULEVARD SPECIFIC PLAN PLAN LINE STUDY		LAYOUT	



**TRAFFIC SIGNAL RELOCATION @ PRATT/KAWAI
AND CARLETON LANE REALIGNMENT**
HORIZ SCALE 1"=50'

LEGEND

INITIAL MEDIAN CONSTRUCTION

REALIGNMENT MEDIAN CONSTRUCTION

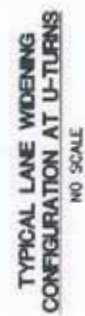
REVISION	DESCRIPTION	BY	DATE	PREPARED BY	DATE	SCALE
1	ISSUED FOR PERMIT	JA	1/27/09	JA	1/27/09	AS SHOWN
2	REVISED TO ADD 2" R/W	CR	1/27/09	CR	1/27/09	AS SHOWN
3	REVISED TO ADD 2" R/W	CR	1/27/09	CR	1/27/09	AS SHOWN



CITY OF CITRUS HEIGHTS
GENERAL SERVICES DEPARTMENT
AUBURN BOULEVARD SPECIFIC PLAN
FUTURE INTERSECTION
DETAILS



C2
12-15

Agenda Packet Page 232

CHAPTER 8.0 MINOR CHANGES AND ERRATA TO THE DRAFT EIR

8.1 Introduction

This chapter presents corrections to the text of the Draft Environmental Impact Report (Draft EIR) needed due to minor typographical errors or due to changes that have been identified in the response to comments (Chapter 4.0). These changes are shown with underline for added text and ~~strike through~~ for deleted text. None of these changes constitute new significant information or result in any new significant impacts related to the Specific Plan, General Plan Amendments, Plan Line Adoption, Rezoning or the Roadway Design Improvement Project.

8.2 Changes And Errata To The Draft EIR

Chapter 1.0 Introduction - Page 1-10

- **Notice of Determination** (CEQA Guidelines ~~15039~~15093 [c]), which is filed following the City's action.

Chapter 4.0, Air Quality - Page 4.3-18

4.3.6.1 Impact AQ-10 Construction Related Impacts

Mitigation Measures - Specific Plan Construction Impacts

~~**Nitrogen Oxide Mitigation Measures.** The following two categories of mitigation measures should be incorporated into the project to minimize the generation of NO_x emissions:~~

~~AQ Mitigation Measure Category 1: Reduce NO_x emissions from off road diesel powered equipment~~

~~The project shall provide a plan for approval by the lead agency, in consultation with SMAQMD, demonstrating that the heavy duty (> 50 horsepower) off road vehicles to be used in the construction project, including owned, leased and subcontractor vehicles, will achieve a project wide fleet average 20 percent NO_x reduction and 45 percent particulate reduction compared to the most recent CARB fleet average at time of construction; and~~

~~The project representative shall submit to the lead agency and SMAQMD a comprehensive inventory of all off road construction equipment, equal to or greater than 50 horsepower, that will be used an aggregate of 40 or more hours during any portion of the construction project. The inventory shall include the horsepower rating, engine production year, and projected hours of use or fuel throughput for each piece of equipment. The inventory shall be updated and submitted monthly throughout the duration of the project, except that an inventory shall not be required for any 30 day~~

~~period in which no construction activity occurs. At least 48 hours prior to the use of subject heavy duty off road equipment, the project representative shall provide SMAQMD with the anticipated construction timeline including start date, and name and phone number of the project manager and on-site foreman.~~

~~and:~~

~~**AQ Mitigation Measure Category 2: Controlling visible emissions from off road diesel powered equipment**~~

~~The project shall ensure that exhaust emissions from all off road diesel powered equipment used on the project site do not exceed 40 percent opacity for more than three minutes in any one hour. Any equipment found to exceed 40 percent opacity (or Ringelmann 2.0) shall be repaired immediately, and the lead agency and SMAQMD shall be notified within 48 hours of identification of non-compliant equipment. A visual survey of all in operation equipment shall be made at least weekly, and a monthly summary of the visual survey results shall be submitted throughout the duration of the project, except that the monthly summary shall not be required for any 30 day period in which no construction activity occurs. The monthly summary shall include the quantity and type of vehicles surveyed as well as the dates of each survey. The SMAQMD and/or other officials may conduct periodic site inspections to determine compliance. Nothing in this section shall supercede other SMAQMD or state rules or regulations.~~

Mitigation Measure AQ-1A: Inhalable Particulate Matter. The following mitigation measures shall be incorporated into the project to minimize the generation of PM₁₀ dust during construction.

- enclose, cover, or water twice daily all soil piles;
- water exposed soil with adequate frequency for continued moist soil;
- water all haul roads twice daily; and
- cover loads of all haul/dump trucks securely.

Mitigation Measure AQ-1B: Asbestos. The demolition or renovation of asbestos-containing building material is subject to the limitations of the National Emissions Standards for Hazardous Air Pollutants (NESHAP) regulations as listed in the Code of Federal Regulations (40CFR Part 61, Subpart M) requiring notification and inspection. Most demolitions and many renovations are subject to a CAL-OSHA Certified asbestos inspection prior to the start of activity. SMAQMD Rule 902, which requires District consultation and permit, applies to demolition, renovation or removal of asbestos-containing material. Compliance with these regulations is considered to reduce this impact to a less-than-significant level.

Chapter 4.0, Biological Resources - Page 4.4-20

Mitigation Measure B-~~32~~3D Preconstruction Tree Survey

Mitigation Measure B-2D3 Avoid Impacts to Nesting Birds

Chapter 4.0, Transportation and Circulation

Figure 4.11-1 (on following page) follows reference on page 4.11-4.

Page 4.11-23

4.11.6.6 Impact T-6: Impacts to the circulation network during construction activities.

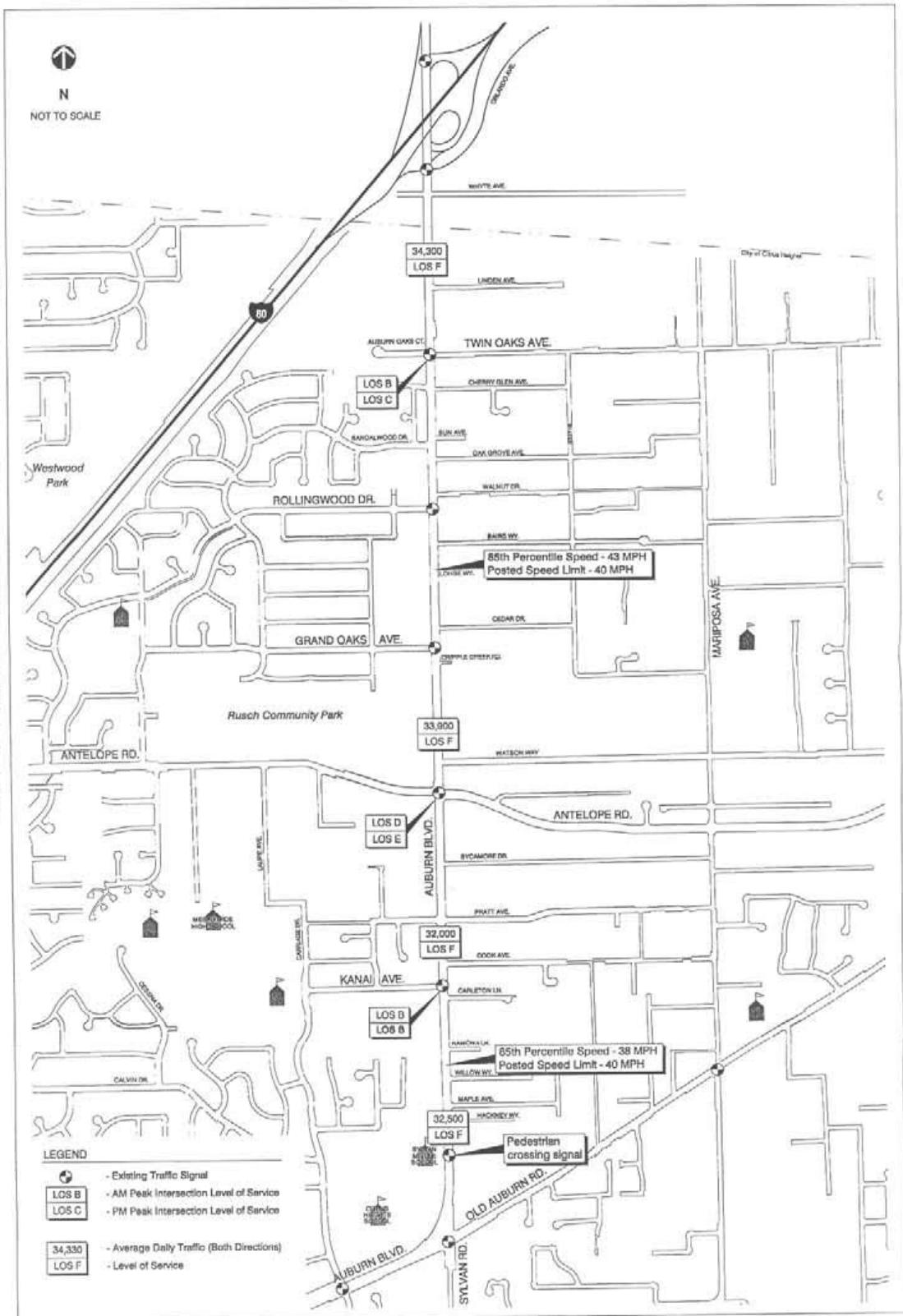
A. Impacts of the Specific Plan, General Plan Amendments, Plan Line Adoption and Future and Near Term Roadway Design Improvement Projects

Construction activities would result in temporary disruption to businesses and residents in the area as well as to some public services such as public transit services and emergency vehicles (i.e. fire and police). Detours and traffic delays may occur; however interruptions to access would be minimized for all properties. These impacts would be temporary and are not considered significant, with the implementation of standard traffic handling for construction periods.

Mitigation Measures

A traffic handling plan will be prepared prior to construction of any roadway improvements. The plan will address traffic management during construction periods, including but not limited to road and lane closures; detours; pedestrian and bicycle routes; and public notification. The traffic handling plan should be prepared in consultation with regional transit in order to minimize disruptions to public transit service along the corridor. Additionally, prior to commencement of construction, a Traffic Management Plan as described in A Traffic Management Plan Guide (See Appendix A of the Final EIR) will be prepared and provided to Caltrans for review in order to address strategies needed to minimize disruption of traffic at the Interstate 80/Auburn Boulevard Interchange.

Figure 4-11-1 Existing Conditions Daily Traffic on Auburn Boulevard (Source: Fehr & Peers Transportation Consultants, 2002)



Chapter 4.0, Utilities and Service Systems

4.12.4 Impacts and Mitigation Measures

4.12.4.1 Impact U-1: Impacts to Water Supply, Water Treatment, Wastewater Treatment and Storm Drain Facilities.

A. Impacts of the Specific Plan and General Plan Amendments

The proposed Specific Plan is largely urbanized with existing connections to water supply, water treatment and wastewater treatment facilities. According to the DEIR for the Citrus Heights General Plan, capacity for water supply, water treatment facilities and wastewater treatment facilities are expected to be adequate to serve the projected development buildout of the General Plan. Development proposed by the specific plan is consistent with the Citrus Heights General Plan, therefore no impacts are anticipated with respect to water supply, water treatment and wastewater treatment. CSD-1 has commented in their letter of January 14, 2005 that previously planned trunk line relief projects are no longer in their current planning for capacity increases, due to cost or design complexities. This will not impede the ability of the district to service the Specific Plan Area.

Mitigation Measures for the Specific Plan and General Plan Amendments

No mitigation measures are required.

Mitigation Measure U-2 Construction Management for Utilities: The construction project management team shall coordinate with utility providers during design stages of roadway projects. The construction project management team shall undertake periodic assessments of upcoming utility and service disruptions during construction. These assessments and an identification of the service area involved shall be coordinated with utility providers and the public outreach program. The public outreach program shall ensure that advance notice of any utility or service shutdowns is extended to affected businesses and residents. Through construction management and project scheduling, all available measures shall be taken to minimize the duration of utility or service shutdowns.

APPENDICES

APPENDIX A
Notice of Completion for Draft EIR

Notice of Completion & Environmental Document Transmittal

Appendix C

Mail to: State Clearinghouse, P. O. Box 3044, Sacramento, CA 95812-3044 (916) 445-0613
For Hand Delivery/Street Address: 1400 Tenth Street, Sacramento, CA 95814

SCH # **2003-062165**

Project Title: The Boulevard Plan, A Specific Plan for the Auburn Boulevard Corridor from Sylvan Corners to the City Limits, Citrus Heights

Lead Agency: City of Citrus Heights

Contact Person: Janet Ruggiero, FAICP

Mailing Address: City of Citrus Heights, 6237 Fountain Square Drive

Phone: 916 725-2448

City: Citrus Heights

Zip: 95621

County: Sacramento

Project Location:

County: Sacramento

City/Nearest Community: Citrus Heights

Cross Streets: Old Auburn Road/Antelope Road/Linden Avenue

Zip Code: 95621

Assessor's Parcel No.: NA

Section: 24

Twp.: 10N

Range: 6E

Base:

Within 2 Miles: State Hwy #: YES

Waterways: yes

Airports: NA

Railways: yes

Schools: yes

Document Type:

CEQA: ☐ NOP

☒ Draft EIR

☐ Early Cons

☐ Supplement/Subsequent EIR

☐ Neg Dec

(Prior SCH No.)

☐ Mit Neg Dec

☐ Other

NEPA: ☐ NOI

☐ EA

☐ Draft EIS

☐ FONSI

Other: ☐ Joint Document

☐ Final Document

☐ Other



Local Action Type:

☐ General Plan Update

☒ Specific Plan

☒ Rezone

☐ Annexation

☒ General Plan Amendment

☐ Master Plan

☐ Prezone

☐ Redevelopment

☐ General Plan Element

☐ Planned Unit Development

☐ Use Permit

☐ Coastal Permit

☐ Community Plan

☐ Site Plan

☐ Land Division (Subdivision, etc.)

☐ Other

Development Type:

☒ Residential: Units 541

Acres

☐ Water Facilities: Type

MGD

☒ Office: Sq.ft. 103,775

Acres

Employees

☐ Transportation: Type

☒ Commercial: Sq.ft. 743,853

Acres

Employees

☐ Mining: Mineral

☐ Industrial: Sq.ft.

Acres

Employees

☐ Power: Type

MW

☐ Educational

☐ Waste Treatment: Type

MGD

☐ Recreational

☐ Hazardous Waste: Type

Total Acres (approx.) 112

☒ Other: Mixed Use Retail 10,000 s.f. Hotel 48,135 s.f.

Project Issues Discussed in Document:

☒ Aesthetic/Visual

☐ Fiscal

☒ Recreation/Parks

☒ Vegetation

☐ Agricultural Land

☒ Flood Plain/Flooding

☐ Schools/Universities

☒ Water Quality

☒ Air Quality

☐ Forest Land/Fire Hazard

☐ Septic Systems

☐ Water Supply/Groundwater

☒ Archeological/Historical

☐ Geologic/Seismic

☒ Sewer Capacity

☒ Wetland/Riparian

☒ Biological Resources

☐ Minerals

☐ Soil Erosion/Compaction/Grading

☒ Wildlife

☐ Coastal Zone

☒ Noise

☐ Solid Waste

☒ Growth Inducing

☒ Drainage/Absorption

☒ Population/Housing Balance

☒ Toxic/Hazardous

☒ Land Use

☐ Economic/Jobs

☒ Public Services/Facilities

☒ Traffic/Circulation

☒ Cumulative Effects

☐ Other

Present Land Use/Zoning/General Plan Designation:

General Plan - Commercial and Residential Zoning: Commercial and Residential

Project Description: (please use a separate page if necessary)

The Boulevard Plan encompasses approximately 460 acres along the Auburn Boulevard corridor between the City of Citrus Heights city limits on the north and Sylvan Corners on the south, and extends approximately 1,000 feet east and west of Auburn Boulevard. The Plan focuses primarily on the parcels and commercial and residential uses (approx. 112 acres) that front on Auburn Boulevard. The Boulevard Plan includes Concepts, Goals and Principles to establish formal policies regarding land use, circulation and community design and presents, key concepts of the plan; Development Standards that present specific guidance for public and private development; Guidelines for making design changes along the corridor in each of four districts; and an Implementation Section that describes how the Plan's policies and guidelines are to be put into place. The purpose of The Boulevard Plan is to improve the corridor's image and commercial competitiveness in the region; improve its function as a transportation facility serving adjacent land uses and provide for better connections with the neighborhoods bordering the corridor.

Note: The State Clearinghouse will assign identification numbers for all new projects. If a SCH number already exists for a project (e.g. Notice of Preparation or previous draft document) please fill in.

Revised 2004

APPENDIX B
A Traffic Management Plan Guide
(Caltrans)

State of California
Department of Transportation

Transportation Management Plan Guidelines

Prepared By:
Division of Traffic Operations
Office of Systems Management Operations

I. INTRODUCTION

A. BACKGROUND

With the construction of California's state highway system virtually complete, the California Department of Transportation (Department) major emphasis on transportation projects has largely shifted from new construction to reconstruction, operation, and maintenance of existing facilities. As traffic demand steadily increases, Department work activities can create significant additional traffic delay and safety concerns on already congested highways. Planning work activities and balancing traffic demand with highway capacity becomes more critical.

In order to prevent unreasonable traffic delays resulting from planned work, Transportation Management Plans (TMPs) must be carefully developed and implemented in order to maintain acceptable levels of service and safety during all work activities on the state highway system.

B. WHAT ARE TRANSPORTATION MANAGEMENT PLANS?

A TMP is a method for minimizing activity-related traffic delay and accidents by the effective application of traditional traffic handling practices and an innovative combination of public and motorist information, demand management, incident management, system management, construction strategies, alternate routes and other strategies.

All TMPs share the common goal of congestion relief during the project period by managing traffic flow and balancing traffic demand with highway capacity through the project area, or by using the entire corridor. Certain low-impact Maintenance and Encroachment Permit activities do not require the development of individual TMPs. "Blanket" TMPs are developed for those activities. A blanket TMP is a generic list of actions that would be taken to keep delay below the delay threshold when performing activities on highways. Each district Maintenance and Encroachment Permit office should have a list of activities to which blanket TMPs apply.

All Capital projects require individual TMPs. Blanket TMPs are suitable for minor projects. Major TMPs are required for high-impact projects. Generally, major TMPs are distinguished by being:

- Multi-jurisdictional in scope, encompassing the Department of California Highway Patrol (CHP), city, county and regional governments, state DOTs, employers, merchants, developers, transit operators, ridesharing agencies, neighborhood and special interest groups, emergency services, and Transportation Management Associations;
- Multi-faceted, comprised of an innovative mix of traffic operations, facility enhancement, demand-management and public relations strategies, as well as more traditional work zone actions, construction methods and contract incentives, customized to meet the unique needs of the impacted corridor;
- In place over a longer period of time, sometimes implemented up to a year or more prior to the start of actual construction, with specific elements often implemented incrementally to coincide with construction phasing.

C. POLICY

The TMP Manager:

- o Acts as the single focal point for development and implementation of TMPs.

The Construction Traffic Manager (CTM):

- o Serves as a liaison between Construction, the DTM and the TMP Manager.
- o Reviews the TMP and traffic contingency plan for constructability issues.
- o Act as a resource for the Resident Engineer, DTM and TMP Manager during TMP implementation and reviews the contractor's contingency plan.

The extent of a TMP is determined by the DTM during the preliminary studies of a capital project. For all TMPs, an itemized estimate of the proposed strategies and their respective costs are included in the Project Study Report (PSR) or Project Study Scoping Report (PSSR) for proper funding consideration. The workload required to develop and implement TMPs is estimated in advance and captured in the district work plan.

For major TMPs, a TMP team may need to be formed and led by the TMP Manager. The itemized strategies and costs are further refined in the project report stage as determined by the TMP team and appropriate functional units using the most current geometric information available. Those elements of the TMP not included as part of the main construction contract should be itemized under State Furnished Material and Expenses using the appropriate Basic Engineers Estimate System (BEES) codes in the plans, specifications and estimates. During construction, TMP activities are to be monitored and evaluated by the TMP team and those elements found not to be cost effective should be modified as deemed appropriate or eliminated. The TMP process is explained in detail in the following sections.

B. FUNDING AND PROGRAMMING

When identifying funding for various TMP elements, it is important to distinguish between capital outlay and capital outlay support.

Work done by district staff for the planning and designing of TMP activities for capital projects are a normal part of the project development process and should be captured as capital outlay support. The TMP Manager and each functional manager should work closely with the project manager to ensure that TMP activities are included in all project work plans. TMP support activities to consider include ridesharing programs, Freeway Service Patrol (FSP) contracts, public awareness campaigns, parallel route improvements and the Request for Proposal (RFP) process up to award of the contract. Note that some of these activities may also have a capital component in addition to the support component discussed here. Workload hours for TMP activities must be included in the Capital Outlay Support (COS) project's work plan in order to be resourced (funded) by COS. These activities should then be charged to each project's expenditure authorization (EA), using the appropriate Work Breakdown Structure (WBS) code for that stage of the project. TMP-related work should be charged only to the WBS codes reserved for those activities. These codes can be found on the Department's Division of Project Management's Intranet web page.

Work done by district staff for implementing TMP elements during construction of capital projects are also a normal part of the project development process. Again, workload (hours) for implementing TMP activities must be included in the COS project's work plan in order to be resourced (funded) by COS. These activities should then be charged to the appropriate project's phase three EA, and WBS code 270 (Perform Construction Engineering and Contract Administration).

Some funds necessary to implement TMP elements not done by the Department staff, including consultant contracts, can be sourced from capital outlay funds allocated by the California Transportation Commission (CTC) as itemized in the plans, specifications and estimates. Some TMP elements, such as parallel route improvements and highway advisory radios, could be a phase of the construction contract or separate construction contracts while others such as public awareness campaigns and transit subsidies must be separate contracts or cooperative agreements.

The TMP elements that need to be in place prior to start of construction are identified and funded as stage construction or first order of work under a single package presented to the CTC. If approved, the Division of Budgets may assign specific amounts for each TMP activity. All TMP activities may not necessarily be included under the main contract. Service contracts such as those for freeway service patrols, public service or consultant contracts, information campaigns, or establishing telephone hotlines must be arranged separately with consultants and other providers. For most projects, it takes four to six months to get a service contract in place. This means that all consultant contracts have been advertised, the consultant selected, and the contract ready for signature and award immediately following CTC allocation of funds. Other activities such as parallel route improvements are usually included in the main construction contract and as a first order of work under a cooperative agreement.

In some cases, the CTC can be petitioned to fund a portion of the TMP as an initial phase of the main project. This is usually for a high priority project where plans, specifications, and estimates for the main project are not yet finalized, but early funds are needed to initiate TMP activities such as making transit arrangements with local governments. The petition to fund an initial phase comes from the district, explaining why a portion of the project must proceed before funding for the main project is allocated. These early funds reduce the programmed funds for the main project accordingly.

The Federal Highway Administration (FHWA) supports the TMP concept and views major reconstruction projects as an excellent opportunity to initiate continuing traffic management strategies that provide improved traffic operations long beyond the completion of work. Examples include: installation of permanent Changeable Message Sign (CMS), full structural section shoulders, continuing auxiliary lanes, and wider shoulders for incident management during construction if cost-effective in the long term. All cost-effective transportation management activities that address the problem of delay or safety are eligible for 100 percent Federal Aid funding.

TMPs and contingency plans for Encroachment Permit projects are developed by the permittee or by Department staff. Staff time for development, review and implementation of TMPs for Encroachment Permits is charged to the permit. Maintenance normally develops TMPs for its projects; Maintenance and staff from other functional areas that expend time on Maintenance TMP charge to the designated Maintenance EA.

C. TMP IN PROJECT INITIATION DOCUMENT

The TMP is part of the normal project development process and must be considered in the Project Initiation Document (PID) or planning stage (project K phase). Since projects are generally programmed, budgeted, and given an Expenditure Authorization (EA) upon PID approval, it is important to allow for the proper cost, scope and scheduling of the TMP activities at this early stage of development. TMPs that are retrofitted to projects already programmed must be handled on a case by case basis and may require a contract change order.

Prior to PID approval, the initiating unit sends conceptual geometrics to the district Division of Operations for evaluation. The DTM estimates the extent of the TMP required and determines whether potential traffic delays are anticipated that cannot be mitigated by traditional traffic handling practices or well-planned construction staging. The TMP Manager must sign-off on the TMP DATA SHEET in the PID. A TMP cost estimate should be developed for each alternative being considered. An estimate should not be based only on the project cost. The cost of a TMP could range from a small percentage of project cost to 20 percent or more. Further guidance can be obtained from the following publications "Wilbur Smith & Associates TMP Effectiveness Study" and Frank Wilson & Associates "A Traffic Management Plan Study for State Route 91" located in Headquarters Traffic Operations, Office of System Management Operations.

TMP Elements

A list of potential TMP strategies with their respective elements is categorized in TABLE 1. As many different elements as are feasible should be considered for the proposed project's preliminary TMP.

When developing a preliminary TMP at this early stage, use the most current layout of the roadway (geometrics) information available and consider:

Contingency Plans	Expected vehicle delay (from data sheet)
Lane closure policies and procedures	Public/media exposure
TMC coordination	Political or environmental sensitivity
Multi-jurisdictional communication and buy-in	Business impacts and affected activity
CHP and local law enforcement involvement	Percent trucks
Emergency closures	Potential increase in accidents
Clearance of alternate routes for STAA and oversized	Permit issues
Special training or workforce development	Conflicting construction projects
Duration of construction (months)	Percent reduction in vehicle capacity
Length of project (miles)	Special factors (if any)
Number of major construction phases	Impact on Transit/Railroad services
Urbanization (urban, suburban, or rural)	Viability of alternative routes
Traffic volumes	

Wilbur Smith Associate's TMP Effectiveness Study and Frank Wilson & Associate's A Traffic Management Plan Study for State Route 91 During Construction of HOV Lanes (both available from Headquarters Division of Traffic Operations, Office of System Management Operations) are excellent sources for guidance on selecting the most cost-effective TMP elements. The district Public Information office is also an experienced source for estimating the effectiveness of public information campaign options, and can help the TMP Manager estimate their cost and effectiveness in reducing traffic demand through the project area.

Public information campaigns serve two main purposes in TMPs. They inform the public about the overall purpose of the project to generate and maintain public support; and they encourage changes in travel behavior during the project to minimize congestion. Because they give travelers the information they need to make their own travel choices, public information campaigns can be the single most effective of all TMP elements.

The FSP is a congestion relief program of roving tow trucks operating in most metropolitan and some rural areas. The FSP program is operated by Regional Transportation Planning Agencies (RTPAs) with funding from the Department. The Department also reimburses the CHP for training and supervisory services provided for the FSP. The RTPA

for commute time service and some weekend and mid-day service to assist motorists with simple repairs (i.e. flat tire, one gallon of gas) or tow the automobile from the highway.

FSP is available for incident management during construction. However, construction-related FSP service needs to be funded as part of the TMP. A cooperative agreement with the RTPA is required, outlining the services provided and the fund transfer. An interagency agreement with the CHP is required for any support services (field supervision and dispatch operator services). These agreements should be initiated with the RTPA and the CHP as soon as it is determined that FSP should be in the project TMP.

The Department's HQ Traffic Operations is currently working on Master Agreements with the RTPAs for future FSP services. This process will simplify the process for both the Department and the RTPAs by eliminating the need for a cooperative agreement for each project. Only a task order form will be needed for each project. A similar agreement is being created with the CHP. Please contact HQ Traffic Operations, Freeways Operations Branch for more information.

TABLE 1

TMP STRATEGIES AND THEIR ELEMENTS	
A. Public Information	Off peak/Night/Weekend Work
Brochures and Mailers	Planned Lane/Ramp Closures
Media Releases (including	Project Phasing
Minority Media Sources)	Temporary Traffic Screens
Paid Advertising	Total Facility Closure
Public Information Center	Truck Traffic/Permit Restrictions
Public Meetings/Speaker's Bureau	Variable Lanes
Telephone Hotline	Extended Weekend Closures
Visual Information (videos, slide shows, etc.)	Reduced Speed Zones
Local cable TV and News	Coordination with Adjacent Construction
Traveler Information Systems (Internet)	Traffic Control Improvements
Internet	Total Facility Closure
B. Motorist Information Strategies	E. Demand Management
Electronic Message Signs	HOV Lanes/Ramps
Changeable Message Signs	Park-and-Ride Lots
Extinguishable Signs	Parking Management/Pricing
Ground Mounted Signs	Rideshare Incentives
Commercial Traffic Radio	Rideshare Marketing
Highway Advisory Radio (fixed and mobile)	Transit Incentives

Planned Lane Closure Web Site	Transit Service Improvements
The Department's Highway Information Network (CHIN)	Train or Light-Rail Incentives
Radar Speed Message Sign	Variable Work Hours
	Telecommute
C. Incident Management	Shuttle Service Incentives
Call Boxes	
Construction or Maintenance Zone Enhanced	F. Alternate Route Strategies
Enforcement Program – COZEEP or MAZEEP	Ramp Closures
Freeway Service Patrol	Street Improvements
Traffic Surveillance Stations (loop detectors and CCTV) Closures	Reversible Lanes
911 Cellular Calls	Temporary Lanes or Shoulder Use
Transportation Management Centers	
Traffic Control Officers	G. Other Strategies
CHP Officer in TMC during construction	Application of new technology
Onsite Traffic Advisor	Innovative products
CHP Helicopter	Improved specifications
Traffic Management Team	Staff Training/Development
D. Construction Strategies	
Incentive/Disincentive Clauses	
Ramp Metering	
Lane Rental	

If the DTM determines that a major TMP is required, the TMP Manager forms a TMP development team. The team's membership will vary according to the TMP elements proposed and the project's impacts. At a minimum, it should include representatives from Construction, Public Affairs, Project Development, Traffic Operations (including Transportation Permits), the CHP and local agencies. Others to be considered as the plan gets refined are Rideshare, Transportation Planning, Public Transportation, Maintenance, Structures, CHP, local law enforcement, local transit agencies, emergency services, and FHWA. Local Maintenance field staff familiar with conditions in the project area should be team members or should be consulted as needed as the TMP develops.

D. TMP IN PROJECT REPORT

As more information becomes available during the project report phase the preliminary scope and cost of the overall TMP and the individual elements should continue to be refined. The TMP team will coordinate the TMP strategies with the project engineer and appropriate units, such as

each team member handling their area of expertise. For major projects, subcommittees or task forces may be formed to handle the planning, implementation, monitoring, and evaluation details of some elements. The TMP Manager will keep the Project Manager and district Construction Coordinator updated and must sign-off on the TMP data sheet of the project report.

It is appropriate at this point to develop a timeline schedule for major TMPs keeping in mind that many elements of the TMP have to begin prior to the start of construction. Many TMP elements listed in Table 1 need to be developed separately but concurrently with the project plans. They may be bid and constructed or initiated separately from the project or be included in the project plans and be installed or implemented as the first order of work.

Some tasks may take a long time depending on the complexity of the major project and the type of transportation management necessary. For example, if building new park-and-ride lots are necessary for the Ridesharing element, the planning phase would have to be extended for several months and a design phase added.

An additional activity involves analyzing the existing traffic volume in the corridor, both on the freeway and surface streets. This will provide a basis for establishing the goal of the TMP, i.e., the number of vehicles that should be removed from the freeway, and in determining the capability of the surrounding surface streets to handle the additional traffic demand. It can also provide a database for evaluating the overall effectiveness of the TMP.

E. TMP IN PS&E

Those TMP elements that are not part of the main contract, but are identified as capital outlay costs tied to the main project, should be itemized as State Furnished Materials and Expenses using the appropriate BEES item cost (see TABLE 2). The Project Engineer should consult with the TMP Manager to ensure that the appropriate "Maintaining Traffic" Standard Special Provisions (SSP) are included in the PS&E. The SSPs should always require the contractor to submit a contingency plan.

The TMP and PS&E should address oversize and overweight vehicles traveling under a transportation permit. Additional construction area signs should be provided that restrict travel to overwidth vehicles whenever the lateral clearance drops to 15 feet or less.

The DTM must concur with the PS&E and with Encroachment Permit and Maintenance TMPs.

TABLE 2

TMP BEES ITEM CODES	
066003	State Furnished Materials
066004	Miscellaneous State Furnished Materials
066005	Concurrent Work
066006	Miscellaneous Concurrent Work
066008	Incentive Payment
066009	Utility Expense

066010 Work by Others

066060 Additional Traffic Control

066061 CHP Enhanced Enforcement

066062 COZEEP Contract

066063 Traffic management plan – public Information

066064 Specter Radar Unit

066065 Freeway Service Patrol

066066 Public Transit Support

066069 Rideshare Promotion

066070 Maintain Traffic

066072 Maintain Detour

066074 Traffic Control

066076 Temporary Traffic Control

066077 Install Traffic Control Devices

066578 Portable Changeable Message Signs

066825 Temporary Striping

066872 Service Contract

128602 Traffic Control System (One Way)

128650 Portable Changeable Message Signs

129150 Temporary Traffic Screen

861793 Telephone Service (Location 1)

860811 Detector Loop

860925 Traffic Monitoring Station (Count)

860926 Traffic Monitoring Station (Speed)

860927 Traffic Monitoring Station (Incident)

860930 Traffic Monitoring Station

861088 Modify Ramp Metering System

861985 Travelers Information system

869070 Power and Telephone Service

991046 Public Address System

991047 Telephone Facility

994920 Bicycle Parking Rack

995000 Bus Shelter
995002 Bus Passenger Shelter (Type S-1)
995004 Bus Passenger Shelter (Type SM-1)
995005 Bus Passenger Shelter (Type LM-1)

F. TMP DURING CONSTRUCTION AND MAINTENANCE OPERATIONS

During construction, those TMP elements that are part of the main contract or Encroachment Permit are implemented under the general direction of district Construction or Encroachment Permits. Those separate contracts/agreements such as for rideshare and transit activities and public awareness campaigns will be under the direction of their respective contract managers.

Special effort should be given to assure that Changeable Message Sign (CMS), Highway Advisory Radio (HAR) and other media tools provide accurate and timely information to motorists regarding lane closure times and

TMP elements must be carefully monitored for cost effectiveness. The TMP team should determine whether the implemented measures are reaching the predetermined goals for cost effectiveness. If an element's predetermined goal is not immediately reached during implementation, but there is a general trend toward meeting that goal, the element can remain in effect and the FHWA will continue to participate. Elements that show no sign of approaching their predetermined goals as determined by the TMP Manager must be modified as deemed appropriate or dropped.

Contractor compliance with lane closure pickup deadlines can be enforced in two ways. A "maintaining traffic" SSP allows a penalty to be assessed to the contractor for value of traffic delay when the contractor exceeds the lane closure window. The minimum penalty is \$1,000 per 10 minutes, but it can greatly exceed the minimum, depending on traffic volumes and the highway facility. The DTM calculates the "delay penalty" during PS&E. The second method is for the state representative to suspend the contract work.

A contractor or the Department forces (such as Maintenance) can be ordered to pick up a lane closure early if traffic impacts become significant either due to a project incident or activities outside the project area. Early pickup should only be ordered when traveler and worker safety will not be compromised. The "maintaining traffic" SSPs for capital projects provide for compensating contractors for early pickup. Encroachment Permit provisions require the permittee to pick up a closure early without compensation.

DTM's are to ensure that lane closures will not be terminated early, or may be extended beyond the lane closure window when the activity needs to be completed for the safety of the public or workers. These activities may include structure inspections and repairs, guardrail repairs, culvert replacement.

In order to avoid significant traffic impacts, it is essential to monitor and respond immediately to delay, pick up closures on time, and have solid traffic and contractor contingency plans.

A Department staff member who can make informed decisions about implementing contingency plans and modifying, terminating or extending approved lane closures should be available to respond to significant delays and other unexpected events whenever lane closures are in place.

The designated employee(s) may be Traffic Operations, Construction, or TMC staff, depending on the district.

At the end of the project a post-TMP evaluation report must be completed by the TMP Manager for all major TMPs and for TMPs where the actual delay exceeded the threshold set by the DTM. Post-TMP meetings with the CHP and other partners can be held to identify what went well and what could have been done differently. Samples of past TMP reports can be obtained from headquarters' Traffic Operations, Office of System Management Operations and from the DTM.

Contingency Plan

Both traffic and contractor contingency plans are required for all planned work. Both blanket and individual TMPs must include contingency plans. The traffic contingency plan, prepared by the Department or a consultant, addresses specific actions that will be taken to restore or minimize affects on traffic when the congestion or delay exceeds original estimates due to unforeseen events such as work-zone accidents, higher than predicted traffic demand, or delayed lane closures. The contractor contingency plan addresses activities under the contractor's control in the work zone. After the contractor's contingency plan is submitted and approved, it becomes part of the TMP contingency plan.

The TMP contingency plan should include, but is not limited to the following:

- Information that clearly defines trigger points which require lane closure termination (i.e., inclement weather, length of traffic queue exceeds threshold;
- Decision tree with clearly defined lines of communication and authority;
- Specific duties of all participants during lane closure operations, such as, coordination with CHP or local police, etc.;
- Names, phone numbers and pager numbers for the DTM or their designee, the Resident Engineer (RE), the Maintenance Superintendent, the Permit Inspector, the on-site traffic advisor, the CHP Division or Area Commander, appropriate local agency representatives, and other applicable personnel;
- Coordination strategy (and special agreements if applicable) between DTM, RE, on-site traffic advisor, Maintenance, CHP and local agencies;
- Contractor's contingency plan;
- Standby equipment, State personnel, and availability of local agency personnel for callout (normally requires a Cooperative Agreement);
- Development of contingencies based on maintaining minimum service level.

G. RETROFITTING PROGRAMMED PROJECTS

Usually the extent of the TMP is to be determined prior to programming (PID approval). However, it may sometimes be necessary to retrofit a TMP to a project that is already programmed due to project changes, policy changes, emergencies or unforeseen conditions. These projects must be handled on a case by case basis since the course of action will depend on how far along the project development process is and how extensive the TMP needs to be. Retrofitted TMPs may require a TMP team and TMP Manager and involvement from all functional units as discussed earlier in these guidelines. The project manager is responsible for

initiating a TMP investigation since they are most knowledgeable of project status. Some suggestions for funding retrofitted TMP are:

Use of Minor Funds

Minor A and B money has been used to pay for TMP measures that total less than \$1,000,000. The districts will not usually be reimbursed for this even though the FHWA agrees to participate (it is not economically feasible for the Department to process minor funds for reimbursement). There have been exceptions however, and that decision is at the discretion of the Federal Resources Branch in headquarters Budgets Program.

Charge to Other Project Phase 4 (Construction) Funds

Funds from other construction contracts in the district may be used if those projects are in the vicinity of, or will be affected by, the project requiring TMP funds. At the discretion of the Deputy District Director for Construction a list of chargeable project EAs may be submitted to headquarters Accounting for prorated charging. Very few Accounting staff are aware of the process required and headquarters Traffic Operations, Office of System Management Operations should be contacted for assistance.

Project Cost or Scope Changes

The CTC has delegated to the Director of the Department the authority to increase a project's cost by up to 20 percent without prior commission approval. This authority has been delegated to other Department managers as described in Project Management Directive PMD6. This increase can be used for TMP implementation and will be 100 percent reimbursable by the FHWA. The increased costs must be absorbed by other projects in the district since the total capital outlay allocation remains the same.

H. LOCAL INVOLVEMENT

The TMP Deputy Directive 60 applies to all projects on state facilities, including those not funded by the state. District Directors are responsible for assuring local compliance. Since many measure projects are split funded, the Department and local entities must work cooperatively to develop an effective TMP. The Department is responsible for approving all PSRs and it is at this point that agreements should be reached concerning the costs and scope of TMP measures.

III. CORRIDOR, REGIONAL AND MULTI-FUNCTIONAL AREA TMPs

When multiple or consecutive projects are within the same general corridor, the cumulative impact can result in excessive traffic delays and detour conflicts. These may be multiple capital projects, the involvement of more than one district, or a combination of capital projects and Encroachment Permit and/or Maintenance activities. Corridor or regional coordination will minimize or eliminate these impacts and reduce inconvenience to the motoring public.

When multiple projects are in the same corridor or on corridors within the same traffic area, it may be possible to develop a single corridor or regional TMP. In other cases, individual TMPs are developed and funded from their own sources, and a bare-bones corridor or regional TMP addresses the cumulative impact. Each project covered by corridor and regional TMP contributes resources in proportion to its traffic impact. During TMP implementation, the TMC serves as an information clearinghouse and coordinates operations. The TMC helps identify conflicts and recommends appropriate action. When provided with accurate and up-to-date lane closure information the TMC provides real-time traffic information via electronic media, CMS, and HAR.

The TMP Manager coordinates the development and implementation of corridor and regional TMPs. The TMP Manager forms a TMP team including, as a minimum, representatives from Construction, Maintenance, Public Affairs and Traffic Operations for each of the affected districts. The initial meeting is held several months in advance of the construction season to set milestones, and allow time to gather project information and prepare and distribute information.

The corridor/regional TMP may need elements in addition to those provided by the individual TMP for each project. Those elements may include changeable message signs at key locations outside individual project limits, the establishment of an information hot line and web-sites for all projects involved. The use of the statewide Caltrans Highway Information Network (CHIN) number (1-800-427-ROAD), and particularly the use of TMCs as a central reporting hub. The Northern Valley TMC in District 3 has established reporting procedures specifically for interregional TMPs that are obtainable from headquarters Traffic Operations.

IV. MAJOR LANE CLOSURE APPROVAL PROCESS

This process applies to all major lane closures on the state highway system. Major lane closures are those lane closures that are expected to result in significant traffic impacts despite the implementation of TMPs. A "significant traffic impact" is defined in DD-60 as (a) 30 minutes above normal recurring traffic delay on the facility, or (b) the delay threshold set by the DTM, whichever is less. When a planned lane closure is expected to have a significant traffic impact, Headquarters District Lane Closure Review Committee (DLCRC) review and approval is required. The functional unit directly involved in the work must submit the major lane closure request to the DLCRC for approval as detailed below.

A traveler's trip should not be increased by more than 30 minutes due to planned Department activities. The DTM may set a lower maximum if the economic impact of a delay over 20 minutes would be high. The lesser of these delay limits is the maximum delay threshold allowed for any activity. Only the DLCRC can approve a higher delay threshold for a project.

Additionally, it should be noted that TMP activities are comprehensive, and involve actions in addition to traffic management through the work zone, as detailed in these TMP Guidelines. All lane closure operations and other planned activities should be evaluated at the earliest possible developmental stage for potential impacts and mitigation strategies. Pre-implementation meetings and contingency plans remain important aspects of all lane closure operations to minimize impacts of unforeseen events.

A. THRESHOLD CRITERIA FOR LANE CLOSURES REQUIRING APPROVAL OF THE DLCRC

DLCRC review and approval is required when planned activities are expected to result in a traffic delay that exceeds 30 minutes or the delay threshold set by the DTM, which ever is less.

DLCRC review and approval is not required for emergency closures due to natural events or incidents. However, the DTM must be notified, and every effort must be made to minimize traveler delay and reopen traffic lanes as soon as practical.

Applicability

The DLCRC, comprised of the CHP, District Public Information Officer, and Deputy District Directors of Construction, Design, Maintenance and Operations, approves all requests for major lane closures that meet the above threshold criteria. The criteria are applicable for moving or static lane closure operations. The DLCRC will decide when to submit lane closure requests that

are of an interregional, statewide, environmental, or otherwise sensitive nature to the Headquarters Lane Closure Review Committee (HQLCRC) for their approval.

The DLCRC is responsible for determining when HQLCRC approval is required. The HQLCRC is comprised of the Division Chiefs for Construction, Maintenance, Design and Local Programs, and Traffic Operations along with the Headquarters Public Information Officer, and a representative from the CHP. The HQLCRC may review the closure or leave the decision to the DLCRC. The HQLCRC should be advised of all planned lane closures that exceed the above threshold criteria. All planned lane closures that exceed the above threshold criteria and are of an interregional, statewide, environmental, or otherwise sensitive nature, as determined by the district LCRC, may also require approval of the HQLCRC.

Contents of Major Lane Closure Request Submittal

The functional unit requesting the lane closure and responsible for its performance prepares a proposed lane closure submittal. Sufficient information is provided to ensure complete understanding of the proposal. The submittal is sent through the DTM for review before sending it on to the LCRC. If additional TMP efforts can reduce the expected additional delay to less than 30 minutes, then the closure does not have to go to the LCRC. The DLCRC/HQLCRC may require additional information during its review. At a minimum, the following information is recommended initially:

1. Location and vicinity maps showing the state highway(s), local street network, and other adjacent lane closures or nearby work that may affect traffic during the same period, including special events;
2. Dates, times and locations of the lane closure(s);
3. Brief description of the work being performed during the lane closure(s);
4. Brief description of each lane closure and its anticipated affect on traffic;
5. Amount of expected delay and corresponding queue length for each lane closure;
6. Summary of TMP strategies that will be used to reduce delay and motorist inconvenience during the lane closure(s) (refer to Table 1). A copy of the approved TMP for the project, if available;
7. Contingency plan (see "Contingency Plan" below).

B. EVALUATION

The LCRC is responsible for approving major lane closures and will use the items below for evaluating lane closure operations. In its evaluation of the proposal, the LCRC will give consideration to the accuracy, reliability, and completeness of information provided as well as other reliable sources of information available to the LCRC.

Proposals will be evaluated on the basis of effectiveness in the following areas:

- Promoting motorist and worker safety;
- TMP strategies;
- Plans for coordination with adjacent construction, maintenance, encroachment permits, and special events;

- Plans for coordination with TMC and field personnel;
- Plans for coordination with public media;
- Plans for use of existing field elements such as traffic surveillance loops, changeable message signs, highway advisory radio, and Closed Circuit Television cameras;
- Lines of communication and authority (top to bottom);
- Plans for monitoring delay (or corresponding queue length) during lane closure operations;
- Alternatives to proposed closures;
- Viability of contingency plans;

C. Post-Closure Evaluation Statement

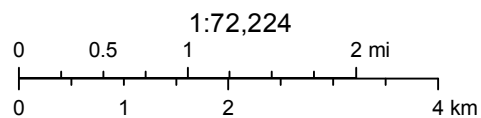
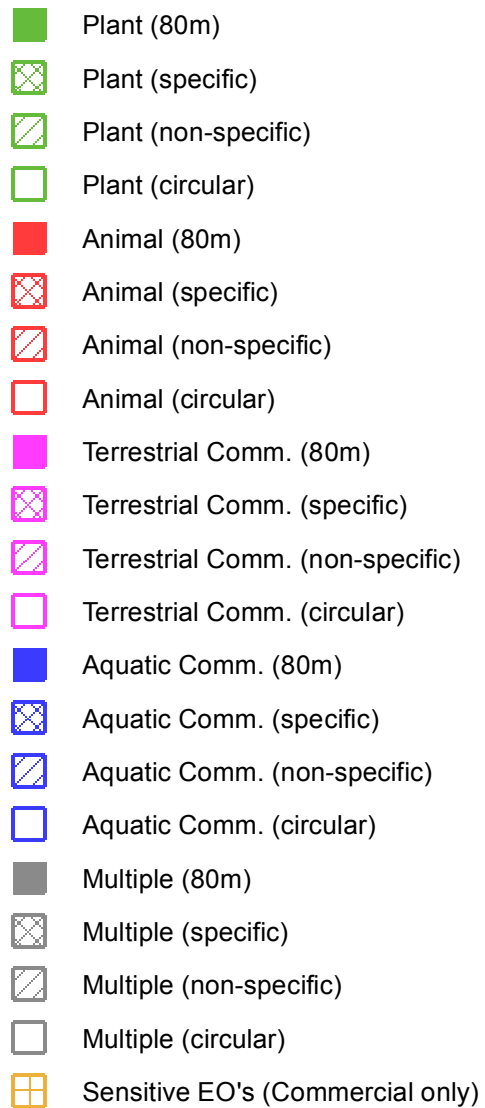
A Post-Closure Evaluation statement will be submitted to headquarters' Traffic Operations Program, Office of System Management Operations, on all projects that exceed expected delay or run outside of the closure window. No more than one page is suggested. The functional unit performing the lane closure will prepare the statement within five working days of the date the lane closure exceeded the threshold criteria. The statement should explain:

- The cause and impact of delays;
- Either actions taken or to be taken to avoid or mitigate an occurrence or recurrence;
- Why the expected delay was exceeded and/or why it was necessary to exceed the closure window;
- How the situation can be avoided in the future.

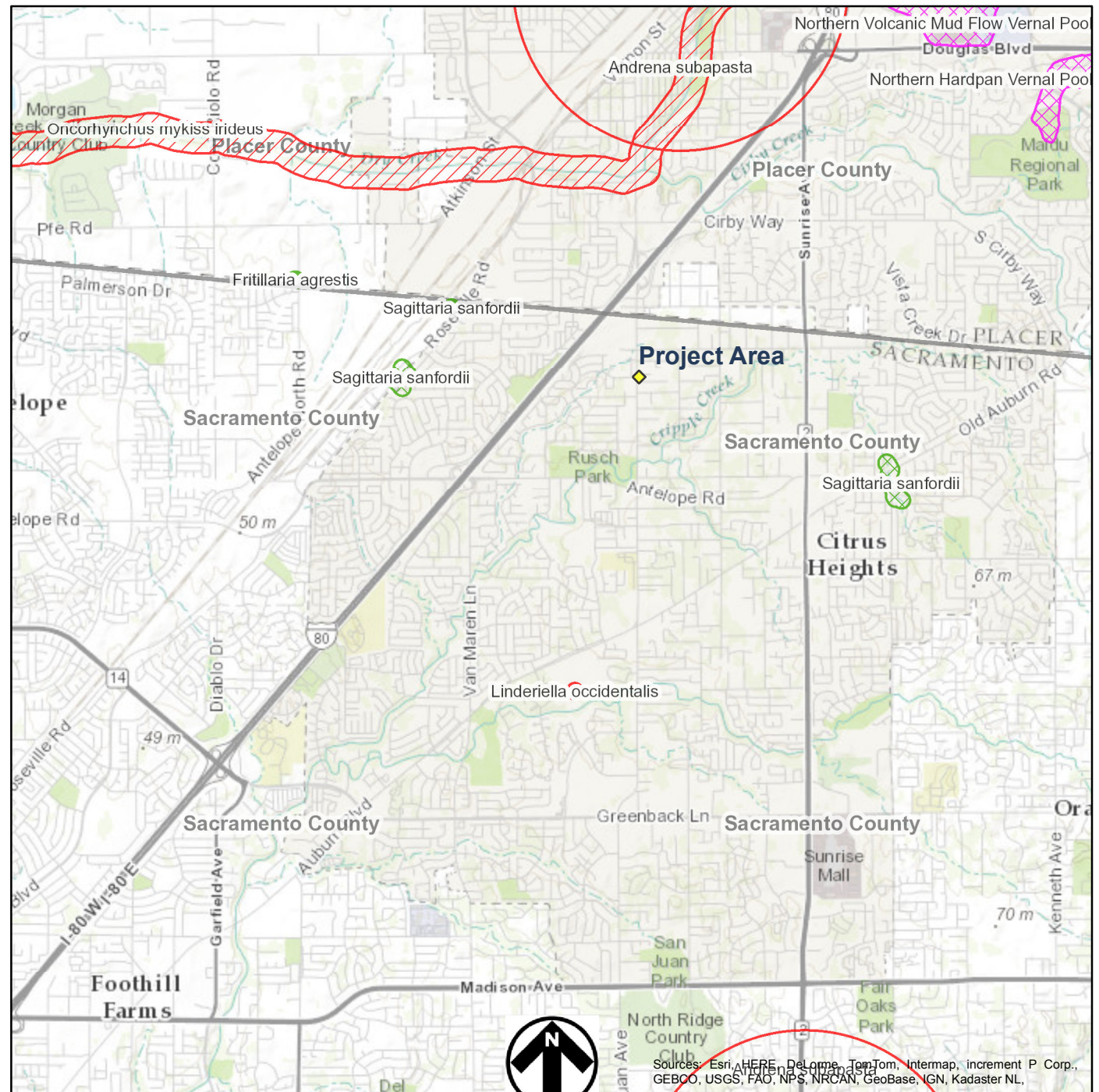
Post-closure evaluation statements are only for closures formally approved by the District LCRC under this process (i.e. exceed the lesser of 30 minutes or the DTM limit).

APPENDIX C: CNDDDB, USFWS, and CNPS Special Status Species Database Results

Auburn Boulevard Complete Streets, Phase 2



June 25, 2015





Selected Elements by Scientific Name

California Department of Fish and Wildlife

California Natural Diversity Database



Query Criteria: Quad is (Carmichael (3812153) or Citrus Heights (3812163) or Folsom (3812162) or Roseville (3812173))

Species	Element Code	Federal Status	State Status	Global Rank	State Rank	Rare Plant Rank/CDFW SSC or FP
<i>Accipiter cooperii</i> Cooper's hawk	ABNKC12040	None	None	G5	S4	WL
<i>Agelaius tricolor</i> tricolored blackbird	ABPBXB0020	None	Endangered	G2G3	S1S2	SSC
<i>Alkali Meadow</i> Alkali Meadow	CTT45310CA	None	None	G3	S2.1	
<i>Alkali Seep</i> Alkali Seep	CTT45320CA	None	None	G3	S2.1	
<i>Ammodramus savannarum</i> grasshopper sparrow	ABPBXA0020	None	None	G5	S3	SSC
<i>Andrena subapasta</i> an andrenid bee	IIHYM35210	None	None	G1G2	S1S2	
<i>Antrozous pallidus</i> pallid bat	AMACC10010	None	None	G5	S3	SSC
<i>Aquila chrysaetos</i> golden eagle	ABNKC22010	None	None	G5	S3	FP
<i>Ardea alba</i> great egret	ABNGA04040	None	None	G5	S4	
<i>Ardea herodias</i> great blue heron	ABNGA04010	None	None	G5	S4	
<i>Athene cunicularia</i> burrowing owl	ABNSB10010	None	None	G4	S3	SSC
<i>Balsamorhiza macrolepis</i> big-scale balsamroot	PDAST11061	None	None	G2	S2	1B.2
<i>Branchinecta lynchi</i> vernal pool fairy shrimp	ICBRA03030	Threatened	None	G3	S2S3	
<i>Branchinecta mesovallensis</i> midvalley fairy shrimp	ICBRA03150	None	None	G2	S2	
<i>Buteo regalis</i> ferruginous hawk	ABNKC19120	None	None	G4	S3S4	WL
<i>Buteo swainsoni</i> Swainson's hawk	ABNKC19070	None	Threatened	G5	S3	
<i>Chloropyron molle ssp. hispidum</i> hispid salty bird's-beak	PDSCR0J0D1	None	None	G2T2	S2	1B.1
<i>Clarkia biloba ssp. brandegeae</i> Brandegee's clarkia	PDONA05053	None	None	G4G5T4	S4	4.2
<i>Desmocerus californicus dimorphus</i> valley elderberry longhorn beetle	IICOL48011	Threatened	None	G3T2	S2	
<i>Downingia pusilla</i> dwarf downingia	PDCAM060C0	None	None	GU	S2	2B.2



Selected Elements by Scientific Name

California Department of Fish and Wildlife

California Natural Diversity Database



Species	Element Code	Federal Status	State Status	Global Rank	State Rank	Rare Plant Rank/CDFW SSC or FP
<i>Dumontia oregonensis</i> hairy water flea	ICBRA23010	None	None	G1G3	S1	
<i>Elanus leucurus</i> white-tailed kite	ABNKC06010	None	None	G5	S3S4	FP
<i>Emys marmorata</i> western pond turtle	ARAAD02030	None	None	G3G4	S3	SSC
<i>Falco columbarius</i> merlin	ABNKD06030	None	None	G5	S3S4	WL
<i>Fritillaria agrestis</i> stinkbells	PMLIL0V010	None	None	G3	S3	4.2
<i>Gratiola heterosepala</i> Boggs Lake hedge-hyssop	PDSCR0R060	None	Endangered	G2	S2	1B.2
<i>Hydrochara rickseckeri</i> Ricksecker's water scavenger beetle	IICOL5V010	None	None	G2?	S2?	
<i>Juncus leiospermus</i> var. <i>ahartii</i> Ahart's dwarf rush	PMJUN011L1	None	None	G2T1	S1	1B.2
<i>Juncus leiospermus</i> var. <i>leiospermus</i> Red Bluff dwarf rush	PMJUN011L2	None	None	G2T2	S2	1B.1
<i>Lasionycteris noctivagans</i> silver-haired bat	AMACC02010	None	None	G5	S3S4	
<i>Legenere limosa</i> legenere	PDCAM0C010	None	None	G2	S2	1B.1
<i>Lepidurus packardii</i> vernal pool tadpole shrimp	ICBRA10010	Endangered	None	G3	S2S3	
<i>Linderiella occidentalis</i> California linderiella	ICBRA06010	None	None	G2G3	S2S3	
<i>Navarretia myersii</i> ssp. <i>myersii</i> pincushion navarretia	PDPLM0C0X1	None	None	G1T1	S1	1B.1
Northern Hardpan Vernal Pool Northern Hardpan Vernal Pool	CTT44110CA	None	None	G3	S3.1	
Northern Volcanic Mud Flow Vernal Pool Northern Volcanic Mud Flow Vernal Pool	CTT44132CA	None	None	G1	S1.1	
<i>Oncorhynchus mykiss irideus</i> steelhead - Central Valley DPS	AFCHA0209K	Threatened	None	G5T2Q	S2	
<i>Orcuttia viscida</i> Sacramento Orcutt grass	PMPOA4G070	Endangered	Endangered	G1	S1	1B.1
<i>Phalacrocorax auritus</i> double-crested cormorant	ABNFD01020	None	None	G5	S4	WL
<i>Progne subis</i> purple martin	ABPAU01010	None	None	G5	S3	SSC
<i>Riparia riparia</i> bank swallow	ABPAU08010	None	Threatened	G5	S2	



Selected Elements by Scientific Name
California Department of Fish and Wildlife
California Natural Diversity Database



Species	Element Code	Federal Status	State Status	Global Rank	State Rank	Rare Plant Rank/CDFW SSC or FP
<i>Sagittaria sanfordii</i> Sanford's arrowhead	PMALI040Q0	None	None	G3	S3	1B.2
<i>Spea hammondi</i> western spadefoot	AAABF02020	None	None	G3	S3	SSC
<i>Taxidea taxus</i> American badger	AMAJF04010	None	None	G5	S3	SSC
Valley Needlegrass Grassland Valley Needlegrass Grassland	CTT42110CA	None	None	G3	S3.1	

Record Count: 45



Plant List

1 matches found. *Click on scientific name for details*

Search Criteria

Rare Plant Rank is one of [1A, 1B, 2A, 2B, 3], Found in Quad 38121F3

Scientific Name	Common Name	Family	Lifeform	Rare Plant Rank	State Rank	Global Rank
Sagittaria sanfordii	Sanford's arrowhead	Alismataceae	perennial rhizomatous herb	1B.2	S3	G3

Suggested Citation

CNPS, Rare Plant Program. 2015. Inventory of Rare and Endangered Plants (online edition, v8-02). California Native Plant Society, Sacramento, CA. Website <http://www.rareplants.cnps.org> [accessed 25 June 2015].

Search the Inventory

[Simple Search](#)

[Advanced Search](#)

[Glossary](#)

Information

[About the Inventory](#)

[About the Rare Plant Program](#)

[CNPS Home Page](#)

[About CNPS](#)

[Join CNPS](#)

Contributors

[The Calflora Database](#)

[The California Lichen Society](#)

© Copyright 2010-2014 California Native Plant Society. All rights reserved.



Plant List

4 matches found. *Click on scientific name for details*

Search Criteria

Rare Plant Rank is one of [1A, 1B, 2A, 2B, 3], Found in Quad 38121E3

Scientific Name	Common Name	Family	Lifeform	Rare Plant Rank	State Rank	Global Rank
<u>Gratiola heterosepala</u>	Boggs Lake hedge-hyssop	Plantaginaceae	annual herb	1B.2	S2	G2
<u>Juncus leiospermus var. ahartii</u>	Ahart's dwarf rush	Juncaceae	annual herb	1B.2	S1	G2T1
<u>Legenere limosa</u>	legenere	Campanulaceae	annual herb	1B.1	S2	G2
<u>Sagittaria sanfordii</u>	Sanford's arrowhead	Alismataceae	perennial rhizomatous herb	1B.2	S3	G3

Suggested Citation

CNPS, Rare Plant Program. 2015. Inventory of Rare and Endangered Plants (online edition, v8-02). California Native Plant Society, Sacramento, CA. Website <http://www.rareplants.cnps.org> [accessed 25 June 2015].

Search the Inventory

[Simple Search](#)

[Advanced Search](#)

[Glossary](#)

Information

[About the Inventory](#)

[About the Rare Plant Program](#)

[CNPS Home Page](#)

[About CNPS](#)

[Join CNPS](#)

Contributors

[The Calflora Database](#)

[The California Lichen Society](#)

© Copyright 2010-2014 California Native Plant Society. All rights reserved.



Plant List

3 matches found. *Click on scientific name for details*

Search Criteria

Rare Plant Rank is one of [1A, 1B, 2A, 2B, 3], Found in Quad 38121F2

Scientific Name	Common Name	Family	Lifeform	Rare Plant Rank	State Rank	Global Rank
Downingia pusilla	dwarf downingia	Campanulaceae	annual herb	2B.2	S2	GU
Navarretia myersii ssp. myersii	pincushion navarretia	Polemoniaceae	annual herb	1B.1	S1	G1T1
Orcuttia viscida	Sacramento Orcutt grass	Poaceae	annual herb	1B.1	S1	G1

Suggested Citation

CNPS, Rare Plant Program. 2015. Inventory of Rare and Endangered Plants (online edition, v8-02). California Native Plant Society, Sacramento, CA. Website <http://www.rareplants.cnps.org> [accessed 25 June 2015].

Search the Inventory

[Simple Search](#)

[Advanced Search](#)

[Glossary](#)

Information

[About the Inventory](#)

[About the Rare Plant Program](#)

[CNPS Home Page](#)

[About CNPS](#)

[Join CNPS](#)

Contributors

[The Calflora Database](#)

[The California Lichen Society](#)

© Copyright 2010-2014 California Native Plant Society. All rights reserved.



Plant List

6 matches found. *Click on scientific name for details*

Search Criteria

Rare Plant Rank is one of [1A, 1B, 2A, 2B, 3], Found in Quad 38121G3

Scientific Name	Common Name	Family	Lifeform	Rare Plant Rank	State Rank	Global Rank
<u>Balsamorhiza macrolepis</u>	big-scale balsamroot	Asteraceae	perennial herb	1B.2	S2	G2
<u>Chloropyron molle ssp. hispidum</u>	hispid bird's-beak	Orobanchaceae	annual herb (hemiparasitic)	1B.1	S2	G2T2
<u>Downingia pusilla</u>	dwarf downingia	Campanulaceae	annual herb	2B.2	S2	GU
<u>Gratiola heterosepala</u>	Boggs Lake hedge-hyssop	Plantaginaceae	annual herb	1B.2	S2	G2
<u>Juncus leiostermus var. leiostermus</u>	Red Bluff dwarf rush	Juncaceae	annual herb	1B.1	S2	G2T2
<u>Legenere limosa</u>	legenere	Campanulaceae	annual herb	1B.1	S2	G2

Suggested Citation

CNPS, Rare Plant Program. 2015. Inventory of Rare and Endangered Plants (online edition, v8-02). California Native Plant Society, Sacramento, CA. Website <http://www.rareplants.cnps.org> [accessed 25 June 2015].

Search the Inventory

[Simple Search](#)

[Advanced Search](#)

[Glossary](#)

Information

[About the Inventory](#)

[About the Rare Plant Program](#)

[CNPS Home Page](#)

[About CNPS](#)

[Join CNPS](#)

Contributors

[The Calflora Database](#)

[The California Lichen Society](#)



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Sacramento Fish and Wildlife Office

FEDERAL BUILDING, 2800 COTTAGE WAY, ROOM W-2605

SACRAMENTO, CA 95825

PHONE: (916)414-6600 FAX: (916)414-6713



Consultation Code: 08ESMF00-2015-SLI-0759

June 25, 2015

Event Code: 08ESMF00-2015-E-02569

Project Name: Auburn Boulevard Complete Streets, Phase 2

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, under the jurisdiction of the U.S. Fish and Wildlife Service (Service) that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the Service under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

Please follow the link below to see if your proposed project has the potential to affect other species or their habitats under the jurisdiction of the National Marine Fisheries Service:

http://www.nwr.noaa.gov/protected_species/species_list/species_lists.html

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the ECOS-IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2)

of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2) (c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF>

Please be aware that bald and golden eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*), and projects affecting these species may require development of an eagle conservation plan (http://www.fws.gov/windenergy/eagle_guidance.html). Additionally, wind energy projects should follow the wind energy guidelines (<http://www.fws.gov/windenergy/>) for minimizing impacts to migratory birds and bats.

Guidance for minimizing impacts to migratory birds for projects including communications towers (e.g., cellular, digital television, radio, and emergency broadcast) can be found at: <http://www.fws.gov/migratorybirds/CurrentBirdIssues/Hazards/towers/towers.htm>; <http://www.towerkill.com>; and <http://www.fws.gov/migratorybirds/CurrentBirdIssues/Hazards/towers/comtow.html>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

The table below outlines lead FWS field offices by county and land ownership/project type. Please refer to this table when you are ready to coordinate (including requests for section 7 consultation) with the field office corresponding to your project, and send any documentation regarding your project to that corresponding office. Therefore, the lead FWS field office may not be the office listed above in the letterhead. Please visit our office's website (<http://www.fws.gov/sacramento>) to view a map of office jurisdictions.

Lead FWS offices by County and Ownership/Program

County	Ownership/Program	Species	Office Lead*
Alameda	Tidal wetlands/marsh adjacent to Bays	Salt marsh species, delta smelt	BDFWO
Alameda	All ownerships but tidal/estuarine	All	SFWO
Alpine	Humboldt Toiyabe National Forest	All	RFWO
Alpine	Lake Tahoe Basin Management Unit	All	RFWO
Alpine	Stanislaus National Forest	All	SFWO
Alpine	El Dorado National Forest	All	SFWO
Colusa	Mendocino National Forest	All	AFWO
Colusa	Other	All	By jurisdiction (see map)
Contra Costa	Legal Delta (Excluding ECCHCP)	All	BDFWO
Contra Costa	Antioch Dunes NWR	All	BDFWO
Contra Costa	Tidal wetlands/marsh adjacent to Bays	Salt marsh species, delta smelt	BDFWO
Contra Costa	All ownerships but tidal/estuarine	All	SFWO

El Dorado	El Dorado National Forest	All	SFWO
El Dorado	LakeTahoe Basin Management Unit		RFWO
Glenn	Mendocino National Forest	All	AFWO
Glenn	Other	All	By jurisdiction (see map)
Lake	Mendocino National Forest	All	AFWO
Lake	Other	All	By jurisdiction (see map)
Lassen	Modoc National Forest	All	KFWO
Lassen	Lassen National Forest	All	SFWO
Lassen	Toiyabe National Forest	All	RFWO
Lassen	BLM Surprise and Eagle Lake Resource Areas	All	RFWO
Lassen	BLM Alturas Resource Area	All	KFWO
Lassen	Lassen Volcanic National Park	All (includes Eagle Lake trout on all ownerships)	SFWO
Lassen	All other ownerships	All	By jurisdiction (see map)

Marin	Tidal wetlands/marsh adjacent to Bays	Salt marsh species, delta smelt	BDFWO
Marin	All ownerships but tidal/estuarine	All	SFWO
Mendocino	Russian River watershed	All	SFWO
Mendocino	All except Russian River watershed	All	AFWO
Napa	All ownerships but tidal/estuarine	All	SFWO
Napa	Tidal wetlands/marsh adjacent to San Pablo Bay	Salt marsh species, delta smelt	BDFWO
Nevada	Humboldt Toiyabe National Forest	All	RFWO
Nevada	All other ownerships	All	By jurisdiction (See map)
Placer	Lake Tahoe Basin Management Unit	All	RFWO
Placer	All other ownerships	All	SFWO
Sacramento	Legal Delta	Delta Smelt	BDFWO
Sacramento	Other	All	By jurisdiction (see map)
San Francisco	Tidal wetlands/marsh adjacent to San Francisco Bay	Salt marsh species, delta smelt	BDFWO

San Francisco	All ownerships but tidal/estuarine	All	SFWO
San Mateo	Tidal wetlands/marsh adjacent to San Francisco Bay	Salt marsh species, delta smelt	BDFWO
San Mateo	All ownerships but tidal/estuarine	All	SFWO
San Joaquin	Legal Delta excluding San Joaquin HCP	All	BDFWO
San Joaquin	Other	All	SFWO
Santa Clara	Tidal wetlands/marsh adjacent to San Francisco Bay	Salt marsh species, delta smelt	BDFWO
Santa Clara	All ownerships but tidal/estuarine	All	SFWO
Shasta	Shasta Trinity National Forest except Hat Creek Ranger District (administered by Lassen National Forest)	All	YFWO
Shasta	Hat Creek Ranger District	All	SFWO
Shasta	Bureau of Reclamation (Central Valley Project)	All	BDFWO
Shasta	Whiskeytown National Recreation Area	All	YFWO
Shasta	BLM Alturas Resource Area	All	KFWO

Shasta	Caltrans	By jurisdiction	SFWO/AFWO
Shasta	Ahjumawi Lava Springs State Park	Shasta crayfish	SFWO
Shasta	All other ownerships	All	By jurisdiction (see map)
Shasta	Natural Resource Damage Assessment, all lands	All	SFWO/BDFWO
Sierra	Humboldt Toiyabe National Forest	All	RFWO
Sierra	All other ownerships	All	SFWO
Solano	Suisun Marsh	All	BDFWO
Solano	Tidal wetlands/marsh adjacent to San Pablo Bay	Salt marsh species, delta smelt	BDFWO
Solano	All ownerships but tidal/estuarine	All	SFWO
Solano	Other	All	By jurisdiction (see map)
Sonoma	Tidal wetlands/marsh adjacent to San Pablo Bay	Salt marsh species, delta smelt	BDFWO
Sonoma	All ownerships but tidal/estuarine	All	SFWO
Tehama	Mendocino National Forest	All	AFWO
	Shasta Trinity National Forest		

Tehama	except Hat Creek Ranger District (administered by Lassen National Forest)	All	YFWO
Tehama	All other ownerships	All	By jurisdiction (see map)
Yolo	Yolo Bypass	All	BDFWO
Yolo	Other	All	By jurisdiction (see map)
All	FERC-ESA	All	By jurisdiction (see map)
All	FERC-ESA	Shasta crayfish	SFWO
All	FERC-Relicensing (non-ESA)	All	BDFWO
*Office Leads:			
AFWO=Arcata Fish and Wildlife Office			
BDFWO=Bay Delta Fish and Wildlife Office			
KFWO=Klamath Falls Fish and Wildlife Office			
RFWO=Reno Fish and Wildlife Office			
YFWO=Yreka Fish and Wildlife Office			

Attachment



United States Department of Interior
Fish and Wildlife Service

Project name: Auburn Boulevard Complete Streets, Phase 2

Official Species List

Provided by:

Sacramento Fish and Wildlife Office
FEDERAL BUILDING
2800 COTTAGE WAY, ROOM W-2605
SACRAMENTO, CA 95825
(916) 414-6600

Consultation Code: 08ESMF00-2015-SLI-0759

Event Code: 08ESMF00-2015-E-02569

Project Type: RECREATION CONSTRUCTION / MAINTENANCE

Project Name: Auburn Boulevard Complete Streets, Phase 2

Project Description: The Auburn Blvd Complete Phase 2 Project is located in the City of Citrus Heights, California. In February of 2005, the City of Citrus Heights adopted a specific plan to guide the revitalization and enhancement of Auburn Boulevard between Sylvan Corners and Interstate 80. The purpose of the project is for the improvement of Auburn Boulevard in order to upgrade the corridors image and improve its function as a transportation facility serving adjacent land uses.

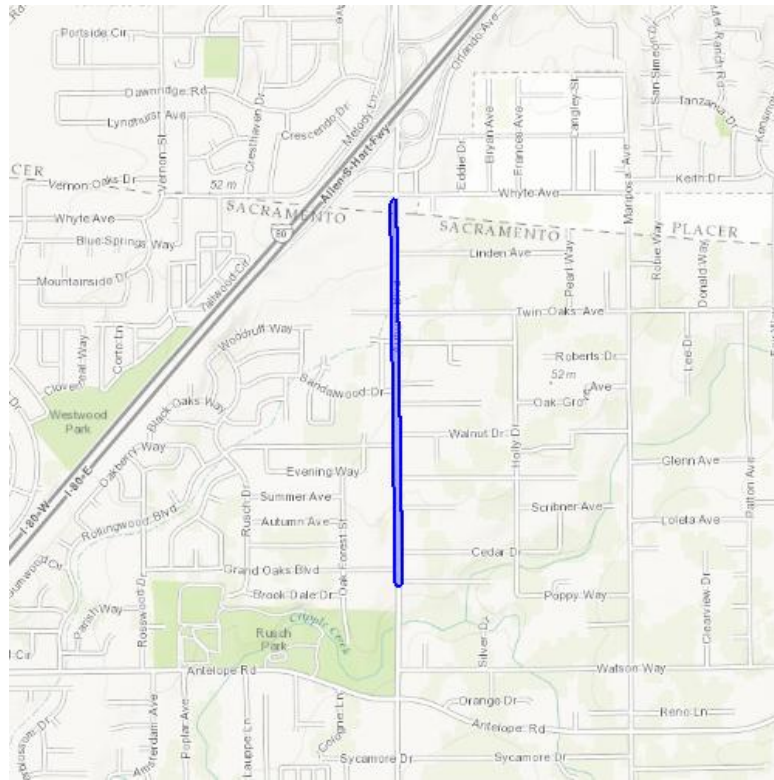
Please Note: The FWS office may have modified the Project Name and/or Project Description, so it may be different from what was submitted in your previous request. If the Consultation Code matches, the FWS considers this to be the same project. Contact the office in the 'Provided by' section of your previous Official Species list if you have any questions or concerns.



United States Department of Interior
Fish and Wildlife Service

Project name: Auburn Boulevard Complete Streets, Phase 2

Project Location Map:



Project Coordinates: MULTIPOLYGON (((-121.290191337452 38.710196708644006, -121.29022990017275 38.710099050471186, -121.29032622289033 38.71005726367628, -121.29042388106316 38.71009582639704, -121.29046566785806 38.71019214911462, -121.29065878690713 38.721811424403064, -121.29062022418638 38.721909082575884, -121.2905239014688 38.72195086937079, -121.29042624329597 38.72191230665003, -121.29038445650107 38.72181598393245, -121.290191337452 38.710196708644006)))

Project Counties: Placer, CA | Sacramento, CA



United States Department of Interior
Fish and Wildlife Service

Project name: Auburn Boulevard Complete Streets, Phase 2

Endangered Species Act Species List

There are a total of 8 threatened or endangered species on your species list. Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Critical habitats listed under the **Has Critical Habitat** column may or may not lie within your project area. See the **Critical habitats within your project area** section further below for critical habitat that lies within your project. Please contact the designated FWS office if you have questions.

Amphibians	Status	Has Critical Habitat	Condition(s)
California red-legged frog (<i>Rana draytonii</i>) Population: Entire	Threatened	Final designated	
California tiger Salamander (<i>Ambystoma californiense</i>) Population: U.S.A. (Central CA DPS)	Threatened	Final designated	
Crustaceans			
Vernal Pool fairy shrimp (<i>Branchinecta lynchi</i>) Population: Entire	Threatened	Final designated	
Vernal Pool tadpole shrimp (<i>Lepidurus packardii</i>) Population: Entire	Endangered	Final designated	
Fishes			
Delta smelt (<i>Hypomesus transpacificus</i>) Population: Entire	Threatened	Final designated	
steelhead (<i>Oncorhynchus (=salmo)</i>)	Threatened	Final designated	



United States Department of Interior
Fish and Wildlife Service

Project name: Auburn Boulevard Complete Streets, Phase 2

<i>mykiss</i> Population: Northern California DPS			
Insects			
Valley Elderberry Longhorn beetle (<i>Desmocerus californicus dimorphus</i>) Population: Entire	Threatened	Final designated	
Reptiles			
Giant Garter snake (<i>Thamnophis gigas</i>) Population: Entire	Threatened		



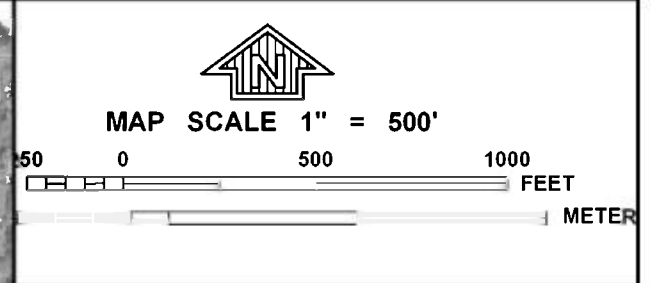
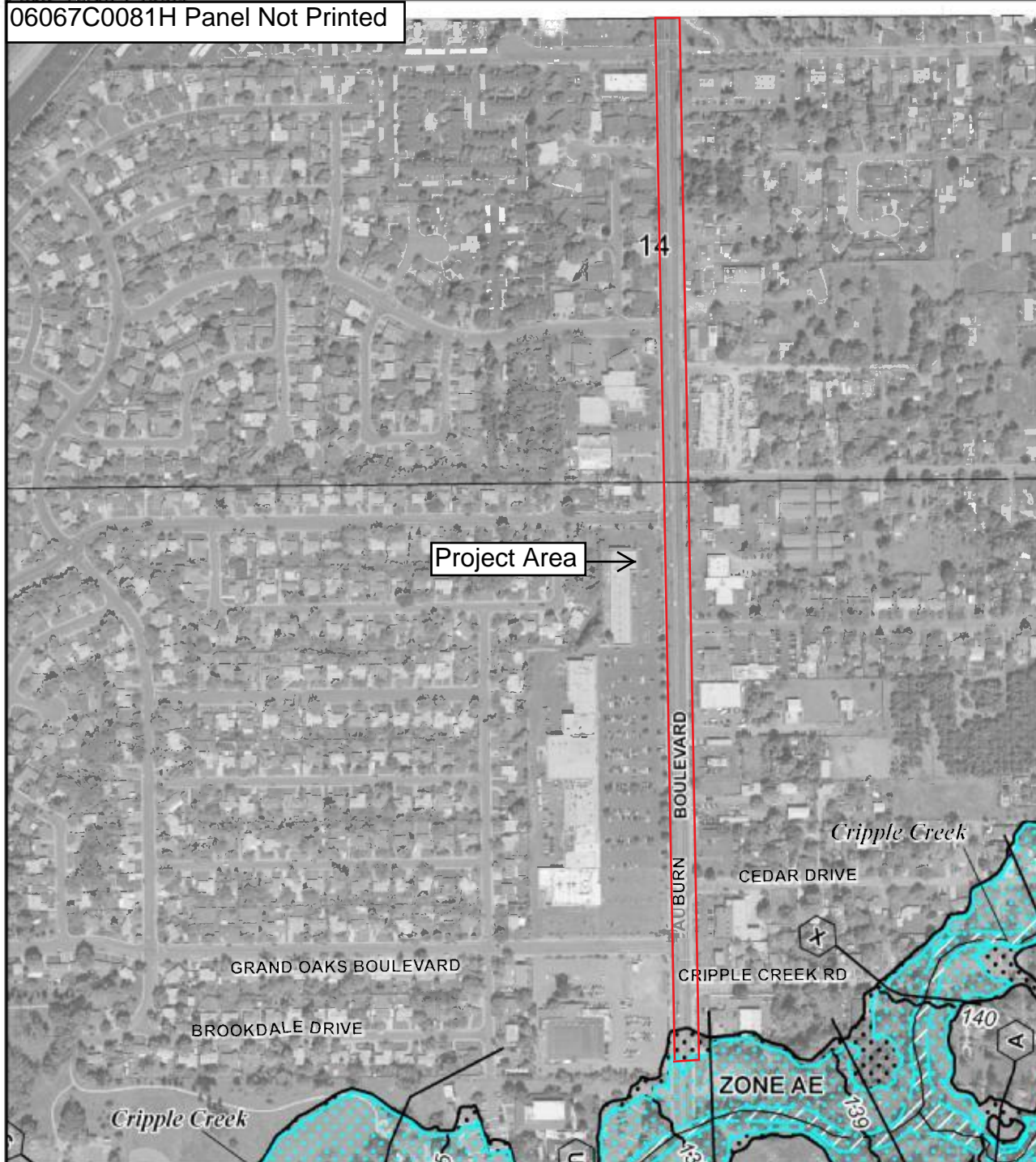
United States Department of Interior
Fish and Wildlife Service

Project name: Auburn Boulevard Complete Streets, Phase 2

Critical habitats that lie within your project area

There are no critical habitats within your project area.

APPENDIX D: FEMA Firmette Map



NATIONAL FLOOD INSURANCE PROGRAM

NFIP

PANEL 0083H

FIRM

FLOOD INSURANCE RATE MAP

SACRAMENTO COUNTY, CALIFORNIA
AND INCORPORATED AREAS

PANEL 83 OF 705
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS:

COMMUNITY	NUMBER	PANEL	SUFFIX
CITRUS HEIGHTS, CITY OF	060765	0083	H

Notice to User: The Map Number shown below should be used when placing map orders: the Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER
06067C0083H

EFFECTIVE DATE
AUGUST 16, 2012

Federal Emergency Management Agency

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at www.msc.fema.gov

APPENDIX E: Mitigation Monitoring Plan

**MITIGATION MONITORING PLAN CHECKLIST FOR THE
AUBURN BOULEVARD COMPLETE STREETS, PHASE 2 PROJECT**

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
AESTHETICS A-1: Implementation of the City of Citrus Heights General Plan Policy 37.1 and compliance with the Tree Preservation Ordinance will encourage the retention of native oaks in the landscape of the specific plan area and will mitigate for the visual impacts resulting from the removal of native oak trees. See Mitigation Measures for biological resources, B-1, B-2A and B-2B and B-2C which address impacts to the oak woodland and riparian habitat adjacent to Cripple Creek.	Prior to Construction	City of Citrus Heights and Contractor		
A-2: Implementation of the Boulevard Plan's Principles and Design Guidelines and enforcement of the City's Zoning Code Landscaping and Lighting standards will avoid impacts associated with light and glare.	Prior to and During Construction	City of Citrus Heights and Contractor		
AIR QUALITY AQ-1A: Inhalable Particulate Matter: The following mitigation measures shall be incorporated into the project to minimize the generation of PM10 dust during construction. <ul style="list-style-type: none"> • enclose, cover, or water twice daily all soil piles; • water exposed soil with adequate frequency for continued moist soil; • water all haul roads twice daily; and • cover loads of all haul/dump trucks securely. 	During Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
AQ-1B: The demolition or renovation of asbestos-containing building material is subject to the limitations of the National Emissions Standards for Hazardous Air Pollutants (NESHAP) regulations as listed in the Code of Federal Regulations (40CFR Part 61, Subpart M) requiring notification and inspection. Most demolitions and many renovations are subject to a CAL-OSHA Certified asbestos inspection prior to the start of activity. SMAQMD Rule 902, which requires District consultation and permit, applies to demolition, renovation or removal of asbestos-containing material. Compliance with these regulations is considered to reduce this impact to a less than-significant level	During Construction	City of Citrus Heights and Contractor		
AQ-2: Route and schedule construction traffic to avoid peak travel times as much as possible to reduce congestion and related air quality impacts caused by idling vehicles along local roads.	During Construction	City of Citrus Heights and Contractor		
BIOLOGY B-1: To ensure consistency with the City of Citrus Heights' Policy 37.1, which requires incorporation of existing trees into development projects, building envelopes for future development projects should be configured to minimize impacts to trees to the extent feasible. The following measures shall be implemented: 1. Building envelopes should be established on plans and specifications for the future development projects to designate the area needed for construction of roads, driveways, and building pads. 2. These building envelopes should be large enough to include not only the improvements, but also work areas for heavy equipment, staging areas, and equipment and material lay down areas. 3. To protect trees elsewhere on construction sites, no construction activities or use of heavy equipment should occur outside of the building envelopes. 4. Oaks that fall within the building envelope but which are not slated for removal should be protected by the following measures, which should be implemented during all	Prior to Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
<p>construction phases of the project:</p> <p>a. Plans and specifications should clearly state protection procedures for oaks to be preserved on the project site. The specifications should also require contractors to stay within designated work areas and should include a provision for penalties if oak trees are damaged;</p> <p>b. No vehicles, construction equipment, mobile offices, or materials should be parked or located within the driplines of oaks and other trees that are to be preserved;</p> <p>c. Soil surface removal should not occur within the driplines of oaks to be preserved. No cuts or trenching should occur within the dripline. If this area cannot be avoided, then the tree should be added to the list of oaks to be replaced through an on-site planting;</p> <p>d. Earthen fill deep should not be placed within the driplines of oak trees to be retained, and no fill should be placed within five feet of their trunks.</p> <p>e. Paving should not be placed within the dripline of oaks to be retained;</p> <p>f. Underground utility line trenching should not occur within the driplines of oaks to be retained. If it is absolutely necessary to install underground utilities within the driplines of oak trees, the trench should either be bored or drilled but not within five feet of the trunk and a certified arborist should be retained to monitor this construction and repair or wrap any damaged roots.</p> <p>g. Living Among the Oaks: A Management Guide for Landowners (UC Cooperative Extension, Berkeley) in Appendix H should be used by the City as a guide in reviewing landscape plans. The information should be distributed to landowners and developers to provide information and guidelines for preparing landscape plans and for protecting oaks after construction is complete.</p>				
<p>B-1B: Prepare and Implement Oak Replacement and Management Plan (Oak Woodland Replacement): In order to compensate for impacts due to removal of native oak trees found within oak woodland and/or riparian habitats (as opposed to isolated landscape or street trees), the following measures shall be implemented:</p> <p>1. Oak trees shall be planted on project sites or off-site in numbers and species composition similar to those impacted.</p> <p>2. Prior to approval of development or redevelopment projects, a qualified biologist or</p>	Prior to Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
<p>arborist should make an accurate count of the number, diameter, and species of trees that would be removed within each building envelope or area subject to disturbance. Based on the estimate, an Oak Replacement and Management Plan (Oak Plan) should be prepared in accordance with the City of Citrus Heights Tree Ordinance, and consistent with the City of Citrus Heights General Plan biological resource goals and policies.</p> <p>a. The goals of the Oak Plan should be to replace trees lost by the project to create healthy, self-sustaining habitats that are not dependent on maintenance or irrigation following the minimum maintenance period.</p> <p>The functions and values of the created habitat should approximate those of the affected habitats, i.e., the functions and values of oak woodland rather than an ornamental landscape planting.</p> <p>b. At a minimum, the Oak Plan should include clear success criteria, monitoring and reporting requirements, and a contingency plan should the responsible parties fail to meet the success criteria that ensure that mitigation goals and ratios are met. The Oak Plan should also include details for the species, size of plants and quantities, planting techniques, techniques for protecting the trees from herbivory, and irrigation, weed control and maintenance plan, and monitoring requirements.</p>				
<p>B-1C: Prepare and Implement Oak Replacement and Maintenance Plan (Landscape Tree Replacement): In order to compensate for impacts due to removal of native oak trees found within landscape settings (i.e. isolated landscape or street trees), the following measures shall be implemented:</p> <p>1. Oaks trees shall be planted on project sites or off-site in numbers and species composition similar to those impacted.</p> <p>a. Prior to approval of development or redevelopment projects, a qualified biologist or arborist should make an accurate count of the number, diameter, and species of trees that would be removed within each building envelope or area subject to disturbance. Based on the estimate, an Oak Replacement and Maintenance Plan (Oak Plan) should be prepared in accordance with the City of Citrus Heights Tree Ordinance, and consistent with the City of Citrus Heights General Plan biological resource goals and policies.</p> <p>The goals of the Oak Plan, at a minimum, should be to replace trees lost by the</p>	Prior to Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
project in an appropriate landscape setting that will allow trees to thrive and be self-sustaining and not dependent on maintenance or irrigation following the minimum maintenance period. Replacement within the specific plan area's planned landscape areas as street trees, trees for public space landscape or roadway medians, should be emphasized when identifying replanting sites. Replacement in a natural habitat setting as described in Measure B-2B would also accomplish these oak tree replacement goals.				
B-1D: Preconstruction Tree Survey: Prior to construction, a qualified biologist or arborist should make an accurate count of the number, diameter, condition and species of trees that would be removed by the roadway improvement project. An Oak Tree Replacement and Management Plan shall be prepared in accordance with Mitigation Measures B-2A, B-2B and B-2C described above.	Prior to Construction	City of Citrus Heights and Contractor		
B-2: <u>Avoid Impacts to Nesting Birds</u> 1. If tree removal for construction will occur during the nesting season (February through July), a minimum of two preconstruction surveys should be conducted in construction areas for nesting birds. Surveys shall be conducted by a qualified wildlife biologist. 2. Surveys should be conducted no more that 14 days prior to the initiation of tree-removal activities during the early part of the breeding season (February through April) and no more than 30 days prior to the initiation of these activities during the late part of the breeding season (May through July). 3. If the surveyor deems that an active bird nest is close enough to the construction area to be disturbed, he or she should (in consultation with CDFG) determine the extent of the construction-free buffer zone to be established around the nest. 4. Trees should be removed outside the nesting season (February through July), or after a qualified wildlife biologist verifies that the nest is empty and the nest tree is no longer used by the adults and young birds.	Prior to and During Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
B-3: Avoid Introduction and Spread of New Noxious Weeds. In the vicinity of Cripple Creek, during construction only certified weed-free straw will be used and all disturbed soils will be thoroughly covered with straw (or mulch or chips created on-site during tree removal) upon completion of grading. No seed mixes should be used unless consisting of locally native grasses and forbs.	During Construction	City of Citrus Heights and Contractor		
B-4: If construction is planned to occur during the raptor nesting season (February – August) a preconstruction raptor nesting survey shall be conducted by a qualified biologist within 7 days prior to vegetation removal. Vegetation surveyed shall include all trees, 10 feet or taller and containing a dbh of 2 inches or greater. Within 2 weeks of the nesting raptor survey, all vegetation cleared by the biologist shall be removed by the contractor. A minimum 500 foot no-disturbance buffer shall be established around any active raptor nest to limit the impacts of construction activities. The contractor shall immediately stop work in the nesting area until the appropriate buffer is established and is prohibited from conducting work that could disturb the birds (as determined by the project biologist and in coordination with wildlife agencies) in the buffer area until a qualified biologist determines the young have fledged.	Prior to and during construction – Mitigation measures shall be included in all construction documents for implementation during construction.	City of Citrus Heights and Contractor		
B-5: If ground disturbance or vegetation removal is to take place during the breeding season (February – August), a pre-construction nesting bird survey shall be conducted within 7 days prior to vegetation removal. Vegetation surveyed shall include all trees, bushes, tall grasses and emergent vegetation. Within 2 weeks of the nesting bird survey, all vegetation cleared by the biologist shall be removed by the contractor. A minimum 100 foot no-disturbance buffer shall be established around any active nest to limit the impacts of construction activities. The contractor shall immediately stop work in the nesting area until the appropriate buffer is established and is prohibited from conducting work that could disturb the birds (as determined by the project biologist and in coordination with wildlife agencies) in the buffer area until a qualified biologist determines the young have fledged.	Prior to and during construction – Mitigation measures shall be included in all construction documents for implementation during construction.	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
<p>CULTURAL RESOURCES</p> <p>CR-1: Handling of Discovered Artifacts or Remains: Should any cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, human remains, or architectural remains be encountered during any development activities, work shall be suspended according to (A) below.</p> <p>It is recommended under CEQA and Policy 41.1 of the Citrus Heights General Plan that:</p> <ol style="list-style-type: none"> 1. In the event that any prehistoric, historic, or paleontological resources are discovered during construction-related earth moving activities, all work within 50 feet of the resources shall be halted and the developer shall consult with a qualified archaeologist or paleontologist to assess the significance of the find. If any find is determined to be significant by the qualified archaeologist, then 2. representatives from the City of Citrus Heights and the qualified archaeologist and/or paleontologist would meet to determine the appropriate course of action. 3. Pursuant to Section 5097.97 of the State Public Resources Code and Section 7050.5 of the State Health and Safety Code, in the event of the discovery of human remains, all work is to stop and the County Coroner shall be immediately notified. If the remains are determined to be Native American, guidelines of the Native American Heritage Commission shall be adhered to in the treatment and disposition of the remains. And that under Policy 42.3 of the Citrus Heights General Plan that planners establish thresholds by which future projects can be judged when considering historic impacts. These standards should include height and massing considerations for projects that are located in close proximity to historic resources (individual structures and districts) and define locations for potential prehistoric resources. 	During Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
CR-2: Evaluation of Historic Resources Older than 45 Years. Prior to approval of projects or issuance of construction or grading permits, cultural resources that appear to be 45 years old or older on a project site need to be recorded for the purposes of inclusion in the State Office of Historic Preservation's filing system. "The 45 year criterion recognizes that there is commonly a five year lag between resource identification and the date that planning decisions are made" (California, State of 1995). Should the five year period lapse between the completion of the initial cultural resources documentation and the start date of the project, the cultural resource studies would need to be updated to include any additional properties/sites that would, by that time, meet the 45 year criteria.	Prior to Construction	City of Citrus Heights		
HAZARDS HM-1: Pursuant to City of Citrus Heights General Plan EIR Mitigation Measure 4.15-3a, no projects shall be approved where there is substantial evidence of existing contamination on a Cortese-listed site that would pose an unacceptable risk to the health of construction workers.	Prior to Construction	City of Citrus Heights		
HM-2: Pursuant to City of Citrus Heights General Plan EIR Mitigation Measure 4.15-3b, establish a process that identifies the steps to be taken prior to commencement of any site preparation activities on Cortese-listed sites. This may contain but not be limited to the following: <ol style="list-style-type: none"> 1. Retain a licensed professional to investigate the environmental status of the soils and/or groundwater contamination. Prepare a site plan that identifies and implements any remediation activities that are required to remove health risks to persons exposed to the site during construction activities. 2. Remove all contaminated soil, dispose of contaminated soil by a licensed contractor to a properly licensed facility, and replace contaminated soil with clean fill dirt. 3. Consult with appropriate regulatory agencies such as Department of Toxic Substances Control, Regional Water Quality Control Board, and Sacramento Department of Environmental Health to determine what actions are required by these agencies to be implemented (e.g., de-watering, groundwater monitoring, etc.). 	Prior to Construction	City of Citrus Heights		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
HM-3: Mitigation Measure HM-1A Handling of Asbestos Material: Control devices and fugitive emissions monitoring are required during demolition activities which will disturb, or have the possibility of disturbing, the asbestos-containing materials. All asbestos containing building material within the buildings planned for demolition should be removed prior to any demolition activity that could break up, dislodge, or similarly disturb these materials. This removal must be done using appropriate engineering controls, in compliance with all regulations, and be a contractor certified by the Contractor's State License Board and registered by the California Division of Occupational Safety and Health (Northwest Envirocon, Incorporated 1997).	During Construction	City of Citrus Heights and Contractor		
HM-3B: Disposal Of The Yellow Thermoplastic Traffic Stripes: Disposal of the yellow thermoplastic traffic stripes will be at a Class 1 disposal facility. All aspects of the project associated with removal, storage, transportation, and disposal of the yellow thermoplastic traffic striping, should be in strict accordance with the appropriate regulations.	During Construction	City of Citrus Heights and Contractor		
HM-4: Any leaking transformers observed during the course of the project should be considered a potential polychlorinated biphenyl (PCB) hazard. Should leaks from electrical transformers (that will either remain within the construction limits or will require removal and/or relocation) be encountered during construction, the transformer fluid should be sampled and analyzed by qualified personnel for detectable levels of PCB's. Should PCBs be detected, the transformer should be removed and disposed of in accordance with Title 22, Division 4.5 of the California Code of Regulations and any other appropriate regulatory agency. Any stained soil encountered below electrical transformers with detectable levels of PCB's should also be handled and disposed of in accordance with Title 22, Division 4.5 of the California Code of Regulations and any other appropriate regulatory agency.	During Construction	City of Citrus Heights and Contractor		
HM-5: Based on preliminary plans, right-of-way acquisition may be required at the Towne Mart gas station at Sandalwood Drive and the abandoned gas station at Oak Grove Avenue. Should final plans indicate that a portion of this parcel will be acquired for new right-of-way, a preliminary environmental screening (limited subsurface sampling and	During Construction	City of Citrus Heights and		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
laboratory analysis) should be performed during the PS&E for potentially elevated levels of petroleum hydrocarbons and MTBE contamination within the limits of construction, and/or right-of way acquisition, adjacent to the existing gas stations. Should the preliminary screening encounter elevated levels of petroleum hydrocarbons and/or MTBE a limited Phase II ISA should be performed. The Phase II ISA should consist of subsurface sampling and laboratory analysis and be of sufficient quantity to define the extent and concentration of contamination within the areal extent and depths of planned construction activities adjacent to the existing gas stations. The Phase II ISA should also provide both a Health and Safety Plan for worker safety and a Work Plan for handling and disposing contaminated soil during construction.		Contractor		
HM-6: The potential exists for hazardous contamination from historic chemical spills at Paradise Cleaners, which is located near the intersection of Auburn Boulevard and Baird Way. At the time of this ISA, there were no documented reports of soil/groundwater contamination related to chemical discharge from Paradise Cleaners. If a potential hazardous contamination is detected, soil samples should be gathered and tested to determine the chemical levels within the soil.	During Construction	City of Citrus Heights and Contractor		
HM-7: To avoid impacts from pavement striping during construction it is recommended that removal requirements for yellow striping and pavement marking materials be performed in accordance with Caltrans Standard Special Provision 14-11.07 REMOVE YELLOW TRAFFIC STRIPE AND PAVEMENT MARKING WITH HAZARDOUS WASTE RESIDUE.	During Construction	City of Citrus Heights and Contractor		
HM-8: As is the case for any project that proposes excavation, the potential exists for unknown hazardous contamination to be revealed during project construction (such as previously undetected petroleum hydrocarbon contamination from nearby gas stations). Should any previously unknown hazardous waste/material be encountered during construction, the procedures outlined in Caltrans Hazards Procedures for Construction shall be followed.	During Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
HM-9: If the project area changes (due to a change in the project design or staging area), further investigation for potential hazardous waste generators would be required to determine their impact to the revised project limits.	Prior to and During Construction	City of Citrus Heights and Contractor		
HYDROLOGY AND WATER QUALITY H-1: Incorporate Development Standards for Improving Water Quality: The City shall incorporate water quality protection measures into the specific plan Development Standards: The standards may include but are not limited to the following: 1. Install and maintain landscaping that requires minimal application of chemical fertilizers, pesticides and herbicides; 2. Emphasize xeriscape landscaping that reduces the need for irrigation by minimizing the use of turf in decorative landscaping, using plant materials adapted to local conditions and efficient irrigation; 3. Minimize irrigation overspray - do not permit use of sprinkler and spray irrigation in areas less than 8 feet wide; 4. Use of drip irrigation systems where feasible; 5. Incorporate features such as filtration strips or bioswales in site design to prevent urban pollutants from entering into Cripple Creek via storm drains from parking lots and paved surfaces.	During Construction	City of Citrus Heights and Contractor		
H-2: Implement Best Management Practices (BMPs). The City shall require implementation of best management practices for public and private development. Such practices may include, but are not limited to: 1. Regular inspection, maintenance and cleaning out of stormwater retention or detention structures; 2. Regular inspection, maintenance and cleaning out of oil and water separators; 3. Encourage property owners to regularly remove trash, dead vegetation and leaf litter; 4. Encourage use of landscaping and horticultural practices that minimize the need for	During Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
chemical fertilizers, herbicides and pesticides.				
H-3: The Project would require a NPDES General Construction Permit for Discharges of storm water associated with construction activities (Construction General Permit 2012-0006-DWQ). A SWPPP would also be developed and implemented as part of the Construction General Permit.	Prior to Construction	City of Citrus Heights and Contractor		
H-4: The construction contractor shall adhere to the SWRCB Order No. 2012-0006-DWQ NPDES Permit pursuant to Section 402 of the CWA. This permit authorizes storm water and authorized non-storm water discharges from construction activities. As part of this Permit requirement, a SWPPP shall be prepared prior to construction consistent with the requirements of the RWQCB. This SWPPP will incorporate all applicable BMPs to ensure that adequate measures are taken during construction to minimize impacts to water quality.	Prior to Construction	City of Citrus Heights and Contractor		
NOISE N-1: Pursuant to City of Citrus Heights General Plan EIR Mitigation Measure 4.6-1: 1. Limit hours of construction to account for more sensitive weekend hours. 2. Limit hours of construction where noise is audible at sensitive land uses beyond the boundaries of the construction site.	During Construction	City of Citrus Heights and Contractor		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE																
			Initials	Date															
<p>N-2: Pursuant to City of Roseville General Plan, Chapter 9, Mitigation Measure 8:</p> <p>The City shall use the Noise Level Performance Standards contained in Table IX-3 for reviewing new development of noise-sensitive uses exposed to fixed noise sources. These standards are also to be used for evaluating potential impacts of proposed new fixed noise sources upon nearby noise-sensitive uses. Where a noise-sensitive land use is proposed near a fixed noise source, such as an industrial facility, noise measurements will be performed to determine whether existing and/or future noise levels due to that source will exceed the standards of Table IX-3 within the property line of the proposed use. Similarly, where a fixed noise-producing use such as an industrial facility is proposed near an existing or future noise-sensitive use, a noise analysis will be prepared to ensure that the noise produced by that use will not exceed the standards of Table IX-3 within the property line of the noise-sensitive use. (Policies 6, 7 and 8).</p> <table><tr><th colspan="3">TABLE IX-3</th></tr><tr><th colspan="3">PERFORMANCE STANDARDS FOR NON-TRANSPORTATION NOISE SOURCES OR PROJECTS AFFECTED BY NON-TRANSPORTATION NOISE SOURCES (As Measured at the Property Line of Noise-Sensitive Uses)</th></tr><tr><th>Noise Level Descriptor</th><th>Daytime (7 a.m. to 10 p.m.)</th><th>Nighttime (10 p.m. to 7 a.m.)</th></tr><tr><td>Hourly L_{eq}, dB</td><td>50</td><td>45</td></tr><tr><td>Maximum level, dB</td><td>70</td><td>65</td></tr></table> <p>¹ For municipal power plants consisting primarily of broadband, steady state noise sources, the hourly (Leq) noise standard may be increased up to 10 dB(A), but not exceed 55 dB(A) Hourly Leq dB.</p> <p>Each of the noise levels specified above should be lowered by five dB for simple tone noises, noises consisting primarily of speech or music, or for recurring impulsive noises. Such noises are generally considered by residents to be particularly annoying and are a primary source of noise complaints. These noise level standards do not apply to residential units established in conjunction with industrial or commercial uses (e.g., caretaker dwellings).</p> <p>No standards have been included for interior noise levels. Standard construction practices should, with exterior noise levels identified, result in acceptable interior noise levels.</p>	TABLE IX-3			PERFORMANCE STANDARDS FOR NON-TRANSPORTATION NOISE SOURCES OR PROJECTS AFFECTED BY NON-TRANSPORTATION NOISE SOURCES (As Measured at the Property Line of Noise-Sensitive Uses)			Noise Level Descriptor	Daytime (7 a.m. to 10 p.m.)	Nighttime (10 p.m. to 7 a.m.)	Hourly L_{eq} , dB	50	45	Maximum level, dB	70	65	During Construction	City of Citrus Heights and Contractor		
TABLE IX-3																			
PERFORMANCE STANDARDS FOR NON-TRANSPORTATION NOISE SOURCES OR PROJECTS AFFECTED BY NON-TRANSPORTATION NOISE SOURCES (As Measured at the Property Line of Noise-Sensitive Uses)																			
Noise Level Descriptor	Daytime (7 a.m. to 10 p.m.)	Nighttime (10 p.m. to 7 a.m.)																	
Hourly L_{eq} , dB	50	45																	
Maximum level, dB	70	65																	

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
N-3: On-site Noise Control: To ensure mitigation of noise due to project-related loading docks and on-site traffic, development proposals should be reviewed to identify potential noise conflicts with existing or proposed noise sensitive uses. Implementation of the noise standards contained in the Noise Element of the Citrus Heights General Plan will mitigate project-related noise to an insignificant level. For development requiring installation of large groundmounted HVAC systems, development review should include an assessment of noise impacts on nearby residential areas.	During Construction	City of Citrus Heights and Contractor		
N-4: The Contractor shall follow City of Citrus Heights and City of Roseville noise ordinances for construction activities: <ul style="list-style-type: none"> • Do not exceed 65 dBA at 50 feet from the job site activities from 8 p.m. to 7 a.m. • Use an alternative waiting method instead of a sound signal unless required by safety laws. • Equip an internal combustion engine with the manufacturer-recommended muffler. • Do not operate an internal combustion engine on the job site without the appropriate muffler. 	During Construction	City of Citrus Heights and Contractor		
POPULATION AND HOUSING	Prior to Construction	City of Citrus Heights		
PH-1: Prior to approving a development project that would result in conversion of trailer parks to other uses; the City shall comply with Government Code Section 65863.7, a copy of which is included in Appendix J of the ABSP EIR.				
PH 1-B: Relocation Assistance for Housing Displacement: <ol style="list-style-type: none"> 1. The City shall provide standard relocation assistance to both tenants and owner occupants in compliance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Replacement housing must be decent, safe, and sanitary (DS&S), which means it must meet all of the minimum requirements established by Federal regulations and conforms to applicable housing and occupancy codes. 2. All real property transactions shall comply with the property acquisition and 	Prior to Construction	City of Citrus Heights		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
relocation standards of the State of California, the Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.				
<p>PH-1C: Business Relocation. The following mitigation measures shall be required to compensate for right-of way acquisition:</p> <ol style="list-style-type: none"> 1. Property owners shall be compensated in accordance with fair market values based on appraisals. Business owners shall be compensated based on an assessment of the value of the business and any loss of good will. 2. All efforts shall be made to identify relocation opportunities for affected businesses that would reduce the loss of goodwill and historic patronage. Wherever feasible, assistance shall be made available in identifying suitable relocation sites within the service area of existing businesses. 	Prior to Construction	City of Citrus Heights		
<p>PH-1D Property Compensation:</p> <ol style="list-style-type: none"> 1. All real property transactions shall comply with the property acquisition and relocation standards of the State of California, the Caltrans Relocation Assistance Program and the Federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. 2. Property owners shall be compensated in accordance with fair market values based on appraisals. Business owners shall be compensated based on an assessment of the value of the business and any loss of good will. 3. All efforts shall be made to identify relocation opportunities for affected businesses that would reduce the loss of goodwill and historic patronage. Wherever feasible, assistance shall be made available in identifying suitable relocation sites within the service area of existing businesses. 	Prior to Construction	City of Citrus Heights		
<p>TRANSPORTATION AND TRAFFIC</p> <p>T-1: Ensure Adequate Parking Supply. In order to ensure that adequate parking supply is maintained in the specific plan area, the city shall establish a special permit process to</p>	Prior to Construction	City of Citrus Heights		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
allow flexibility in the number of required parking spaces when deemed appropriate.				
<p>T-1B: Compensate for Parking Impacts:</p> <p>1.The determination of project-caused parking impacts shall be made in accordance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Project proponents shall compensate for acquisition of underlying property in compliance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.</p> <p>2. In the event that it is determined that the project will result in a reduction of parking spaces below the number required by zoning, but would not preclude continued use of the parcel that is allowed by zoning as determined by the appropriate land use authority (City of Citrus Heights), the project proponent(s) shall:</p> <p style="padding-left: 40px;">(a) Investigate feasibility of and compensate for cost of reconfiguring parking area or relocating parking on the same parcel to provide additional spaces; or</p> <p style="padding-left: 40px;">(b) Investigate feasibility of and compensate for the cost of providing off-site parking; and/or</p> <p style="padding-left: 40px;">(c) The appropriate land use authority (City of Citrus Heights) shall grant a special parking permit to allow the continued use with reduced parking.</p> <p>3. If it is determined during the right-of-way appraisal and acquisition process that the project would result in a reduction of parking spaces that would preclude continued use of the parcel in accordance with the existing zoning, and none of the measures under No. 2 above are feasible, the project proponent(s) shall provide compensation in accordance with Caltrans Relocation Assistance Program and the federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.</p>	Prior to Construction	City of Citrus Heights		
<p>T-2: Construction Period Traffic Management Plan. A traffic handling plan will be prepared prior to construction of any roadway improvements. The plan will address traffic management during construction periods, including but not limited to road and lane closures; detours; pedestrian and bicycle routes; and public notification. The traffic handling plan should be prepared in consultation with regional transit in order to</p>	Prior to Construction	City of Citrus Heights		

Mitigation Measure	Reporting Milestone	Reporting / Responsible Party	VERIFICATION OF COMPLIANCE	
			Initials	Date
minimize disruptions to public transit service along the corridor. Additionally, prior to commencement of construction, a Traffic Management Plan as described in A Traffic Management Plan Guide (See Appendix A of the Final EIR) will be prepared and provided to Caltrans for review in order to address strategies needed to minimize disruption of traffic at the Interstate 80/Auburn Boulevard Interchange.				
UTILITIES AND SERVICE SYSTEMS U-1: Construction Management for Utilities: The construction project management team shall coordinate with utility providers during design stages of roadway projects. The construction project management team shall undertake periodic assessments of upcoming utility and service disruptions during construction. These assessments and an identification of the service area involved shall be coordinated with utility providers and the public outreach program. The public outreach program shall ensure that advance notice of any utility or service shutdowns is extended to affected businesses and residents. Through construction management and project scheduling, all available measures shall be taken to minimize the duration of utility or service shutdowns.	Prior to Construction	City of Citrus Heights		

APPENDIX F: Draft Initial Study/Mitigated Negative Declaration Comment Letters and Responses

November 9, 2015

City of Citrus Heights Planning Division
Mr. Casey Kempenaar
7927 Auburn Boulevard
Citrus Heights, CA 95610

Via: *Email and Regular Mail*

ckempenaar@citrusheights.net
Page 1 of 2

Subject: Initial Study Proposed Mitigated Negative Declaration Auburn Boulevard Complete Streets Project, Phase 2 – City of Roseville Comments

Dear Mr. Kempenaar:

Thank you for the opportunity to provide comment on the subject project Initial Study/Proposed Mitigated Negative Declaration (IS/MND). The project is unique in that the north end includes planned improvements within the City of Roseville. The City of Roseville appreciates the early coordination that has occurred to date and looks forward to continuing these efforts through the project's implementation phase as outlined below.

Improvement Costs

As discussed during early coordination, the City of Roseville is not prepared to cost share in the proposed improvements. Also, the cost of any City of Roseville utility relocations required to accommodate improvements will need to be funded by the project.

Project Outreach

The commercial businesses located at the north end of the project (including the Shell and Chevron gas stations and Jack in the Box restaurant) are located in the City of Roseville. If turn restrictions to these existing Roseville business are planned, the city of Roseville will need to understand the justification (such as a high accident rate and/or the need due to an increase in volume of u-turns). In this regard, Roseville staff would also like to work with the City of Citrus Heights and project's engineers to consult with these business and address any related concerns if project improvements are proposed this area.

Fiber and Signal Synchronization

The City of Roseville supports installation of fiber as part of overall project improvements to facilitate the future potential synchronization of City of Roseville and Citrus Heights traffic signals.

Whyte Avenue/Auburn Boulevard

Based on review of IS/MND Figure 3, it appears the project would restrict left turns from Whyte Avenue. Prior to any improvements at this intersection the City of Roseville recommends consideration of the following:

- Conduct outreach to all effected residents and businesses both east and west of the project location (prepare a map of outreach limits);
- Please review left turn restrictions with our Engineering Division of Public Works for design;
- The Auburn Boulevard median should be extended north to Orlando and should be a stamped concrete material; or, a gull-wing design might be used to limit left turns from Whyte Avenue. Please review preliminary designs with our Engineering Division of Public Works; and,
- Please enhance the crosswalk(s) for pedestrian safety at the intersection of Auburn/Riverside and Orlando Dr.

The City of Roseville previously studied this improvement as part of the Louis/Orlando Transfer station project and we would be happy to share our study results.

Louis/Orlando Transfer Station

The City recommends the project link connections of bicycles and pedestrians to the approved Louis-Orlando Transfer Point project (LOTP). It appears the bike lane stops at Whyte Avenue and we could find no indication of how cyclists or pedestrians would travel to and from the LOTP to other uses along Auburn using the existing and planned facilities.

Also the IS/MND transportation analysis doesn't address short term construction impacts to transit operations. Are any transit related travel delays anticipated as a result of project construction? Please contact Mike Wixon, City of Roseville Alternative Transportation Manager for additional information regarding the Whyte Avenue/Auburn Boulevard intersection improvements and/or LOTP connectivity issues ((916) 774 5480).

Utilities

Roseville Electric has two existing streetlights located on the west side of Auburn Boulevard south of Whyte Avenue with underground conduit and conductor feeding the lights (see attached). Per IS/MND Figure 3, these lights may need to be moved with the project. There may also be other water and/or sewer utility conflicts to consider at the time of final design.

Thank you for consideration of our comments.

Sincerely,



Mark Morse
Environmental Coordinator

Attachment 1: Roseville Electric Infrastructure

~WHYTE AVE.~

EXIST. 250W MERC. VAPOR ST. LT.
TO BE REMOVED BY CITY DUE
TO G.O. 95 INFRACTION WITH
SMUD PRIMARY OVERHEAD.
DEV. TO REMOVE BASE.

8655 AUBURN BLVD.
JACK-IN-THE-BOX REST.

DEV. TO REPAIR DAMAGED ST. LT. CONDUIT
AND REPLACE DAMAGED WIRE W/
2-#12 CU. THHN (1-WHITE, 1-BLACK)

EXIST. 250W MERC. VAPOR ST. LT.
TO REMAIN.

RELOCATE EXIST. ST. LT. TO THIS LOCATION.
CONSTRUCT CONCRETE BASE
PER SPECS FOR COMMERCIAL CONST.

EXIST. 250W MERC. VAPOR ST. LT
TO BE RELOCATED

EXIST. CONDUIT AND CONDUCTOR
FROM SMUD TRANSFORMER TO
SERVICE BOX

NEW BUS TURNOUT

~AUBURN BLVD.~

NEW BUS STOP



**ELECTRIC
DEPARTMENT**
(916) 783-9151
EXT. 237

ENG. <i>BONOMI</i>	SCALE <i>1" = 20'</i>
DR. <i>BONOMI</i>	DATE <i>8/15/96</i>
APP.	DR. NO. <i>120-675</i>
TITLE <i>JACK-IN-THE-BOX REST. 8655 AUBURN BLVD.</i>	

CONSTRUCTION		JOB NO. <i>960153</i>
FINISH	START	
	ACCOUNT NO. <i>41001-6130</i>	



CITY OF CITRUS HEIGHTS

CITY COUNCIL STAFF REPORT MEMORANDUM

DATE: April 22, 2021

TO: Mayor and City Council Members
Christopher W. Boyd, City Manager

FROM: Bill Zenoni, Administrative Services Director
Tammy Nossardi, Finance Manager

SUBJECT: **Approval of Budget for Fiscal Years 2021-22 and 2022-23 and
Authorization for Execution of an Agreement with Greater
Sacramento Economic Council**

Summary and Recommendation

Staff recommends the City Council adopt Resolution approving the City of Citrus Heights Two Year Budget for Fiscal Years 2021-22 and 2022-23.

Staff recommends the City Council adopt Resolution authorizing the City Manager to execute an agreement between the Greater Sacramento Economic Council and the City of Citrus Heights in an amount not to exceed \$35,238 for Fiscal Year 2021-22.

Fiscal Impact

Two Year Budget

The Proposed Budget, for Fiscal Years 2021-22 and 2022-23, as presented to the City Council on April 8, 2021, includes appropriations of \$72,719,281 in Fiscal Year 2021-22 and \$67,923,707 in Fiscal Year 2022-23 for all City funds.

The General Fund budget for Fiscal Year 2021-22 projects revenue of \$38,956,010 and expenditures of \$39,033,315. The General Fund budget for Fiscal Year 2022-23 anticipates revenue of \$40,379,549 available to fund expenditures of \$36,694,635.

Background and Analysis

Fiscal Years 2021-22 and 2022-23 Budget

Development of this two-year budget has been a challenging process, given the fiscal uncertainties caused by the ongoing COVID pandemic and limited financial resources available to fund the

City's operating and capital costs. The budget presented for City Council consideration was developed over the past several months with input from staff in all City departments. While budget reductions have been required, this proposed two-year budget reflects the City's long history of fiscal prudence.

The Proposed Two-Year Budget for Fiscal Years 2021-22 and 2022-23 was reviewed by the Finance and Administration Committee on March 25, 2021. The City Council conducted a Study Session on April 8, 2021 to review and discuss the Proposed Budget.

Agreement with Greater Sacramento Economic Council

The Greater Sacramento Economic Council (GSEC) is a regional economic development organization working through a public-private partnership of local businesses and governments to retain, attract, grow and create sustainable business in the six-county Sacramento Region. GSEC is funded by 21 cities and counties and over 100 private investors. The City of Citrus Heights and the Greater Sacramento Economic Council have benefited from a productive partnership since 2015. Among the many benefits this partnership provides the City are the following:

- A voting seat on the GSEC Board of Directors.
- Access to valuable economic and real estate data.
- Opportunity to provide input on GSEC's marketing strategy which assists the City with its own marketing campaign.
- Collaboration with a team of regional economic developers, data analysts and marketing professionals to market and support the Greater Sacramento region.

The annual contribution for Fiscal Year 2021-22 is \$35,238, unchanged from the Fiscal Year 2020-21 cost. Funding for this membership is included in the Economic Development budget.

Attachments

1. City of Citrus Heights Proposed Budget for Fiscal Years 2021-22 and 2022-23
2. Resolution Approving the Budget for Fiscal Years 2021-22 and 2022-23
3. Resolution authorizing the City Manager to execute an agreement between the Greater Sacramento Economic Council and the City of Citrus Heights in an amount not to exceed \$35,238 for Fiscal Year 2021-22
4. Greater Sacramento Economic Council Annual Commitment Letter

RESOLUTION NO. 2021 - ____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CITRUS HEIGHTS
APPROVING THE BUDGET FOR FISCAL YEARS 2021-22 AND 2022-23**

WHEREAS, the proposed budget for the City of Citrus Heights for Fiscal Years 2021-22 and 2022-23 has been presented to the City Council and reviewed at a public meeting on April 8, 2021;

WHEREAS, in approving the budget for Fiscal Years 2021-22 and 2022-23, the City is pursuing budget strategies to maintain financial stability and service levels.

NOW THEREFORE BE IT RESOLVED AND ORDERED by the City Council of the City of Citrus Heights that the budget for Fiscal Years 2021-2022 and 2022-23 as presented at scheduled City Council meetings on April 8, 2021 and April 22, 2021 is approved. The total appropriated expenses (all funds) are \$72,719,281 for Fiscal Year 2021-22 and \$67,923,707 for Fiscal Year 2022-23.

The City Clerk shall certify the passage and adoption of this Resolution and enter it into the book of original resolutions.

PASSED AND ADOPTED by the City Council of the City of Citrus Heights, California this 22nd day of April 2021 by the following vote, to wit:

AYES: **Council Members:**
NOES: **Council Members:**
ABSTAIN: **Council Members:**
ABSENT: **Council Members:**

Steve Miller, Mayor

ATTEST:

Amy Van, City Clerk

RESOLUTION NO. 2021- ____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CITRUS HEIGHTS,
AUTHORIZING THE CITY MANAGER TO EXECUTE AN AGREEMENT BETWEEN THE
GREATER SACRAMENTO ECONOMIC COUNCIL AND THE CITY OF CITRUS
HEIGHTS IN AN AMOUNT NOT TO EXCEED \$35,238 FOR FY 2021-22**

WHEREAS, Greater Sacramento Economic Council is a regional economic development organization working through a public-private partnership of local businesses and governments to retain, attract, grow and create sustainable business and industry clusters in the six-county Sacramento Region;

WHEREAS, the City of Citrus Heights has been an active member of the Greater Sacramento Economic Council since 2015;

WHEREAS, the City of Citrus Heights desires to renew its membership with the Greater Sacramento Economic Council;

WHEREAS, as a member, the City of Citrus Heights shall have a seat on the Board of Directors of the Greater Sacramento Economic Council; and will continue its participation on the Economic Development Director's Taskforce.

NOW, THEREFORE, BE IT RESOLVED AND ORDERED by the City Council of the City of Citrus Heights that the City Manager is directed to execute an agreement between the Greater Sacramento Economic Council and the City of Citrus Heights in an amount not to exceed \$35,238 for Fiscal Year 2021-22, subject to the terms agreed upon by the City Manager and City Attorney.

The City Clerk shall certify the passage and adoption of this Resolution and enter it into the book of original resolutions.

PASSED AND ADOPTED by the City Council of the City of Citrus Heights, California, this 22nd day of April 2021 by the following vote, to wit:

AYES: Council Members:
NOES: Council Members:
ABSTAIN: Council Members:
ABSENT: Council Members:

Steve Miller, Mayor

ATTEST:

Amy Van, City Clerk



CITY OF CITRUS HEIGHTS

PROPOSED BUDGET

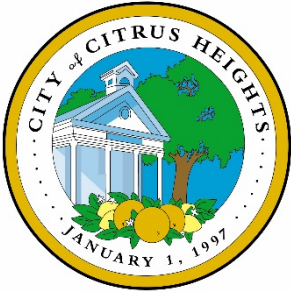
FISCAL YEAR 2021-22 AND 2022-23



**CITRUS
HEIGHTS**

Solid roots. New growth. Agenda Packet Page 319





CITY OF CITRUS HEIGHTS

Fiscal Years 2021-22 and 2022-23 Proposed Budget

City Council

Steve Miller, Mayor
Porsche Middleton, Vice Mayor
Jeannie Bruins, Council Member
Bret Daniels, Council Member
Tim Schaefer, Council Member

City Manager

Christopher W. Boyd

Department Directors

Ron Lawrence, Police Chief
William Zenoni, Interim Administrative Services Director
Colleen McDuffee, Community Development Director

City of Citrus Heights

6360 Fountain Square Drive
Citrus Heights, CA 95621
(916) 725-2448 (916) 725-5799 (fax)
www.citrusheights.net

MISSION STATEMENT

The City of Citrus Heights is committed to providing high quality, economical, responsive city services to our community.



VISION STATEMENT

Citrus Heights will be the city of choice for residents and businesses to prosper and thrive and a model of neighborhood engagement.

Table of Contents

MESSAGE FROM THE CITY MANAGER.....	1
COMMUNITY PROFILE	3
FINANCIAL SUMMARIES	5
AUTHORIZED POSITIONS	20
GENERAL FUND	
CITY COUNCIL	25
CITY ATTORNEY	27
CITY MANAGER.....	28
ADMINISTRATIVE SERVICES DEPARTMENT	39
POLICE DEPARTMENT	44
COMMUNITY DEVELOPMENT DEPARTMENT	49
GENERAL SERVICES DEPARTMENT	54
ENTERPRISE FUNDS	64
SPECIAL REVENUE FUNDS	65
APPENDIX	
BENEFIT OVERVIEW	68
GOALS FOR FY 2021-22 AND 2022-23	72
GLOSSARY OF BUDGET TERMINOLOGY	77
FIVE YEAR CAPITAL IMPROVEMENT PROGRAM	80



A stylized blue tree graphic with a thick trunk and a dense, rounded canopy of leaves. The tree is centered horizontally and spans most of the page width. A horizontal grey band cuts across the middle of the tree, containing text.

CITY OF
CITRUS
HEIGHTS

MESSAGE FROM THE CITY MANAGER

Message from the City Manager

April 8, 2021

Honorable Mayor and Members of City Council:

I am pleased to present for your consideration the City of Citrus Heights Proposed Budget for Fiscal Years 2021-22 and 2022-23. Development of this budget has been a challenging process, given the fiscal uncertainties caused by the ongoing COVID pandemic and limited financial resources available to fund the City's operating and capital costs.

Throughout the past year, we have seen our community band together to support each other and emergency response workers; City staff worked hard to innovate and keep all City service lines operational remotely; the City partnered with local restaurants to provide thousands of free meals to our at-risk seniors; and we have learned to connect in new ways, while staying socially distant. Despite all the challenges of the pandemic, the City of Citrus Heights has continued to honor our long history of fiscal prudence and this updated Proposed Budget is a testament to those efforts.

As we look ahead and balance our budget, we are up against several challenges. We have known since our City incorporation that there would come a time when our expenses would be higher than our revenue, largely because of our revenue-neutrality agreement with the County. For more than two decades, we have had to balance our budget without property tax revenue, which has had compounding consequences. On top of that, our City has felt the significant impacts of the economic fallout from the pandemic, while revenue has dropped from an ongoing decline in retail sales tax. These factors have led to our current budget reality and, like many cities in our region, we have had to make cuts in order to balance our budget.

While we are optimistic about being included in the Federal American Rescue Plan funding, our City's Proposed Budget does not rely on Federal relief, so that we can ensure our plan is viable. We know that the Federal relief funds will not be able to address all of our City's concerns, as they will come with specific spending guidelines. At this time, we are still waiting for clarification regarding the amount of funding the City will receive and the guidelines on permitted use of those funds. Once that information is available, we will return to the City Council for discussion of possible amendments to this two-year budget.

Public Safety: Without additional general fund revenue, we must look to further reduce City expenses. Because we already operate on a lean budget, the biggest area of opportunity lies in personnel costs, the majority of which are in our Police Department as is typical for cities like ours. Our Proposed Budget includes cutting 26 positions in the Police Department and holding those vacant for the next two years. We know from our City's largest engagement effort that public safety is a priority for our residents. It is important to note that, even after these cuts, the Police Department still makes up about 70 percent of our personnel expenses from the General Fund, just as it has in past years. We want to ensure our residents know that while police services are going to be streamlined, the Police Department will continue to respond to emergencies as always. Since its creation, our award-winning Police Department has achieved significant decreases in crime. Although our current budget reality requires us to reduce services, including reductions in the Police Department, staff will continue to provide the quality work our residents and businesses deserve.

Roads and Infrastructure: Third-party analysis tells us that Citrus Heights needs at least \$12 million annually to improve our roads, and there is not enough revenue to support that kind of annual expense in this

Proposed Budget. City staff aggressively apply for – and have a history of being awarded – competitive grant funds, and those efforts will continue. Most available grant funds are for projects like our “Complete Streets” efforts, which widen sidewalks for accessibility, increase safety, promote walkability, and more. As always, we are obligated to apply grant funds for very specific purposes and services. The Proposed Budget funds \$72.7 million of operating and capital costs in Fiscal Year 2021-22 and \$67.9 million in operating and capital costs in Fiscal Year 2022-23, across all City funds. The Five Year Capital Improvement Program, which will be presented to the Planning Commission on April 28th and to the City Council for adoption on May 13th, totals \$56.6 million.

Economic Development: While we plan to reduce expenses to balance our budget, we will also continue to steward along important development projects that diversify our economy and are important for our community's economic future. Our staff continue to usher in significant projects for the City, like:

- Sunrise Tomorrow, the Specific Plan and vision to revitalize the 100-acre Sunrise Mall site.
- Green Acres, an exciting example of adaptive reuse of our City's existing retail spaces.
- The Dignity Health/USPI Surgery Center at Stock Ranch and Sylvan, which brings quality jobs and services to our community.
- The New Sylvan Project, where our Purchase and Sale Agreement with Woodside Homes will bring exciting new housing options to our City.

Planning for the Future: The City of Citrus Heights has always taken charge of its own future. We became a city by intent, established our own police department, and have optimism about our future despite the challenges we are up against. We have always engaged in long-term and comprehensive financial planning to ensure that the City can keep its budget balanced. The City's main operating fund is the General Fund. General Fund revenues for Fiscal Year 2021-22 are projected to total \$38.9 million with budgeted expenditures totaling \$39.0. Fiscal Year 2021-22 represents the final year of the revenue-neutrality agreement with Sacramento County. Beginning in Fiscal Year 2022-23, the City will finally retain its allocation of property tax revenue. For Fiscal Year 2022-23, the budget anticipates revenue to the General Fund of \$40.4 million and expenditures of \$36.7 million. It should be noted, however, that this two-year budget was developed with the assumption that 27 permanent positions (including 26 positions in the Police Department) as well as 7 limited-term and part-time positions would be held vacant throughout the two-year budget period.

I want to thank all those who put a great deal of time, energy, and effort into the development of our Proposed Budget. Special recognition is due to the Administrative Services team for their coordination of this important document. I also wish to thank our City Council for their continued leadership and support.

Sincerely,



Christopher W. Boyd, City Manager





CITY OF
CITRUS
HEIGHTS

COMMUNITY PROFILE

About Us

Citrus Heights officially incorporated on January 1, 1997.

Citrus Heights is an established community with an estimated population of 87,811 as of January 1, 2020 (State of California Department of Finance).

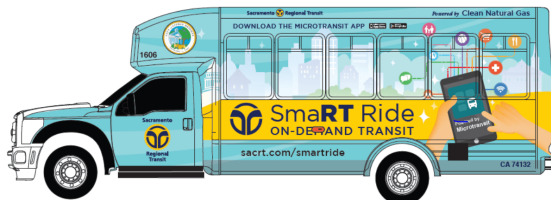
The City has a total area of 14.2 square miles and is part of the Sacramento metropolitan area.

In 2018, the City launched its new brand, “Solid roots. New growth.” It highlights the City’s strong leadership and collaborative community spirit that won us cityhood, has kept us in a strong financial position, and continues to be the foundation for our community’s growth today.



Transportation/Access

The Sacramento International Airport is located approximately 20 miles from Citrus Heights. Rail transportation (Amtrak) is accessible in Roseville (about ten miles from the City). The Sacramento Regional Transit District provides public transportation services throughout Citrus Heights, including the recently implemented microtransit project, Smart Ride. Smart Ride is an on-demand, smart phone app enabled transit service that provides rides to any destination within city limits.



Education

The City of Citrus Heights is primarily served by the San Juan Unified School District (SJUSD).

There are three K-8 schools, six elementary schools, one middle school, and two high schools located within the City of Citrus Heights. SJUSD also offers other educational schools and programs such as special education centers, adult schools, adult handicapped schools, preschools, and before- and after-school programs.

Citrus Heights is home to three post-secondary schools: National Career Education, Carrington College and Citrus Heights Beauty College.

Police Department

The City launched its own Police Department in June 2006. Between 2019 and 2020, CHPD had a decrease in violent crimes by 9% and a decrease in property crimes by 5%. As for traffic, Citrus Heights had three fatal collisions in 2020, up from zero in 2019, a 17% decrease in injury collisions, a 25% decrease in DUI collisions, and an overall decrease of 8% in total traffic collisions.

Fire Department

All fire protection services for Citrus Heights residents and businesses are provided by the Sacramento Metro Fire District.

Income

Citrus Heights is characterized as a middle-income suburban community. The median annual household income is estimated at \$86,300 (U.S. Department of Housing and Urban Development, 2020).

Housing Units

Within the Citrus Heights city limits, there are 35,158 housing units (SACOG, 2019). As of September 2020, the median sale price of homes listed in Citrus Heights is \$389,375.

Climate

Citrus Heights has a climate characterized by mild winters and warm, dry summers. Average daily high temperatures range from 54° in winter to 95° in the summer months. Average yearly precipitation is 22.7 inches, with almost no rain during the summer months, and an average rainfall of 3.7 inches in January.

Getting Away

Citrus Heights offers residents close proximity to the legendary recreation and leisure centers of Lake Tahoe, the Napa Valley Wine Country, national parks, and seashores. Additionally, Citrus Heights is located within a two-hour drive of San Francisco. The Sacramento River and Folsom Lake are two of the many scenic locales within a 15-minute drive from Citrus Heights.

Businesses & Shopping

Citrus Heights has long been known for its vast retail and service offerings. With over 2,000 businesses, including the Sunrise Mall, Citrus Heights is home to many major department stores, specialty stores, and service businesses, like Macy's, JCPenney, Kohl's, Target, Best Buy, Lowe's Home Improvement, Costco, Sam's Club, and Wal-Mart.

Parks and Recreation Programs

Citrus Heights features numerous parks and playgrounds which make it an ideal place for friends and family to spend time. Recreational activities and programs are offered through the Sunrise Recreation and Park District. In addition, the City of Citrus Heights sponsors many free public events throughout the year such as the Senior Health Fair, Sunday FunDay, and Hot August Bites.



Demographics

In-depth census and demographic information about Citrus Heights is available from the following resources:

United States Census Bureau American Fact Finder: <https://data.census.gov/cedsci/>

Sacramento Area Council of Governments (SACOG): <http://www.sacog.org/demographics-other-data>

California Finance Department: <http://www.dof.ca.gov/forecasting/demographics/>



A stylized tree logo in teal. The canopy is a dense, rounded mass of leaves, and the trunk is thick and tapers slightly towards the base, which has several roots extending outwards. A horizontal grey band passes through the middle of the tree, behind the text.

CITY OF
CITRUS
HEIGHTS

FINANCIAL SUMMARIES

Financial Summaries

Financial Overview

This section provides a financial overview of all City funds by fund type. It presents the total available resources and total use of resources, including beginning and ending fund balances, revenues, expenditures, operating transfers in, and operating transfers out.

Summary of All City Funds

The “Summary of All City Funds” schedule on the following page groups the City’s funds into four classifications:

- General Fund
- Enterprise Fund
- Special Revenue Funds
- Capital Funds

The majority of funding for City operations is derived from the General Fund.

The budget for capital funds is reviewed by the City Council as part of the five-year Capital Improvement Program (CIP). Capital improvement projects for FY 2021-22 and FY 2022-23 are included in this budget.

Debt Obligations

The City has no long-term bonded debt obligations. During Fiscal Year 2018-19 the City executed a lease revenue credit facility financing in the amount of \$12 million which may be utilized to fund capital and operating expenditures until the City receives its share of the property tax revenue from Sacramento County in Fiscal Year 2022-23 under the terms of the 1997 Revenue Neutrality Agreement. Repayment of any outstanding principal is due by September 2024.

SUMMARY OF ALL CITY FUNDS
Fiscal Year 2021-22

	<i>General Fund</i>	<i>Enterprise Fund</i>	<i>Special Revenue Funds</i>	<i>Capital Funds</i>	<i>Total All Funds</i>
<u>Revenues</u>					
Property Tax	6,290,000	-	-	-	6,290,000
Sales Tax	13,250,000	-	-	-	13,250,000
Utility Users Tax	2,780,000	-	-	-	2,780,000
Other Tax/Assessment Revenue	373,000	511,428	-	-	884,428
Franchise Fees	1,215,000	-	-	-	1,215,000
Business License	277,800	-	-	-	277,800
Licenses and Permits	1,492,900	-	4,065,722	-	5,558,622
Motor Vehicle In Lieu	9,343,570	-	-	-	9,343,570
Gas and Road Tax	-	-	6,261,077	-	6,261,077
Use of Money	50,000	180,250	15,333	-	245,583
Fines and Forfeitures	978,500	-	-	-	978,500
Charges for Service	951,226	137,332	4,000	18,540	1,111,098
Other Revenues	1,954,015	-	3,098,342	6,034,761	11,087,118
Transfers In	-	-	178,270	8,497,531	8,675,801
Total Revenues	\$ 38,956,011	\$ 829,010	\$ 13,622,744	\$ 14,550,832	\$ 67,958,597
<u>Expenditures</u>					
Legislative	135,436	-	-	-	135,436
Management and Support	8,643,535	222,179	1,218,126	-	10,083,840
Police	19,480,358	-	1,202,149	-	20,682,507
General Services	2,024,768	788,714	11,375,309	15,404,963	29,593,754
Community Development	1,682,218	-	2,206,048	-	3,888,266
Revenue Neutrality	6,290,000	-	-	-	6,290,000
Transfers Out	777,000	-	1,268,478	-	2,045,478
Total Expenditures	\$ 39,033,315	\$ 1,010,893	\$ 17,270,110	\$ 15,404,963	\$ 72,719,281

SUMMARY OF ALL CITY FUNDS
Fiscal Year 2022-23

	<i>General Fund</i>	<i>Enterprise Fund</i>	<i>Special Revenue Funds</i>	<i>Capital Funds</i>	<i>Total All Funds</i>
<u>Revenues</u>					
Property Tax	6,445,000	-	-	-	6,445,000
Sales Tax	13,800,000	-	-	-	13,800,000
Utility Users Tax	2,850,000	-	-	-	2,850,000
Other Tax/Assessment Revenue	388,000	526,771	-	-	914,771
Franchise Fees	1,245,000	-	-	-	1,245,000
Business License	300,950	-	-	-	300,950
Licenses and Permits	1,532,950	-	4,197,800	-	5,730,750
Motor Vehicle In Lieu	9,623,877	-	-	-	9,623,877
Gas and Road Tax	-	-	6,447,082	-	6,447,082
Use of Money	50,000	185,658	15,756	-	251,414
Fines and Forfeitures	1,002,602	-	-	-	1,002,602
Charges for Service	1,132,459	141,452	4,000	19,096	1,297,007
Other Revenues	2,008,711	-	3,009,581	10,414,625	15,432,917
Transfers In	-	-	181,059	5,361,195	5,542,254
Total Revenues	\$ 40,379,549	\$ 853,881	\$ 13,855,278	\$ 15,794,916	\$ 70,883,624
<u>Expenditures</u>					
Legislative	144,118	-	-	-	144,118
Management and Support	11,065,107	228,845	1,254,668	-	12,548,620
Police	20,125,266	-	1,112,195	-	21,237,461
General Services	2,078,005	819,139	9,854,410	15,443,086	28,194,640
Community Development	1,735,139	-	1,948,729	-	3,683,868
Transfers Out	1,547,000	-	568,000	-	2,115,000
Total Expenditures	\$ 36,694,635	\$ 1,047,984	\$ 14,738,002	\$ 15,443,086	\$ 67,923,707

SUMMARY OF ALL CITY FUNDS

FUND BALANCE PROJECTION

Fund	FY 2021-22			FY 2022-23		
	Budgeted Revenue	Budgeted Expenditures	Projected Ending Fund Balance	Budgeted Revenue	Budgeted Expenditures	Projected Ending Fund Balance
100 - General Fund *	\$ 38,956,010	\$ 39,033,315	\$ 5,606,565	\$ 40,379,549	\$ 36,694,635	\$ 9,291,479
201 - Community Events	-	-	-	-	-	-
203 - Code Enforcement	789,590	807,281	96,184	791,377	829,643	57,918
205 - Gas Tax (HUTA)	2,278,399	2,184,565	1,197,690	2,312,574	2,041,445	1,468,819
206 - SB1 RMRA	1,721,790	2,600,000	2,011,186	1,773,444	2,100,000	1,684,630
209 - Stormwater	3,811,000	5,551,230	2,682,936	3,850,000	4,665,709	1,867,227
210 - Measure A Road Maintenance	2,260,888	1,749,578	2,391,688	2,361,064	1,808,681	2,944,071
211 - Transit	278,730	283,192	(23,687)	287,092	303,673	(40,268)
212 - Transportation Development Act	74,160	83,000	149,064	76,385	16,195	209,254
221 - Police Special Revenue	83,471	125,690	589,688	84,295	128,822	545,161
222 - Police Grants	334,072	258,261	77,650	243,642	230,912	90,380
223 - SLES Fund	133,000	172,700	91,814	133,000	89,500	135,314
224 - Police Donations	-	1,500	4,156	-	1,500	2,656
231 - 03 CalHome 028	-	-	56,540	-	-	56,540
232 - 06 CalHome 0249	103	76,355	352,572	106	78,621	274,057
233 - 14 CalHome 9841	-	-	78,552	-	-	78,552
234 - CDBG	900,000	1,703,977	(803,977)	900,000	1,348,266	(1,252,243)
235 - CDBG PI & RL	-	160,000	21,026	-	-	21,026
236 - 99 EDI Grant	-	-	28,571	-	-	28,571
237 - 98 Home Grant	-	-	292,675	-	-	292,675
251 - Other Grants	46,050	-	44,061	37,132	-	81,193
310 - Measure A Construction - Traffic	226,089	548,776	1,223,549	236,106	320,500	1,139,155
311 - Measure A Construction - Ped Safety	323,432	583,416	709,554	340,128	334,000	715,682
330 - Capital Projects Fund	-	-	3,727,933	-	-	3,727,933
331 - Replacement Fund	408,540	680,000	170,358	1,129,096	699,000	600,454
390 - Streets	13,592,771	13,592,771	(39,153)	14,089,586	14,089,586	(39,153)
510 - Solid Waste (Unrestricted)	829,010	1,010,893	522,197	853,881	1,047,984	328,094
827 - Housing Agency Special Revenue Fund	-	-	31,509	-	-	31,509
<u>Development Fees</u>						
262 - Roadway	103,345	450,000	434,417	146,820	150,000	431,237
263 - Affordable Housing	16,480	51,500	331,012	16,974	53,045	294,941
264 - Tree Mitigation	36,050	36,050	108,145	37,132	37,132	108,145
265 - Park Facilities	76,000	51,500	75,159	114,000	53,045	136,114
266 - Transit Impact	28,377	75,000	115,173	38,570	25,000	128,743
Total Development Impact Fees	\$ 260,252	\$ 664,050	\$ 1,063,906	\$ 353,496	\$ 318,222	\$ 1,099,180
<u>Assessment Districts</u>						
280 -AD Zone 1	10,638	16,492	(8,106)	10,641	16,642	(14,107)
281 - AD Zone 2	12,913	13,774	78,391	12,928	14,415	76,904
282 - AD Zone 4	10,677	19,141	(13,858)	10,855	19,589	(22,592)
283 - AD 98-01	11,590	11,601	142,720	11,619	14,097	140,242
284 - AD 98-02	22,725	30,858	74,500	22,741	31,122	66,119
285 - AD 98-02 Zone 2	16,488	14,253	40,883	16,488	18,045	39,326
286 - AD 03-01 Zone 1	94,700	199,650	874,059	94,880	112,755	856,184
287 - AD 03-01 - Zone 2	19,693	57,478	8,997	19,704	57,973	(29,272)
288 - AD Zone 3	6,550	15,221	(33,813)	6,550	15,569	(42,832)
289 - Citrus Heights Lighting District	433,905	464,623	(28,164)	433,905	470,706	(64,965)
292 - AD 98-02 Zone 3	11,360	5,640	23,714	11,360	5,900	29,174
Total Assessment Districts	\$ 651,239	\$ 848,731	\$ 1,159,323	\$ 651,671	\$ 776,813	\$ 1,034,181
Total City	\$ 67,958,596	\$ 72,719,281		\$ 70,883,624	\$ 67,923,707	

* General Fund Fund Balance includes Committed and Unassigned

Enterprise Fund

Enterprise funds account for activities which operate as separate public enterprises. Enterprise funds receive revenues from fees charged to programs or other department users, and the programs are largely cost-covering. The City has one enterprise fund – the Solid Waste Fund.

Solid Waste Fund: This fund accounts for the revenues and expenses associated with garbage collection in the City. The City contracts solid waste services with Republic Services.

Special Revenue Funds

Special Revenue Funds are used to account for the proceeds of specific revenue sources (other than expendable trusts and major capital projects) that are restricted to expenditures for specified purposes.

Transit Fund: This fund accounts for the revenues and expenses associated with the administration and maintenance of city-wide transit services in conjunction with the Sacramento Regional Transportation Authority (RT).

Stormwater Drainage Fund: This fund accounts for the revenues and expenses associated with the administration and coordination of stormwater drainage activities.

Street Maintenance: These funds account for Transportation Development Act (TDA), Gas Tax, SB1 (Road Repair and Accountability Act) and Measure A Street Maintenance funds. It represents the revenues and expenses associated with the maintenance and improvement of the City's roads and street landscaping.

Code Enforcement and Sacramento Abandoned Vehicle Program: These funds account for the revenues and expenses associated with code enforcement and abandoned vehicle activities and the City's Rental Housing Inspection Program.

Grants and Housing (Community Development Block Grant, Housing Grants, and other miscellaneous grants): These funds account for the revenues and expenses associated with specialized grant monies received by the City which may only be spent under specific guidelines.

Police Grants: These funds account for the revenues and expenses associated with police grant monies received by the City and may only be spent under specific guidelines.

Assessment Districts: These funds account for the revenues and expenses associated with the maintenance of eleven City lighting and landscape assessment districts. Lighting and landscape districts are created to finance common area landscape maintenance and lighting. Homeowners are assessed through their property tax bill.

Development Impact Fees (Transit, Roadway, Affordable Housing and Park Facilities): These funds account for the revenues and expenses associated with new development of infrastructure, affordable housing, and parks.

Capital Funds

Capital Projects Funds are used to account for the financial resources to be used for the acquisition or construction of major capital facilities and infrastructure.

Measure A Construction Fund: This fund accounts for the revenues and expenses associated with street and highway construction/improvements. Various projects are approved annually through the Sacramento Transportation Authority, a regional agency with which the City has a voting seat. Funding is provided through a special augmentation of sales tax.

Federal/State Funded Projects: These funds account for the revenues and expenses associated with street and highway construction/improvements. Funding is received through federal and state grants and special state funding.

General Capital Improvements: This fund accounts for the revenues and expenses associated with construction projects or the purchase of public facilities and projects.

Replacement Fund

This fund accounts for capital improvement and equipment replacement costs, including the City's vehicle fleet and facility, equipment and information technology costs.

REPLACEMENT FUND								
		2019-20		2020-21		2021-22		2022-23
		Actual		Budget		Proposed		Proposed
Revenues	\$	250,311	\$	18,000	\$	408,540	\$	1,129,096
Expenditures	\$	332,585	\$	-	\$	680,000	\$	699,000

General Fund Expenditures

The General Fund is used to account for money which is not required legally or by sound financial management to be accounted for in another fund. The departmental budgets in the General Fund include:

City Council: Includes Council stipends and benefits, partial salary and benefits for the City Manager's Executive Assistant, travel expenses, and other Council related business.

City Attorney: Includes all general government legal services.

City Manager: Includes expenditures for the City Manager, City Clerk, City Information, Economic Development, Community Events and Non-Departmental. Non-Departmental expenditures include the payment for revenue neutrality, special studies, projects, and augmentations to support community events and code enforcement activities.

Administrative Services Department: Includes expenditures for administration of Human Resources, Information Technology, Risk Management, and Finance.

Police Department: Includes expenditures for all local law enforcement including Patrol, Traffic Enforcement, Investigations, Support Services (which includes records management and the emergency communications center), Animal Services, and administration of Code Enforcement.

Community Development Department: Includes expenditures for Planning, Building and Safety, and Grants and Housing.

General Services Department: Includes expenditures for Engineering, Street Maintenance, Solid Waste, Stormwater Drainage, Transit, Fleet and Facility Management, the City's Community Centers, Assessment District Management, and Capital Improvement Project planning and administration.

GENERAL FUND SUMMARY

	FY 2020-21 Budget	FY 2021-22 Proposed Budget	FY 2022-23 Proposed Budget
Projected Beginning Fund Balance		\$ 5,683,870	\$ 5,606,565
<u>Revenues</u>			
Property Tax	\$ 5,977,848	\$ 6,290,000	\$ 6,445,000
Sales Tax	11,134,320	13,250,000	13,800,000
Utility Users Tax	3,211,238	2,780,000	2,850,000
Franchise Fees	1,292,000	1,215,000	1,245,000
Other Tax Revenue	391,000	373,000	388,000
Business License	311,840	277,800	300,950
Licenses and Permits	1,156,608	1,492,900	1,532,950
Motor Vehicle In Lieu	9,253,296	9,343,570	9,623,877
Use of Money	36,500	50,000	50,000
Fines & Forfeitures	950,100	978,500	1,002,602
Charges for Service	1,097,507	951,226	1,132,459
Line of Credit Draw	1,200,000	-	
Other Revenues	1,867,757	1,954,014	2,008,711
Total Revenues	\$ 37,880,014	\$ 38,956,010	\$ 40,379,549
<u>Expenditures</u>			
<u>Legislative</u>			
City Council	\$ 161,313	\$ 135,436	\$ 144,118
<u>Management & Support</u>			
City Manager	753,923	683,164	708,008
City Attorney	300,000	309,000	318,270
City Clerk	255,812	224,781	195,474
Elections	55,000	-	51,000
Economic Development	559,933	602,447	619,596
Human Resources	837,127	600,059	621,987
Information Services	1,042,127	1,009,777	1,047,371
Risk Management	1,769,319	1,642,612	1,695,162
Finance	962,780	1,026,810	1,067,922
Non-Departmental	759,725	2,025,735	2,205,820
Line of Credit Payment	323,150	332,845	2,342,830
Community Events*	-	166,305	171,667
History & Arts	20,000	20,000	20,000
Total Management and Support	\$ 7,638,896	\$ 8,643,535	\$ 11,065,107

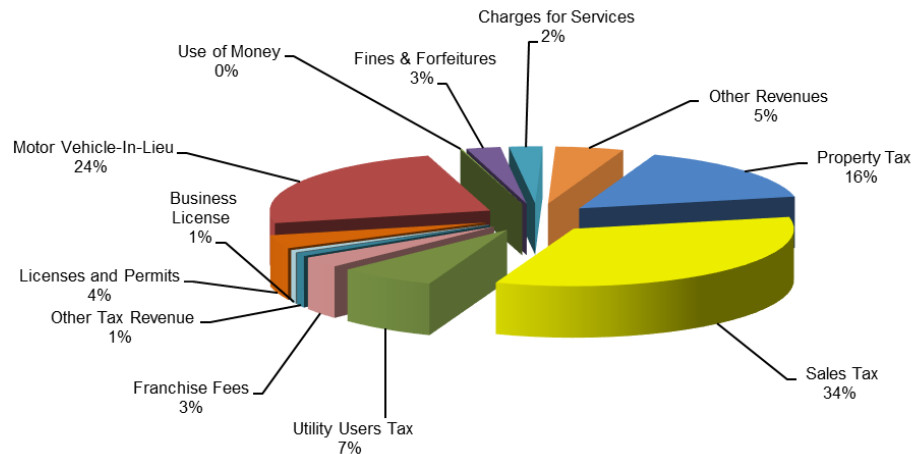
GENERAL FUND SUMMARY (continued)

	FY 2020-21 Budget	FY 2021-22 Proposed Budget	FY 2022-23 Proposed Budget
<u>Police</u>			
Police Operations	21,806,457	18,729,919	19,354,101
Animal Control	697,449	750,439	771,165
Total Police	\$ 22,503,906	\$ 19,480,358	\$ 20,125,266
<u>General Services</u>			
Engineering	607,809	476,899	488,913
Facility Management	717,794	718,988	741,149
Fleet Management	174,850	160,822	166,058
Community Center - Operations	554,083	632,667	645,427
Sylvan Community Center	16,270	18,447	19,002
Children & Youth Center	16,450	16,945	17,456
Total General Services	\$ 2,087,256	\$ 2,024,768	\$ 2,078,005
<u>Community Development</u>			
Housing	81,410	83,927	86,420
Planning	824,688	631,820	651,869
Building Inspection	868,563	949,785	979,664
Planning Commission	16,200	16,686	17,186
Total Community Development	\$ 1,790,861	\$ 1,682,218	\$ 1,735,139
Transfers Out	\$ 269,843	\$ 777,000	\$ 1,547,000
Revenue Neutrality**	5,977,848	6,290,000	-
Total Expenditures	\$ 40,429,923	\$ 39,033,315	\$ 36,694,635
Operating Surplus/(Shortfall)	(2,549,909)	(77,305)	3,684,914
Projected Ending Fund Balance		\$ 5,606,565	\$ 9,291,479

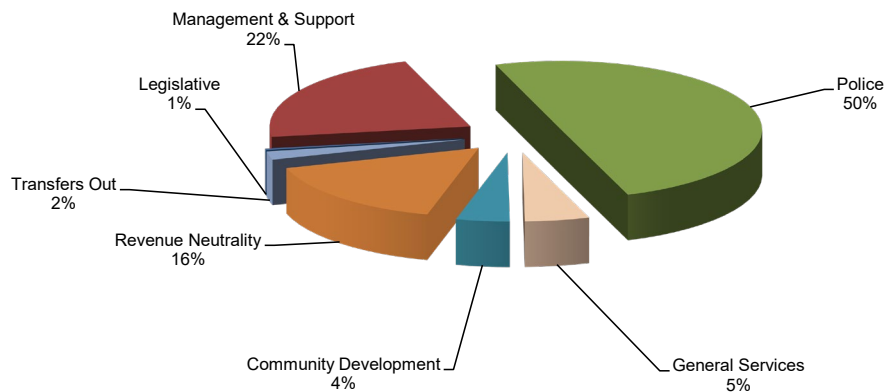
*Community Events budgeted in separate fund in FY 2020-21

** Final Revenue Neutrality payment in FY 2021-22

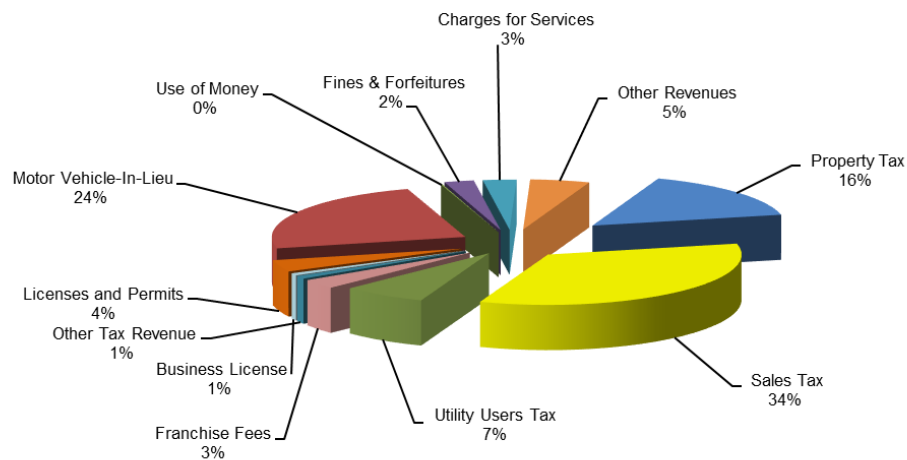
GENERAL FUND REVENUES
FY 2021-22 Proposed Budget
\$38.9 Million



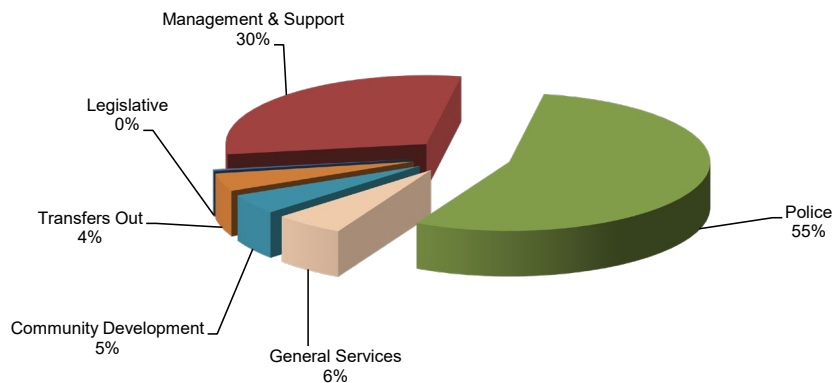
GENERAL FUND EXPENDITURES
FY 2021-22 Proposed Budget
\$39.0 Million



GENERAL FUND REVENUES
FY 2022-23 Proposed Budget
\$40.4 Million



GENERAL FUND EXPENDITURES
FY 2022-23 Proposed Budget
\$36.7 Million



General Fund Revenues

Major General Fund Revenue Sources

Property
Tax

Sales Tax

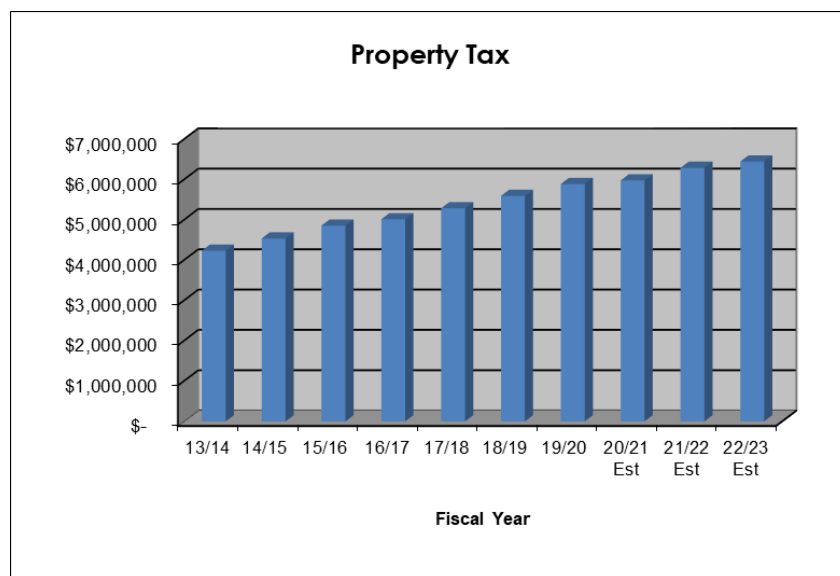
Utility
Users
Tax

Franchise
Fees

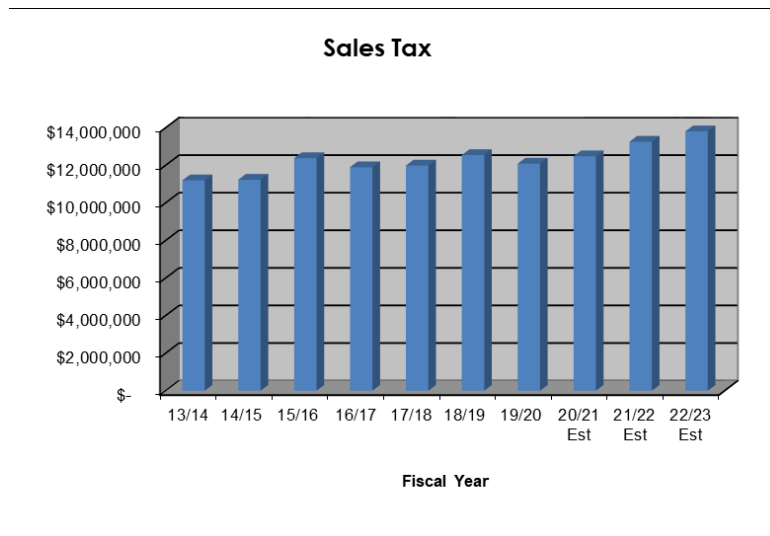
Licenses
and
Permits

Motor
Vehicle-
in-Lieu

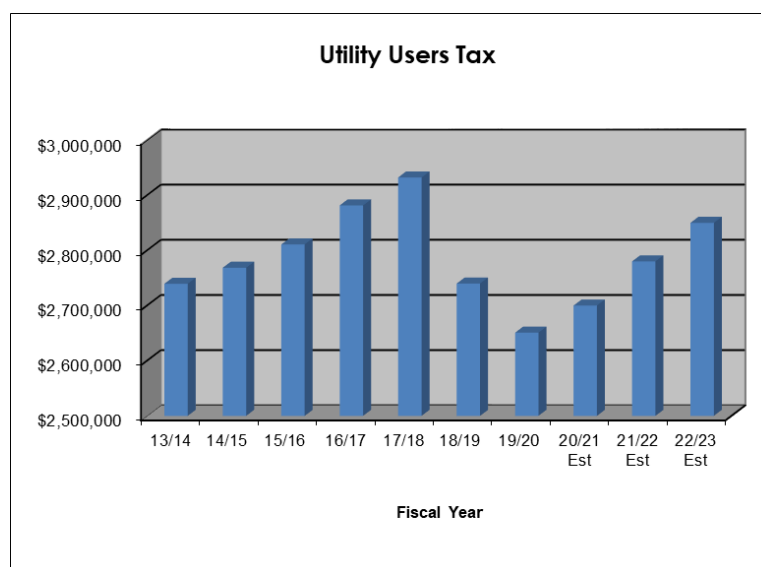
Property Tax: Property tax is imposed on real property (land and permanently attached improvements such as buildings) and personal property (movable property). It is based on the property's assessed value as determined by the County Assessor. The property tax rate is limited to 1% of the assessed value plus rates imposed to fund indebtedness approved by the voters. For FY 1998-99 through FY 2021-22, the City of Citrus Heights has agreed to pay the County of Sacramento an amount equal to the annual property tax revenue in payment of the City's revenue neutrality obligation. Property Tax revenue is projected to increase by 5.2% in FY 2021-22 and 2.5% in FY 2022-23.



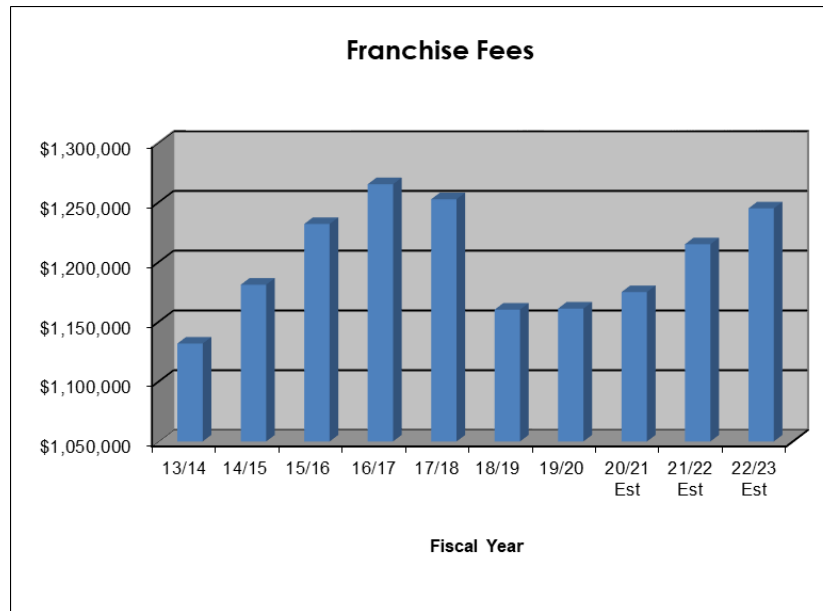
Sales and Use Tax: Sales tax is imposed on retailers for the privilege of selling tangible personal property in California. The State Board of Equalization collects 1% which is then allocated back to cities and counties based on a point-of-sale formula. The use tax complements the sales tax (at the same rate) and is imposed for transactions in which the sales tax is not collected by the seller. Revenues are projected to increase by 6.0% in FY 2021-22 and by 4.1% in FY 2022-23.



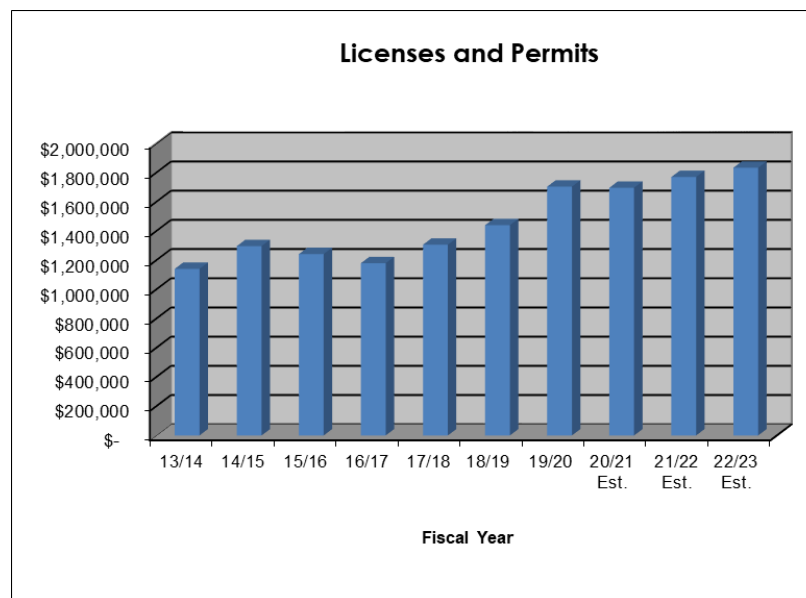
Utility Users Tax: The utility users tax is imposed on customers of electricity, gas, telephone services, cable television, and sewer service. The tax rate is 2.5% of the charges for such services. Utility User Tax revenue is projected to increase by 3% in FY 2021-22 and 2.5 % in FY 2022-23.



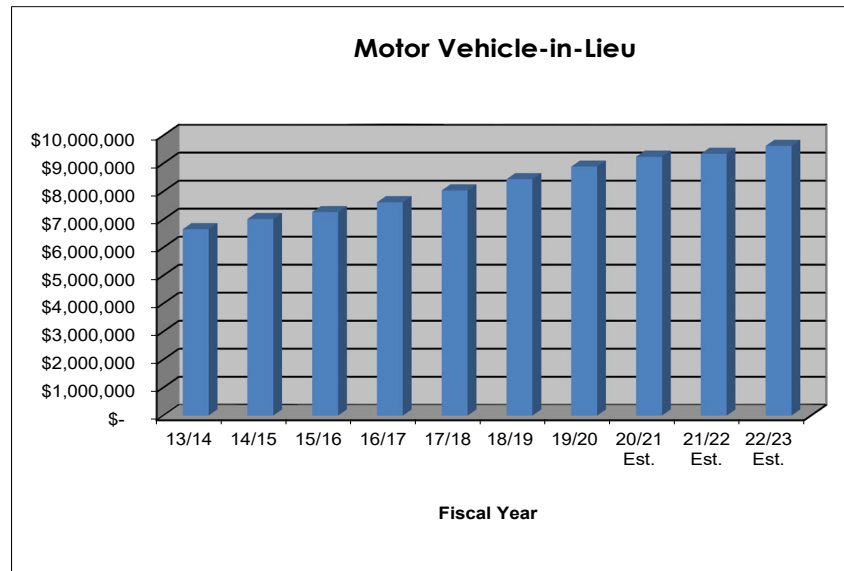
Franchise Fees: State statutes provide Citrus Heights the authority to impose fees on utility companies and other businesses for the privilege of using City rights-of-way. The City receives various franchise fees from utilities, waste management, and cable companies. Franchise Fee revenue is projected to total approximately \$1.2 million in each year of the two year budget cycle.



Licenses and Permits: Revenue in this category is generated from business license fees and a variety of permit fees to support the City's building, planning and engineering activities. Animal license fee revenue is also included in the licenses and permits category. Revenue generated from the various City licenses and permits is projected to increase by 4% in FY 2021-22 and by 3.5% in FY 2022-23.



Motor Vehicle-in-Lieu (Vehicle License Fee): Established in 1935 as a uniform statewide tax, the Vehicle License Fee (VLF) is a tax on the ownership of a registered vehicle in place of taxing vehicles as personal property. By law, all revenues from the VLF fund city and county services, but the state legislature controls the tax rate and the allocation among local governments. In 2004, the Legislature permanently reduced the VLF tax rate and eliminated state general fund backfill to cities and counties. Instead, cities and counties now receive additional transfers of property tax revenues in lieu of VLF. The property tax-in-lieu of VLF amount changes annually based on each agency's gross taxable value and is projected to increase by 1.1% in FY 2021-22 and an additional 3.0% in FY 2022-23.



Vehicle Fines: The City shares with the State and County all fines collected upon conviction of a misdemeanor or an infraction in any municipal or justice court, and bail moneys forfeited following conviction of a misdemeanor or infraction committed within City boundaries. Revenue from parking violations is also included in this amount. Estimates are based on historical activity.

Interest Earnings: The City earns revenue from the investment of idle funds. In making investment decisions, consideration is given to safety, liquidity, and yield. The Government Code limits the City to certain types of investments. Estimated revenues are based on projected cash balances and future interest rates.

Interfund Charges (Cost Allocation): The General Fund includes the following departments that provide services to other funds. The cost of providing these services is charged to the user fund.

- Risk Management Services
- Human Resources
- Information Technology
- Facility Management
- Fleet Management
- City Manager and Finance
- City Attorney
- City Clerk
- Engineering

A stylized orange tree with a thick trunk and a dense, rounded canopy of leaves. The tree is centered on the page. A horizontal gray band passes through the middle of the tree's canopy. On the left side of this band, there is a black rectangular box containing the text 'CITY OF CITRUS HEIGHTS'. To the right of this box, within the same gray band, is the text 'AUTHORIZED POSITIONS' in white.

CITY OF
CITRUS
HEIGHTS

AUTHORIZED POSITIONS

**CITY OF CITRUS HEIGHTS
AUTHORIZED POSITIONS SUMMARY**

**FY 2021-22 and FY 2022-23 BUDGET
Authorized Full-Time Equivalent (FTE) Positions**

FY 2020-21	210.25 Authorized FTE Positions
FY 2021-22	210.25 Authorized FTE Positions
FY 2022-23	210.25 Authorized FTE Positions

As the policy board, City Council approves all new allocated positions and status (employee versus contract). In addition, City Council reviews all staffing levels during the budget process. A position allocation system is the standard mechanism used by municipalities to track and monitor staffing levels and approvals. An “Authorized Positions Summary” is included in this section.

Staffing Philosophy – City Council desires to retain a mix of contract and employee staff in order to ensure a cost effective and flexible service delivery system. The City is committed to continual analysis of contract versus employee staffing determinations. City employees create the oversight and administrative guidance necessary for contractual staff.

In FY 2021-22 and FY 2022-23, the City has an authorized staffing level of 210.25 regular FTE positions and a mixture of at-will, non-benefited staff.

	FY 2020-21 Authorized FTE	FY 2021-22 Authorized FTE	FY 2022-23 Authorized FTE
Department / Position	FTE	FTE	FTE
CITY MANAGER'S OFFICE			
City Manager	1.00	1.00	1.00
Assistant to the City Manager	1.00	1.00	1.00
Executive Assistant	1.00	1.00	1.00
Office Assistant/Receptionist	1.00	1.00	1.00
City Clerk	1.00	1.00	1.00
Management Analyst I/II	1.00	1.00	1.00
Economic Development and Communications Manager	1.00	1.00	1.00
Communications Officer	1.00	1.00	1.00
TOTAL CITY MANAGER'S OFFICE	8.00	8.00	8.00
ADMINISTRATIVE SERVICES DEPARTMENT			
Administrative Services Director	1.00	1.00	1.00
Executive Assistant	1.00	1.00	1.00
Human Resources Manager	1.00	1.00	1.00
Senior Management Analyst	1.00	1.00	1.00
Management Analyst	1.00	1.00	1.00
Finance Manager	1.00	1.00	1.00
Payroll Technician	2.00	2.00	2.00
Accounting Manager	1.00	1.00	1.00
Accountant	1.00	1.00	1.00
Accounting Technician	2.00	2.00	2.00
Office Assistant	1.00	1.00	1.00
Information Technology Manager	1.00	1.00	1.00
Senior Database and Applications Analyst	1.00	1.00	1.00
Information Technology Analyst I/II	1.00	1.00	1.00
Information Technology Support Specialist	1.00	1.00	1.00
TOTAL ADMINISTRATIVE SERVICES DEPARTMENT	17.00	17.00	17.00

	FY 2020-21 Authorized FTE	FY 2021-22 Authorized FTE	FY 2022-23 Authorized FTE
Department / Position	FTE	FTE	FTE
COMMUNITY DEVELOPMENT DEPARTMENT			
Community Development Director	1.00	1.00	1.00
Chief Building Official	1.00	1.00	1.00
Senior Building Inspector	1.00	1.00	1.00
Building Inspector I/II	2.00	2.00	2.00
Development Services Technician I/II	2.00	2.00	2.00
Planning Manager	1.00	1.00	1.00
Associate Planner	2.00	2.00	2.00
Assistant Planner	1.00	1.00	1.00
Administrative Assistant	2.00	2.00	2.00
Grants & Housing Programs Technician	1.00	1.00	1.00
Housing & Human Services Program Coordinator	1.00	1.00	1.00
TOTAL COMMUNITY DEVELOPMENT DEPARTMENT	15.00	15.00	15.00
GENERAL SERVICES DEPARTMENT			
General Services Director	1.00	1.00	1.00
Operations Manager	2.00	2.00	2.00
Program Assistant	3.00	3.00	3.00
City Engineer	1.00	1.00	1.00
Principal Civil Engineer	1.00	1.00	1.00
Associate Civil Engineer	2.00	2.00	2.00
Assistant Engineer	1.00	1.00	1.00
Engineering Technician I/II	1.00	1.00	1.00
Construction/Maintenance Inspector Supervisor	1.00	1.00	1.00
Construction/Maintenance Inspector I/II	2.00	2.00	2.00
Facility and Grounds Manager	1.00	1.00	1.00
Senior Maintenance Worker	1.00	1.00	1.00
Maintenance Worker I/II	1.00	1.00	1.00
Custodian	2.75	2.75	2.75
Event and Community Center Technician	2.00	2.00	2.00
TOTAL GENERAL SERVICES DEPARTMENT	22.75	22.75	22.75

	FY 2020-21 Authorized FTE	FY 2021-22 Authorized FTE	FY 2022-23 Authorized FTE
Department / Position	FTE	FTE	FTE
POLICE DEPARTMENT			
Chief of Police	1.00	1.00	1.00
Assistant Chief of Police	-	-	-
Executive Assistant	1.00	1.00	1.00
Animal Services Officer (Senior)	1.00	1.00	1.00
Animal Services Officer I/II	2.00	2.00	2.00
Code Enforcement Officer (Senior)	1.00	1.00	1.00
Code Enforcement Officer I/II	5.00	5.00	5.00
Community Services Officer (Senior)	1.00	1.00	1.00
Community Services Officer I/II	5.50	5.50	5.50
Crime Analyst	1.00	1.00	1.00
Crime Scene/Property Evidence Specialist (Senior)	1.00	1.00	1.00
Crime Scene/Property Evidence Specialist I/II	2.00	2.00	2.00
Fleet Technician	1.00	1.00	1.00
Management Analyst (Senior)	1.00	1.00	1.00
Police Commander	2.00	2.00	2.00
Police Communications Supervisor	3.00	3.00	3.00
Police Dispatcher I/II	16.00	16.00	16.00
Police Lieutenant	6.00	6.00	6.00
Police Officer	70.00	70.00	70.00
Police Records Assistant I/II (Senior)	1.00	1.00	1.00
Police Records Assistant I/II	6.00	6.00	6.00
Police Sergeant	13.00	13.00	13.00
Program Assistant	6.00	6.00	6.00
Support Services Supervisor	1.00	1.00	1.00
TOTAL POLICE DEPARTMENT	147.50	147.50	147.50
TOTAL AUTHORIZED FTE POSITIONS	210.25	210.25	210.25

Authorized Positions	FY 2020-21	FY 2021-22	FY 2022-23
Authorized FTE	210.25	210.25	210.25
At-Will, Non-benefited Positions	as needed	as needed	as needed



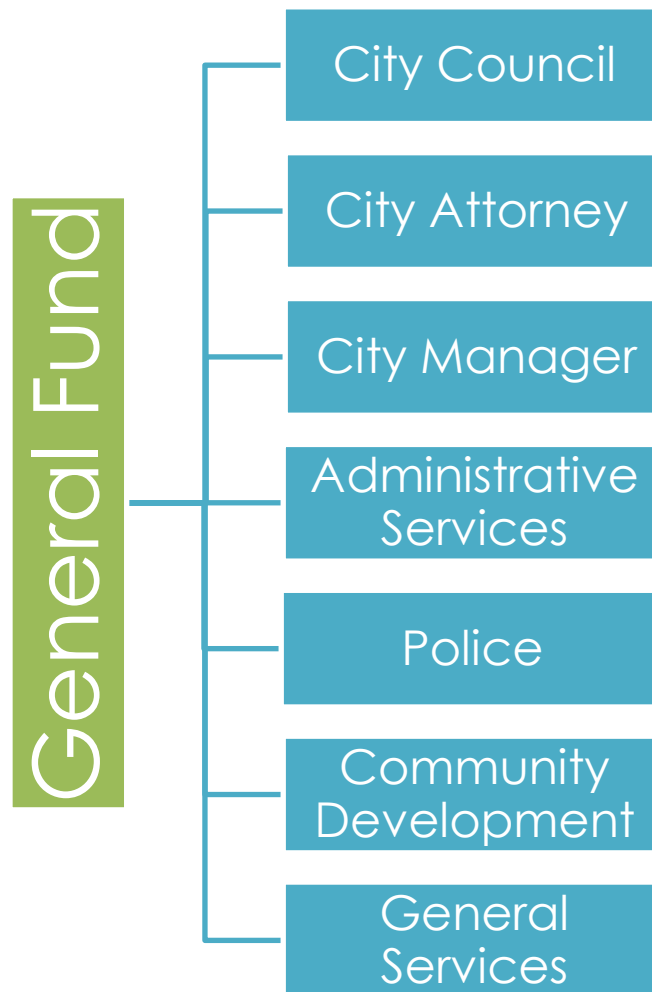


CITY OF
CITRUS
HEIGHTS

GENERAL FUND

General Fund

The General Fund is the principal operating fund of the City, and accounts for all activities except those legally or administratively required to be accounted for in another fund. The City's general operating expenditures, the fixed charges and capital cost not paid through other funds are paid by the General Fund.



A stylized orange tree logo with a thick trunk and a dense, rounded canopy. A horizontal grey band passes through the middle of the tree, containing text.

CITY OF
CITRUS
HEIGHTS

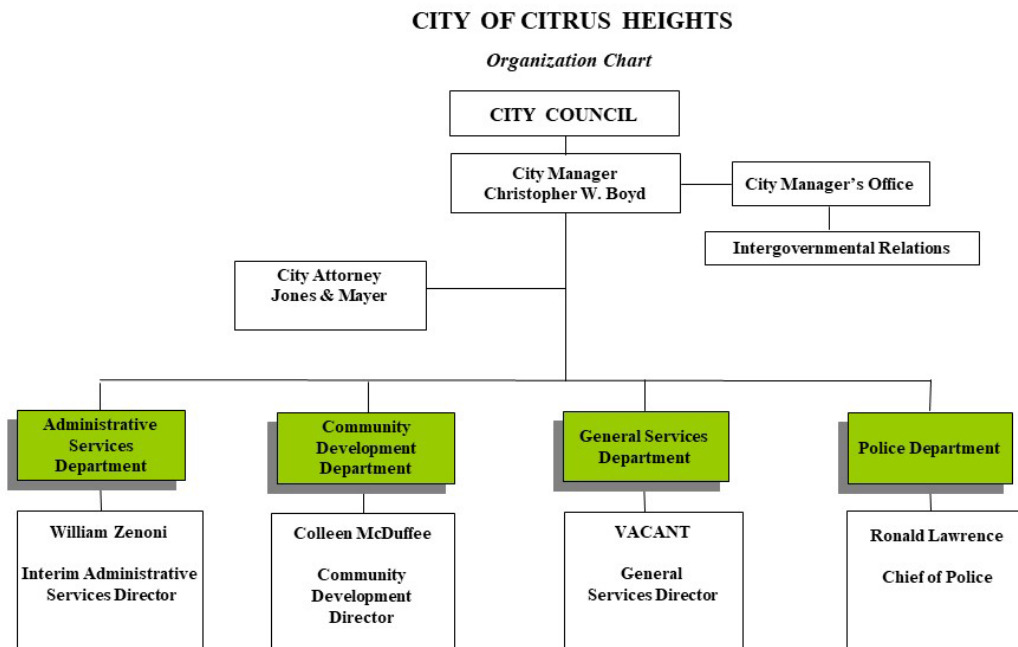
CITY COUNCIL

City Council

The Citrus Heights City Council (“Council”) consists of five members, elected by district to four-year overlapping terms. Council members must be a resident in the district in which he or she seeks election at the time nomination papers are issued. The positions of Mayor and Vice Mayor are chosen by a majority vote of Council, and the positions typically rotate annually. The Mayor conducts Council meetings and represents the City at ceremonial occasions.

Council serves as the policy board for the municipality. As an elected “Board of Directors,” Council provides policy direction, establishes goals, and sets priorities for City government. Council uses this document, the annual budget, to set policy establishing approved programs, projects, and services. Once the budget is adopted, Council funds approved programs, projects, and services focusing on advancing the City’s stated priorities. In addition to serving as policy makers for the community, Council is responsible for advocacy with numerous regional bodies, boards, and jurisdictions, ensuring that the City’s perspective and priorities are protected. Council is responsible for numerous land use decisions within its borders, including the implementation of the General Plan.

Council appoints the City Manager, City Attorney, and all members of advisory boards and commissions. Boards and commissions are advisory to Council except the Planning Commission, which has officially been delegated some decision-making parameters pertaining to the General Plan and Zoning Code. While seeking input, Council retains ultimate authority and responsibility for setting public policy.



CITY COUNCIL				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 53,148	\$ 61,292	\$ 56,295	\$ 56,904
Benefits	62,400	63,223	58,463	59,116
Services & Supplies	23,785	36,798	20,678	28,098
Total	\$ 139,333	\$ 161,313	\$ 135,436	\$ 144,118

Three-Year Goals (2021-2024)

The City Council has always made strategic planning a priority, and meets in work sessions to establish and focus on the City's goals and priorities. Approximately every six months, the City Council meets to review and refine the City's strategies and corresponding implementation goals. The goals for the next three years are:

- Maintain and enhance fiscal stability.
- Maintain public infrastructure and enhance alternative modes of transportation.
- Diversify for a resilient economy.
- Sustain and preserve public safety.
- Enhance community vibrancy and engagement.





A stylized orange tree logo with a thick trunk and a dense, rounded canopy. A horizontal grey band passes through the middle of the tree, containing text.

CITY OF
CITRUS
HEIGHTS

CITY ATTORNEY

City Attorney

The City Attorney's office provides legal advice and services to Council and staff. A review of legal issues ensures that recommendations, policies, and administrative procedures are undertaken only after consideration of sound professional advice. At Council direction, the City Attorney's office may also represent the City in legal proceedings.

General legal services are provided through a contract with the law firm of Jones & Mayer. Jones & Mayer provides specialized legal assistance when necessary and as authorized by the City.

CITY ATTORNEY		2019-20	2020-21	2021-22	2022-23
		<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Services & Supplies	\$	335,007	\$ 300,000	\$ 309,000	\$ 318,270
Total	\$	335,007	\$ 300,000	\$ 309,000	\$ 318,270

Accomplishments FY 2019-20 and FY 2020-21

- Defended several Pitchess motions to protect confidential police records.
- Negotiated easement agreements with private landowners adjacent to Arcade Cripple Creek Trail (Electric Greenway) project.
- Conducted 1234 Ethics Training and Harassment training for Council, Planning Commission and staff.
- Worked closely with PD and code enforcement on problem properties (initiated receivership actions).
- Advised on COVID-19 related executive orders, which impacted Brown Act and city services.
- Advised staff on businesses with business license issues, including revocation.
- Worked on several land use projects, including Mitchell Farms and New Sylvan.
- Advised on changes to zoning code to be consistent with state laws (i.e. ADUs).

A stylized green tree logo with a thick trunk and a dense, rounded canopy. A horizontal grey band passes through the middle of the tree, containing the text 'CITY OF CITRUS HEIGHTS' and 'CITY MANAGER'.

CITY OF
CITRUS
HEIGHTS

CITY MANAGER

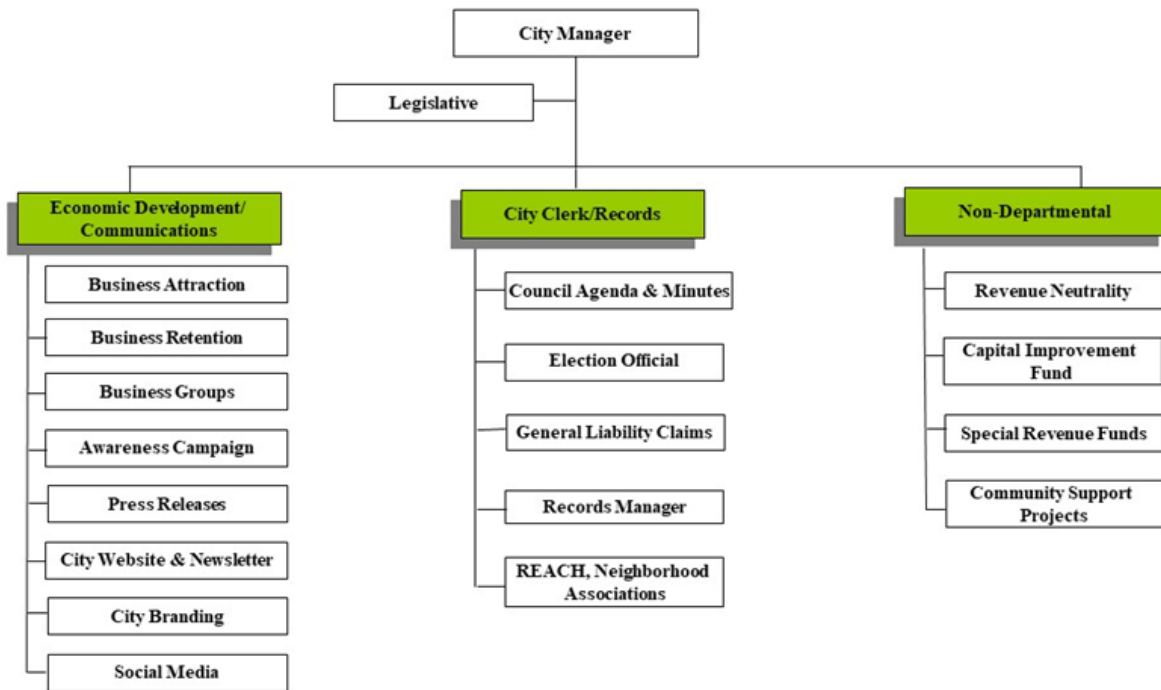
City Manager

The principal purpose of the City Manager is to implement the policy direction of the Council and to provide professional expertise in the management of a municipal corporation with a budget of approximately \$60 million and a workforce in excess of 200 staff. By acting as the Chief Executive Officer, the City Manager coordinates and facilitates the activities of the four City departments that provide direct services to the community. The department is also responsible for completing management studies, operational analyses, and special projects. In addition to managing citywide activities, the City Manager provides assistance to Council. For budget purposes, the City Manager is segmented into the following divisions:

- **City Manager** develops, advises, and makes recommendations to Council on policies, programs, legislative issues, and various City business matters. The office is comprised of the City Manager, Assistant to the City Manager, and Executive Assistant to the City Manager. The City Manager supervises executive staff and mentors key managers from each department. In addition, the City Manager supports the City Council and ad hoc committees; oversees the City's Capital Improvement Program (CIP); and leads the City's legislative advocacy program. The City Manager also oversees multi-jurisdictional collaboration efforts between the City, special districts, and the San Juan Unified School District. The City Manager also oversees bi-annual Strategic Planning retreats, the action plan from which serves as a planning guide for major Council directed initiatives for all departments.
- **City Clerk** responsibilities include records management, legislative administration, public notices, elections management, codification of city ordinances, receipt of claims, and the administration of the City's Conflict of Interest Code. The City Clerk is the City's Election Official, Records Manager, and the Filing Officer/Official for Statements of Economic Interest. As part of the legislative process, the City Clerk's Office prepares the Council agendas, minutes, and public hearing notices; maintains records of Council business and actions; and provides comprehensive information to the community and the internal organization. The City Clerk also supports the Risk Management Division by assisting with the management of general liability claims and contract administration for the City.
- **City Information** is charged with building community through a variety of methods. The division provides information to the community through various means including press releases, the website, City newsletters, and social media. It oversees the City's marketing materials and branding concepts in order to ensure consistency. The public information function serves as a "go-between" with the media and local area reporters in order to respond to requests, and ensure the City is represented in a consistent, accurate manner.
- **Economic Development** is responsible for working closely with other City departments and the private sector to implement a range of strategies designed to strengthen local businesses, expand the Citrus Heights economy, and improve the overall quality of life.
- **Non-Departmental Division** contains General Fund expenditures that cannot be attributed to a specific department or division. These expenses include the City's revenue neutrality payment to Sacramento County, transfers to the Capital Improvement Fund, transfers to partially funded Special Revenue Funds, and special City projects. A list of community support projects can be found in the Financial Summaries section of this budget.
- **Community Events** includes planning and coordinating city-sponsored events to offer a variety of family-oriented activity and entertainment opportunities throughout the year. These events include Sunday FunDay and the Holiday Tree Lighting event. Events generally have a positive impact on the community and create a sense of unity. It is also a time for City staff to engage with the community in a less formal setting.

CITY MANAGER'S OFFICE

Organization Chart



City Manager

The City Manager develops, advises, and makes recommendations to Council on policies, programs, and various City business matters. The division performs the following functions:

- Manages the implementation of policies and programs approved by Council.
- Periodically updates Council on the City's financial condition and other economic trends and issues.
- Advises Council regularly concerning program and project activities that implement Council policy.
- Provides ongoing administrative direction and supervision to City departments.
- Develops and recommends a viable annual City budget that fulfills approved goals and community priorities and future General Plan policies.
- Represents Council at various regional bodies, boards, and agencies as needed.
- Develops a successful Strategic Planning session twice a year and monitors its implementation.

CITY MANAGER				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 517,401	\$ 511,824	\$ 495,600	\$ 510,288
Benefits	209,029	204,502	174,117	180,273
Services & Supplies	18,164	57,597	33,447	37,447
Total	\$ 744,594	\$ 773,923	\$ 703,164	\$ 728,008

Total includes History & Arts Commission beginning in FY 2020-21

Accomplishments FY 2019-20 and FY 2020-21

- Responded to COVID-19 with a multi-pronged, effective emergency response across all departments.
- Maintained strong relationships with regional organizations such as the County Health Department and regional City Managers group to respond efficiently to COVID-19. Partnerships ensured sharing of knowledge and best practices in effective pandemic response.
- Prior to the onset of COVID-19, City Manager took measures to decrease expenses, including but not limited to, saving salary costs through maintaining vacancies, negotiating cost benefit sharing with personnel, and prioritizing expenditures. This resulted in a savings of more than \$1 million from the anticipated FY 19/20 budget.
- Presented the City's first two-year, mid-cycle budget update, which is a best practice in municipal financial management.
- City Manager mentoring of staff across organization with an eye toward career development.
- Immediately declared state of emergency with the California Office of Emergency Services (CalOES) in order to gain access to federal aid to assist with recovery efforts.
- Maintains regular 2x2 meetings with San Juan Unified School District, and Sacramento Fire District to foster collaboration on items of mutual interest.
- Held a Community Support Funding - Budget Workshop.
- Held a Capital Improvement Program community workshop.



* Sunrise Tomorrow community workshop to revitalize the Sunrise Mall site



*Green Acres coming in Spring 2021 to Citrus Heights

City Clerk

As the manager of the Council's business records and actions, the City Clerk is uniquely positioned to provide comprehensive information to the community and the internal organization. The City Clerk is also responsible for overseeing the Council's business records.

The City Clerk manages the agenda process and public notification requirements for the City Council's business meetings, prepares minutes of the Council meetings, and provides public access to the minutes and agendas. The City Clerk also prepares legislation, proclamations, notices, other official documents, and coordinates recruitment and membership on City commissions and task forces. The City Clerk is the filing official for Statement of Economic Interest forms and is responsible for updating the City's Conflict of Interest Code. The City Clerk also manages the election process for Council and coordinates activities and requirements with the county election staff. This includes notifying candidates, City staff, and other officials of their financial and campaign disclosure statement filing requirements. The City Clerk oversees the records management system for the City, as well as develops and implements a citywide records retention schedule.

CITY CLERK				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 142,170	\$ 145,063	\$ 100,230	\$ 103,237
Benefits	56,418	54,099	36,501	37,287
Services & Supplies	87,549	56,650	88,050	54,950
Total	\$ 286,137	\$ 255,812	\$ 224,781	\$ 195,474

ELECTIONS				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Services & Supplies	\$ -	\$ 55,000	\$ -	\$ 51,000
Total	\$ -	\$ 55,000	\$ -	\$ 51,000

Accomplishments FY 2019-20 and FY 2020-21

Increasing Transparency and Efficiency:

- Held an Ethics Training and mandated harassment training for City Council and Board/Commission Members.
- Served as the filing official for all Fair Political Practice filings.
- Updated the City's Records Retention Schedules.
- Administered the November 2020 General Municipal Election.
- Presented and implemented updates to the City Council Procedures Manual.
- Successfully recruited applications virtually for the City's board and commission.
- Worked with the Information Technology Division to implement a new Electronic Document Management System.

- Provided elections and redistricting educational programming via Facebook Live.
- Due to COVID-19 transitioned City Council meetings to a virtual platform by utilizing YouTube and Zoom Webinar, and updated the Council Agenda to include virtual participation information.
- Due to COVID-19 increased City Council meeting social media communications to encourage engagement virtually.
- Due to COVID-19 continued to successfully process online submittals of Public Records Request.

Economic Development & Communications Division

Economic Development, in partnership with other City departments and the private sector, implements a broad-range of strategies designed to improve the overall quality of life for residents and maintain essential city services valued by the community. Economic Development initiatives include: implementing the City's Economic Development Strategy and Action Plan; creating and implementing a business/jobs retention program and philosophy which are implemented throughout all City departments; developing ongoing relationships with the local broker, developer and business communities; serving as a liaison to Sunrise MarketPlace PBID; working with the Chamber of Commerce on a variety of business strengthening, jobs creation and retention activities; serving as a liaison to business associations and individual businesses/property owners; promotion of the City; and pursuing potential leads and requests for information.

Communications is charged with providing a wide variety of information to the community. Specifically, the division drafts and develops the City's electronic, citywide newsletter, oversees the content of the City's website and social media, and develops informational brochures and pamphlets in coordination with City departments. This division writes and disseminates press releases in order to promote City programs and services, while ensuring the City is represented in an accurate manner in the media.

Accomplishments FY 2019-2020 and FY 2020-2021

Foundational Communications

- Hired Communications Officer.
- Created Strategic Communication Plan.
- Re-oriented PIT crew with defined goals, meetings, and workflows for identifying and creating timely updates for community consumption.
- Increased consistency and quality of social media presence.
- Created Communications Intern program with Sac State, registered City Hall as a learning site (so interns can receive school credit).
- Launched City Hall internal newsletter "*In the Know: Inside Edition.*"
- Created Master Calendar as staff resource for reoccurring meetings and events.
- Created Virtual State of the City Broadcast:
 - Promotion through new channels to reach as many residents as possible (Mailer, local media, newsflash, social media, website, newsletter, connected TV, YouTube, etc.).
 - Coordinated video content creation spotlighting 5 City leaders and 5 community members to illustrate City's vision for the future and community resilience.
 - Website promotion.
 - Program production (facilitated via Zoom):
 - Surveys.
 - Polls.
 - VIP room (say hello on camera).
 - Host event by serving as broadcast's Emcee.
 - Talking point outlines for three speakers, as well as coordinating practice sessions.
 - Post-promotion, coordinating recordings aired on Metro Cable (to ensure availability to all audiences).

- Created Virtual Open House to highlight City service lines during socially distant time:
 - Designated 18 different service lines, working with contacts for each to gather highlights, contact information, FAQs, videos, links, contact information, etc. to spotlight each service line.
 - Built out a 3D virtual platform via SeekBeak, to showcase each service line's highlights (in house).
 - Facilitate "tours" to ensure public understand Virtual Open House.
 - Promoted across all owned channels, shared through local publications.
- Supported 5 virtual Community Workshops for various departments, creating best practices for Zoom logistics, moderating, interactions, and more.
- Held Public Information Team meetings weekly, with mini trainings to learn website maintenance best practices, News Flash formatting, content creation tips, video making tutorial (3-part series), Canva design, and more.
- Held Facebook Live Q&A sessions with:
 - City Manager, Police Chief, City Clerk, and Planning/Housing & Human services, Economic Development, Traffic team, Health expert (for flu shot clinic), Sunrise Recreations and Parks department, and supported existing Facebook Live series behind the camera for others.
- Building relationships with local media for Citrus Heights news coverage outside of already mentioned campaigns, including: Old Auburn, repaving, Cooling Center, COVID impacts on budget, and more.
- Worked with CH Messenger to publish monthly City-related articles, ensuring a consistent presence in local print media.
- Launched City's Flashvote service, reaching over 700 signups and establishing a platform for consistent two-way communications with residents.
- Re-vamped "Stay Connected" page, to provide a one-stop shop for all the ways residents can stay connected with City News, and provide feedback.
- Tracked earned media: 224 local and regional media hits, resulting in an estimated 13.9 M impressions.

Campaign-based Communications

- Executed first public relations plan "Best Lights in Citrus Heights," engaging staff, officials, and the community:
 - Won the CAPIO EPIC Award.
 - Won PRSA CCC Merit Award.
- Launched Love Local Campaign: A joint initiative between Communications and Economic Development to promote shopping local.
 - Social media.
 - Chamber support.
 - Landing page: interactive map.
 - Earned local/regional media:
 - (Yalla Yalla).
- Sunrise Tomorrow Specific Plan Communications plan:
 - Over 4 million earned media impressions, across 15 different news outlets including televisions, print, and radio.
- Join the Conversation Communications Support:
 - Digital Advertisement content creation and launch.
 - Creation of Educational mailers for Join the Conversation.
 - Social media promotion.
 - City Council presentation on findings.

- Trail and Regional Park Naming Committee Project Lead:
 - Formed Review Committee with Community stakeholders.
 - Name submission forms and Policy.
 - Webpage design and promotion.
 - Owned and earned media promotion.
- Supported New Sylvan Communications, reviewing Offering Memo, website, social media, media relations (Sac Biz, Sentinel) and piloting LinkedIn promotion to potential developers.
- Launched 14 things we love about Citrus Heights social media campaign for February.
- Supported communications campaign spotlighting two business associations: social media, video, and newsletter content.
- REACH Potluck promotion, presentation with Community Award for CH resident, post-event video with summary.
- Building safety month video to promote safety and explain why building standards are important.
- Great Plates promotion plan: web content review, video creation, social media, and local news coverage (Fox40, Messenger, and Sentinel).
- Cooling Centers communications mini-campaign, including social media, article, news coverage (Sac Bee, Sentinel).
- Measure M education:
 - Mailers.
 - Web content.
 - Answering public questions.
- Budget education:
 - Post-election news about Measure M.
 - Media Spokesperson (CBS, ABC, and Sentinel).
 - Community outreach.
- Collaborated with CHPD to put on and promote Miracle on Fountain Square Drive event:
 - COVID-19 safety precautions.
 - Team coordination.
 - Live tree lighting video with Mayor.
- Created 3D presentation for SACOG grant application, including three embedded videos, coordinated team practice sessions, facilitated presentation on the back end.
- Launched Instagram, rounding out the City's presence on all relevant channels.

COVID-19 Response Communications

- Created COVID-19 informational mailer for CH residents.
- Created signage for City Hall during closures.
- Attend weekly JIC calls with PIOs throughout the County Regional PIO COVID-19 weekly meetings, ensuring Citrus Heights has a "seat at the table" for breaking COVID updates from County PIOs.
- Launched COVID-19 portal on the website, with stand-out links from homepage:
 - Button-based webpages for different audiences, to provide quick access to the best information for Citrus Heights residents.
 - "City Hall is Here for You" service line campaign and crisis communication plan.
 - Created/promoted City Hall is Here for You City Council Video.
 - Created COVID Senior Resources Webpage with shopping hours and resources for Citrus Heights seniors.

Economic Development & Communications Division

ECONOMIC DEVELOPMENT				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 208,573	\$ 161,998	\$ 263,149	\$ 271,043
Benefits	62,035	54,685	82,776	84,335
Services & Supplies	146,560	249,050	256,522	264,218
Total	\$ 417,168	\$ 465,733	\$ 602,447	\$ 619,596

Accomplishments FY 2019-20 and FY 2020-21

Economic Development Program accomplishments

- Launched Business Retention Program and completed outreach and eight business retention visits.
- Launched Focus on Business Bi-Weekly E-Newsletter to 2800 CH Business contacts.
- Launched business welcome kit program that goes out to new business license holders monthly, averaging about 30 a month.
- Awarded \$10,000 grant via Economic Development Support Fund for Diamond level sponsorship with Citrus Heights Chamber of Commerce.
- Leveraged Activate Auburn Grant Program to provide design assistance and match dollars to four businesses on Auburn Blvd.
- Supported Grand Opening of The Oars Senior Living.
- Supported Grand Opening of Hobby Lobby.
- Supported Grand Opening of Chandos Tacos.
- Recruited Green Acres Nursery, creating over 50 jobs and activating a long vacant retail big box store.
- Redesigned materials and launched mini communications campaign on Crime Prevention through Environmental Design in partnership with Citrus Heights Police Department.
- Oversaw bi-monthly Project Coordination meetings to review development funnel, clear barriers for projects, and ensure business-friendly environment.
- Represented Citrus Heights as Vice Chair of the Economic Development Director Task Force of Greater Sacramento Economic Council.
- Convened working group of Sacramento County jurisdictions and represented Citrus Heights to work with Sacramento Employment Training Agency to incorporate workforce development programming into COVID response and long term economic development strategy.
- Held two Business Association Appreciation events and executed post event business retention surveys for the Auburn Blvd Business Association & Antelope Crossing Business Association (2020 Strategic Goal).
- Completed two Mayor business retention visits (2020 Strategic Goal).

Economic Development Project Accomplishments

- Continued support for Sunrise Tomorrow Specific Plan (2020 strategic goal):
 - On boarded consultant to complete Specific Plan.
 - Community Workshop on Feb 11 hosted over 300 guests:
 - 4.69 Million impressions on communications workshop outreach.
 - Spearheaded branding of project.

- Directed project website:
 - Multiple online community initiatives, including interactive idea wall, map, contact list, and updates.
- Coordinated stakeholder interviews:
 - Video of stakeholder takeaway for public.
- Assisted completion of Market Analysis.
- Produced Community Vision Workshop to present Specific Plan.
- Edited and launched City's webpages.
- Direct marketing activities for potential development.
- New Sylvan project execution:
 - Completed site inventory and capacity assessment.
 - Created and marketed Offering Memorandum.
 - Launched City's Webpages.
 - Highlighted property on GSEC Familiarization Tour with site selectors.
 - Executed Purchase and Sale Agreement with Woodside Homes for development.
 - Assisting Woodside Homes through the due diligence and entitlement process.

Economic Development COVID-19 Response

- Liasoned with regional government and business associations daily/weekly for ongoing information sharing and resource pull through.
- Joined the Sac Metro Chamber Rapid Response Team to aide in COVID-19 related information sharing and business triage.
- Launched COVID-19 Business resource webpage where CH businesses receive information on grants and loan opportunities.
- Created Business Roadmap webpage to aide CH businesses in COVID-19 public health guideline related information.
- Hosted two NorCal SBDC COVID-19 Paycheck Protection Program workshops with the Citrus Heights Chamber and SBDC representatives.
- Directly liasoned 78 Citrus Heights businesses with COVID-19 related information.
- Launched "Love Local" ongoing marketing and shop local campaign:
 - Google map of 135 CH businesses (take-out and delivery during COVID).
 - Joined Chamber to provide CH businesses "Love Local" window decals.
 - Boosted business social media for "Love Local" campaign sharing over 20 graphics and reaching over 20,000 jointly.

Find us on 

Facebook.com/CityofCitrusHeights



Non-Departmental Division

The budgeted division called Non-Departmental contains General Fund expenditures that cannot be attributed to a specific department or division. These expenditures include:

- The revenue neutrality payment due to Sacramento County (equals annual property tax revenue). In 1997, the City and the County of Sacramento entered into a revenue neutrality agreement pursuant to Government Code Section 56845 as a means of mitigating the financial impacts of the City's incorporation on the County's general fund. Currently, all secured and unsecured ad valorem property taxes levied and collected, pursuant to state law within the City's corporate limits (except for landscaping and lighting for special assessments), are retained by the County. The County has collected and retained property taxes since January 1, 1997 and will continue to do so through June 30, 2022.
- Community support funded a variety of community projects including diversity employment outreach, Citrus Heights Marching Band and REACH.
- Transfers to partially funded Special Revenue Funds.
- Starting in FY 2021-22, OPEB Trust and Education Reimbursement expenditures will be budgeted in Non-Departmental, as these expenditures are not specific to one department or division.

NON-DEPARTMENTAL				
	FY 2019-20	FY 2020-21	FY 2021-22	FY 2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Revenue Neutrality Payment	\$ 5,888,260	\$ 5,977,848	\$ 6,290,000	\$ -
Community Support	178,909	148,800	153,264	157,862
<u>Line of Credit</u>				
Principal	-	-	-	2,000,000
Interest	76,664	323,150	332,845	342,830
OPEB Expense	-	-	248,000	245,000
CalPERS UAL Liability	-	-	1,256,192	1,423,429
Operating Expenses	1,128,853	331,350	368,279	379,529
<u>Transfers Out to Other Funds</u>				
Code Enforcement Fund	-	57,854	59,500	59,500
SAVSA	59,109	26,671	27,500	27,500
Community Events Fund	176,317	185,318	-	-
Compensated Absences	-	-	300,000	350,000
Replacement Fund	255,532	-	390,000	1,110,000
Total Transfers to Other Funds	\$ 490,958	\$ 269,843	\$ 777,000	\$ 1,547,000
Total	\$ 7,763,644	\$ 7,050,991	\$ 9,425,580	\$ 6,095,650



CITY OF
CITRUS
HEIGHTS

ADMINISTRATIVE SERVICES DEPARTMENT

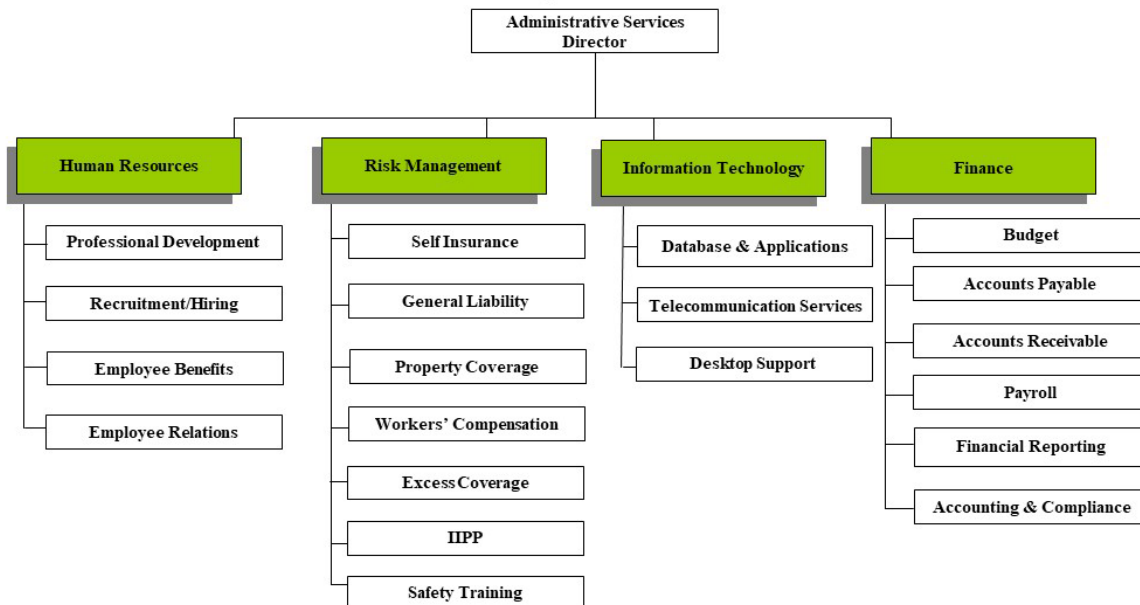
Administrative Services Department

The Administrative Services Department serves as an internal customer service provider of integrated services including Finance, Human Resources, Information Technology and Risk Management.

- **Finance Division** provides internal support to other City departments and external support to other government agencies by providing financial information to aid in their decision making process. The Finance Division is responsible for budget preparation and development, financial reporting, auditing, investments, debt management, accounts payable, revenue collection, payroll and grant accounting.
- **Human Resources Division** implements the City's mission, vision, and core values through policy development and implementation. The Human Resources Division administers the City's health benefits, leave and compensation programs and ensures compliance with federal and state employment laws. Human Resources personnel manage recruitments, hiring and agency-wide employment training. The division works closely with supervisors and managers on employee or organizational issues.
- **Information Technology Division** encompasses the City's computer technology and telecommunications systems. The division provides a vision for future technology needs and oversees the procurement of new equipment. It maintains the City's network and infrastructure, and works with departments to address specific software needs. The technical training the division provides employees ensures equipment and software are used efficiently. The combined professional acumen of staff in this division enhances business and day-to-day operations, allowing the City to function optimally.
- **Risk Management Division** seeks to maintain a safe and fair environment for employees and the community and to ensure that City assets are protected from loss, theft, and misuse. The division provides safety trainings and coordinates treatment for employee injuries, providing support to the employee with the mission of assisting with a healthy recovery. Risk Management personnel manage all liability claims, property claims, insurance for special events and contractual risk transfer for City contracts.

ADMINISTRATIVE SERVICES DEPARTMENT

Organization Chart



Finance Division

The Finance Department provides internal support to other City Departments and external support to other government agencies by providing financial information to facilitate their decision-making process. The Finance Division also provides financial analysis and forecasting using the City's Ten-Year Financial Model and administers over 45 funds in accordance with Generally Accepted Accounting Principles.

FINANCE				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 582,269	\$ 598,319	\$ 710,868	\$ 739,786
Benefits	179,500	221,556	184,797	195,866
Services & Supplies	<u>135,842</u>	<u>142,905</u>	<u>131,145</u>	<u>132,270</u>
Total	\$ 897,611	\$ 962,780	\$ 1,026,810	\$ 1,067,922

Accomplishments FY 2019-20 and FY 2020-21

- Prepared City's first 2-year budget.
- Audited and processed 6,422 accounts payable invoices in FY 2019-20.
- Added Alarm Billing to the City's on-line payment options.
- Presented regular updates of City's Treasury report to City Council.
- Submitted FY 2018-19 & FY 2019-20 Comprehensive Annual Financial Reports to Governmental Finance Officers Association (GFOA) for award consideration and received Certificate of Achievement for Excellence in Financial Reporting award for 19th and 20th consecutive years.
- Presented mid-cycle and mid-year budget updates to City Council.
- Coordinated with FEMA/CalOES for COVID-19 expense reimbursement.
- Implemented Automated Clearing House (ACH) payments for accounts payable invoices.
- Transitioned to working from home due to the COVID-19 pandemic with little interruption to City services.
- Successfully completed four financial audits, virtually, with no findings.

Human Resources Division

The primary mission of the Human Resources Division is to develop a staff and workforce philosophy that embodies the City Council's mission, vision, and core values in daily decision-making. The Human Resources Division is charged with recruiting, hiring, and retaining talented and qualified employees to assist departments in serving their customers. It is the division's responsibility to implement City Council's compensation philosophy, both benefits and salary. The division is also responsible for monitoring and implementing Federal and State laws that apply to the City's workforce and to develop and implement appropriate policies and procedures.

HUMAN RESOURCES				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 418,609	\$ 430,045	\$ 274,864	\$ 290,611
Benefits	153,711	160,270	73,161	81,811
Services & Supplies	181,112	246,812	252,034	249,565
Total	\$ 753,432	\$ 837,127	\$ 600,059	\$ 621,987

Accomplishments FY 2019-20 and FY 2020-21

- Facilitated 21 recruitments resulting in approximately 31 new employees to full-time, part-time, and temporary positions.
- Completed an agency-wide Administrative Support Classification Study.
- Completed numerous COVID-19 related training webinars.
- Created numerous COVID-19 compliance policies.
- Created and implemented Temporary Teleworking Policy.
- Implemented Families First Coronavirus Response Act.
- Created and implemented COVID-19 Prevention Plan.
- Staff completed COVID-19 exposure and contact tracing certification program.
- Provided training for management staff.
- Implemented the Employee Appreciation Program.

Information Technology Division

As a local agency, the City actively utilizes various computer technologies and systems to provide, organize, and develop information to its customers on a daily basis, including an internal network, access to the Internet, and a website. The mission of the Information Technology Division is to enhance customer service and operational efficiency through the use of technology. It is the responsibility of the division to identify changing technology and how it can best be integrated into our current system to increase the efficiency of City operations. The division provides daily assistance to end-users and departments, strategically utilizing technology to enhance business and daily operations. In order to provide up-to-date information and maximize e-commerce opportunities, this division continuously monitors and refines the City's web presence. The division also assists in the research and deployment of business applications to enhance staff's effectiveness and efficiency and to provide greater customer service to the public.

INFORMATION TECHNOLOGY				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 513,626	\$ 539,211	\$ 514,728	\$ 532,199
Benefits	180,547	175,437	137,995	143,407
Services & Supplies	317,495	320,604	349,973	364,471
Capital Acquisition	-	6,875	7,081	7,294
Transfers	-	-	-	-
Total	\$ 1,011,668	\$ 1,042,127	\$ 1,009,777	\$ 1,047,371

Accomplishments FY 2019-20 and FY 2020-21

- Continuing computer upgrades and laptop replacements.
- Migrating data and department users from old Electronic Document Management System (EDMS) to the new system.
- Continue to migrate computers from Windows 7 to Windows 10.
- Implemented and supported "work from home" needs due to COVID-19.
- Upgraded Antivirus system.
- Replaced Antispam system.
- Upgraded Security/Access systems.
- Added Encroachments to Permits system.

Risk Management Division

The City's Risk Management Division seeks to establish and maintain internal fiscal and policy controls to avert risk to the organization and employees, including ensuring City assets are protected from loss, theft, and misuse, as well as protecting employees from injury and mistreatment. The division works to reduce City losses from liability claims, lawsuits, and property damage. The division is also responsible for reviewing submitted claims against the City to determine any liability and the appropriate actions.

The Risk Management Division seeks to foster a safe and ergonomically sound workplace environment and minimize injuries to employees resulting in workers' compensation costs and lost time. Employee injuries are closely monitored with the goal of ensuring that the employees receive sound medical treatment that returns them to work in a healthy and prompt manner. Through a proactive employer relations program, the City seeks to ensure that employees are adequately trained on workplace policies and procedures, the appropriate means for reporting an incident, and the consequences of violation. The goal is to maintain an environment that is free of workplace wrongdoing. The division is also charged with establishing and monitoring contract procedures and insurance requirements to ensure the City's interests are protected, while retaining the ability to conduct business in a prompt and reasonable manner.

RISK MANAGEMENT				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 231,865	\$ 238,494	\$ 269,231	\$ 281,646
Benefits	83,682	80,895	80,176	85,848
Services & Supplies	1,055,845	999,930	1,293,205	1,327,668
Total	\$ 1,371,392	\$ 1,319,319	\$ 1,642,612	\$ 1,695,162

Accomplishments FY 2019-2020 and FY 2020-21

- Continued to monitor employee injuries in order to facilitate returning to work as soon as possible in a healthy and efficient manner.
- Implemented security measures to prevent fraudulent activities.
- Managed open workers' compensation claims and open liability claims working with the City Clerk, third-party administrators, and legal counsel.
- Monitored ongoing Wellness Program to reduce sworn personnel work-related injuries.
- Monitored legislative actions with impacts to labor, risk management, and benefits.
- Facilitated training on occupational safety topics including annual bloodborne pathogen training.
- Implemented the safety committee's quarterly newsletter for staff.
- Piloted injured employees return to work Bridge Assignment Program.
- Implemented COVID-19 screening process for City employees and guests.
- Provided work-from-home ergonomic tips and virtual ergonomic assessments.

A stylized blue tree logo with a thick trunk and a dense, rounded canopy. A horizontal grey band passes through the middle of the tree, containing text.

CITY OF
CITRUS
HEIGHTS

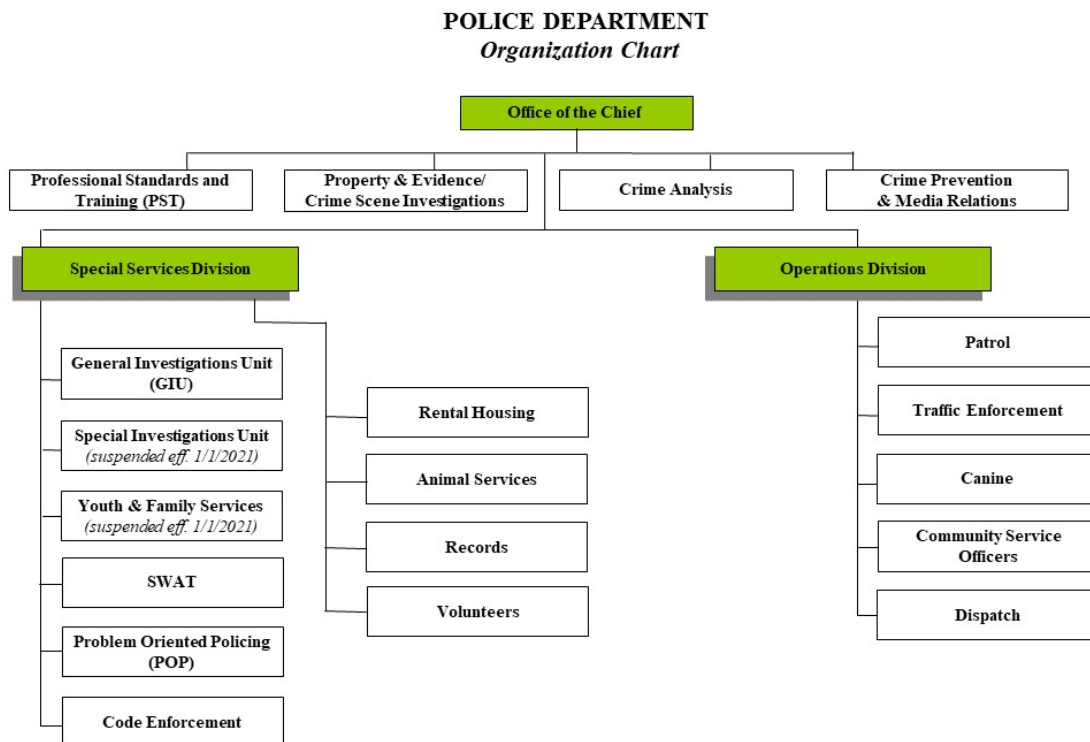
POLICE DEPARTMENT

Police Department

The City's Police Department has been serving the residents of Citrus Heights for twelve years.

The Police Department is a full-service organization, which is responsible for general law enforcement and peacekeeping. It has among its many responsibilities: improving the quality of life in the community, preserving public order, protecting life and property, investigating crimes, apprehending criminal offenders, investigating traffic collisions and enforcing criminal and traffic laws. The department is divided into three divisions:

- **Office of the Chief**
- **Operations Division**
- **Special Services Division**



Office of the Chief

Office of the Chief of Police is comprised of the chief, a management analyst, an executive assistant, Professional Standards and Training unit, Property and Evidence unit, Crime Scene Investigations unit, and Crime Prevention and Media Relations unit. This division is responsible for the effective administration, management, and coordination of police services in the community and ensuring the highest standards of professionalism and integrity.

Professional Standards and Training (PST) consists of the PST lieutenant, PST sergeant, and a CSO who are dedicated to ensure adherence to the laws, rules, and regulations which govern the department's responsibility to provide for the public's safety, security, and quality of life. PST is managed by the PST Lieutenant who coordinates all department training, hiring, media relations/public information, internal investigations, and oversight of the Property & Evidence unit.

POLICE DEPARTMENT				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 12,677,111	\$ 13,808,276	\$ 11,868,656	\$ 12,215,133
Benefits	5,208,650	5,487,501	4,472,351	4,702,211
Services & Supplies	2,180,679	2,510,680	2,378,912	2,426,757
Capitol Acquisition	-	-	-	-
Transfers	-	-	10,000	10,000
Total	\$ 20,066,440	\$ 21,806,457	\$ 18,729,919	\$ 19,354,101

Accomplishments FY 2019-20 and FY 2020-21

- Analyzed department staffing model to promote optimal efficiency, oversight and reallocation of resources by creating a police lieutenant position.
- Established the Police Corporal program for sworn personnel and the Senior program for professional staff to enhance police supervision and aid in succession planning.
- Coordinated Team Building Workshops for management staff to provide introspection in evaluating their own leadership style as well as coming together to create a Strategic Plan and develop a SWOT analysis.
- Management Staff completed leadership assessments to strengthen working relationships and support professional growth between the team members.
- Issued 13 General Orders (emergency department policy) related to COVID-19 mitigation efforts designed to protect our employees, as well as the public, while maintaining public safety services. These General Orders followed CDC recommendations and provided specific direction for employees in and outside the building. Created new signage throughout the building providing guidelines for requirements of masks, social distancing and disinfecting protocols.
- The department utilized the Company Nurse screening app that required employees to complete online screening protocols prior to entering the building each day.

Operations Division

Operations Division operates under the Community Policing Philosophy. The division consists of Patrol Services comprised of six teams of uniformed patrol officers. Additional elements deployed within the division are Traffic Enforcement, Canine, the Communications Center, Community Service Officers, and the Mobile Crisis Support Team. Operations Division staff provides round-the-clock enforcement presence, and are first responders for life-threatening emergencies, traffic collisions, and in-progress criminal activity. They conduct preliminary investigations on the majority of crimes reported to the Police Department and implement proactive strategies to detect and deter crime. Operations Division staff is actively involved in addressing neighborhood problems and quality of life issues.

The Traffic Enforcement Unit augments the traffic enforcement effort of patrol officers by providing directed traffic enforcement in identified problem areas. This unit also conducts follow-up collision investigations and handles major collision investigation and reconstruction.

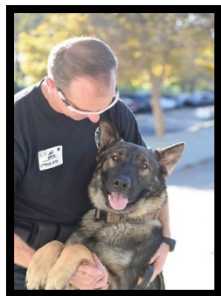
Community Service Officers provide assistance to patrol officers and the Traffic Unit by responding to non-emergency calls for service.

The Mobile Crisis Support Team pairs Crisis Intervention Trained (CIT) Officers with licensed mental health professionals to respond to calls involving subjects in a mental health crisis. This first responder model focuses on mitigating the impact of these crises while providing enhanced service to those with mental health needs.

The Communications Center answers non-emergency and all emergency 9-1-1 calls that come into the police department, with police dispatchers coordinating field response to calls for service.

Accomplishments FY 2019-20 and FY 2020-21

- Successfully completed our first-ever Communications Center evacuation drill.
- Completed new dispatch training room and re-design of Communications Supervisors workstations to enhance capabilities and training for dispatch staff and communications training staff.
- Successfully upgraded the Use of Force Simulator providing valuable training to officers.
- Trained additional staff certified in UAS equipment to enhance field technological capabilities.
- Developed strategies to manage staffing shortages and reduced workforce mitigation to combat the impacts on the fiscal budget and ensure basic patrol services were maintained for the community.
- Completed multiple DUI checkpoints and obtained grant funding for continued DUI enforcement with hopes of hosting additional checkpoints when COVID precautions stabilize.
- Launched Senior Lead Officer/Corporal selection process for developing future Sergeants.
- Launched a Leadership Academy designed to develop leadership skills of our Senior Lead Officer/Corporal positions to assist with succession planning.
- Stood up "Virtual" Department Operations Center to maintain constant situational awareness related to the Pandemic as well as several civil unrest, fire, and severe weather incidents.
- Reconfigured deployment model and service protocols, aligning with public health orders and CDC recommendations.
- Maintained adequate staffing with a re-organization of assignments with reduced staffing during the pandemic and with budget shortfalls.
- Developed strategies to improve sanitization of fleet vehicles, the department equipment, and combined use areas to mitigate COVID-19.
- Assisted with multiple state and regional emergencies for mutual aid including fires, and civil unrest.



Special Services Division

Special Services Division consists of a variety of units and specialty assignments including the General Investigations unit, Special Investigations unit, Youth and Family Services unit, Problem Oriented Policing, Special Weapons and Tactics (SWAT) Team, Code Enforcement and Rental Housing unit, Animal Control, Records, and Volunteers.

The General Investigations unit includes crimes against persons and property crime investigators. The Special Investigations unit detectives focus their efforts on violent criminal offenders, sex offenders, gang and narcotics investigations, human trafficking and tracking habitual offenders.*

**Note: As a result of budget deficits and shrinking staffing levels, the Special Investigations unit has been suspended.*

The Youth and Family Services (YFS) unit is comprised of a YFS sergeant, the School Resource Officers (SRO's), and a juvenile detective. The juvenile detective coordinates the department's Juvenile Diversion and Education program and investigates juvenile related crimes. The YFS Unit is responsible for administering all of the Department's youth related services to include; the Police Activities League (PAL) and Police Explorer program.*

**Note: As a result of budget deficits and shrinking staffing levels, the Youth and Family Services unit has been suspended.*

The Problem Oriented Policing and Code Enforcement and Rental Housing units focus on improving the quality of life in the community through the development of community partnerships, addressing neighborhood problems and providing crime prevention information. The Code Enforcement unit enforces nuisance abatement codes involving zoning, inoperable vehicles, graffiti, and housing. This unit's program goal is to obtain voluntary compliance with the City's Municipal Ordinance. Code Enforcement officers work closely with other City departments and divisions including Finance, Building and Safety, Planning, and Engineering during any enforcement effort. The unit also assists other local and state agencies, including the Fire District, in matters of code enforcement. This unit is both reactive to citizens' requests for services as well as proactive in canvassing the community to correct blight conditions to maintain and improve property values and the quality of life in the City.

The Animal Services unit focuses on promoting healthy human-animal relationships in our community. Through education and enforcement, our goal is to empower responsible pet owners and deliver visible and compassionate support to the community.

The Records unit manages front counter and telephone customer service, volunteer program coordination, police records management and processing.

ANIMAL SERVICES				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 211,723	\$ 262,896	\$ 295,860	\$ 304,739
Benefits	77,729	106,853	117,519	121,346
Services & Supplies	249,111	327,700	337,060	345,080
Capital Acquisition	-	-	-	-
Transfers	-	-	-	-
Total	\$ 538,563	\$ 697,449	\$ 750,439	\$ 771,165

Accomplishments FY 2019-20 and FY 2020-21

- Dedicated officer assigned to Sunrise Marketplace, resulting in approx. 30% drop in total crime in SMP.
- sUAS (small Unmanned Aerial Systems) program grew to 11 pilots conducting over 400 flights.
- Created three virtual “court rooms” for staff to attend court.
- Modified front counter and lobby operations to meet public health order mandates and CDC recommendations.
- During 2020 pandemic, Navigator was able to house 106 clients
- Launched Rental Housing Inspection Program, identified over 5800 safety related violations and worked with property owners to correct many of these issues.
- Code Enforcement handled all administrative business complaints, related to Public Health Order Compliance, through personal contact and education.
- POP and Code Unit administered a program to provide free masks to local businesses.
- Provided Active Threat Training to teachers and administrators of San Juan and Mesa Verde HS.
- Formed new Youth and Family Services (YFS) unit comprised of a Sergeant, 2 school resource officers, an anti-vape grant officer, and a juvenile detective.
- The YFS team shifted during the pandemic to provide several special events for our local youth including over 20 “drive by” birthday caravans, a video graduation celebration for our high school seniors, and events like “Reverse Trunk or Treat” and “Miracle on Fountain Square” which provided pandemic friendly holiday celebrations.
- Graduated nine students from Mesa Verde Pathway program.
- Obtained \$600,000 Grant for anti-tobacco/ vaping officer and education campaign.
- Joined Regional Trident taskforce to further combat illicit drugs in our community.



A stylized green tree with a thick trunk and a dense, rounded canopy. A horizontal grey band passes through the middle of the tree, containing text.

CITY OF
CITRUS
HEIGHTS

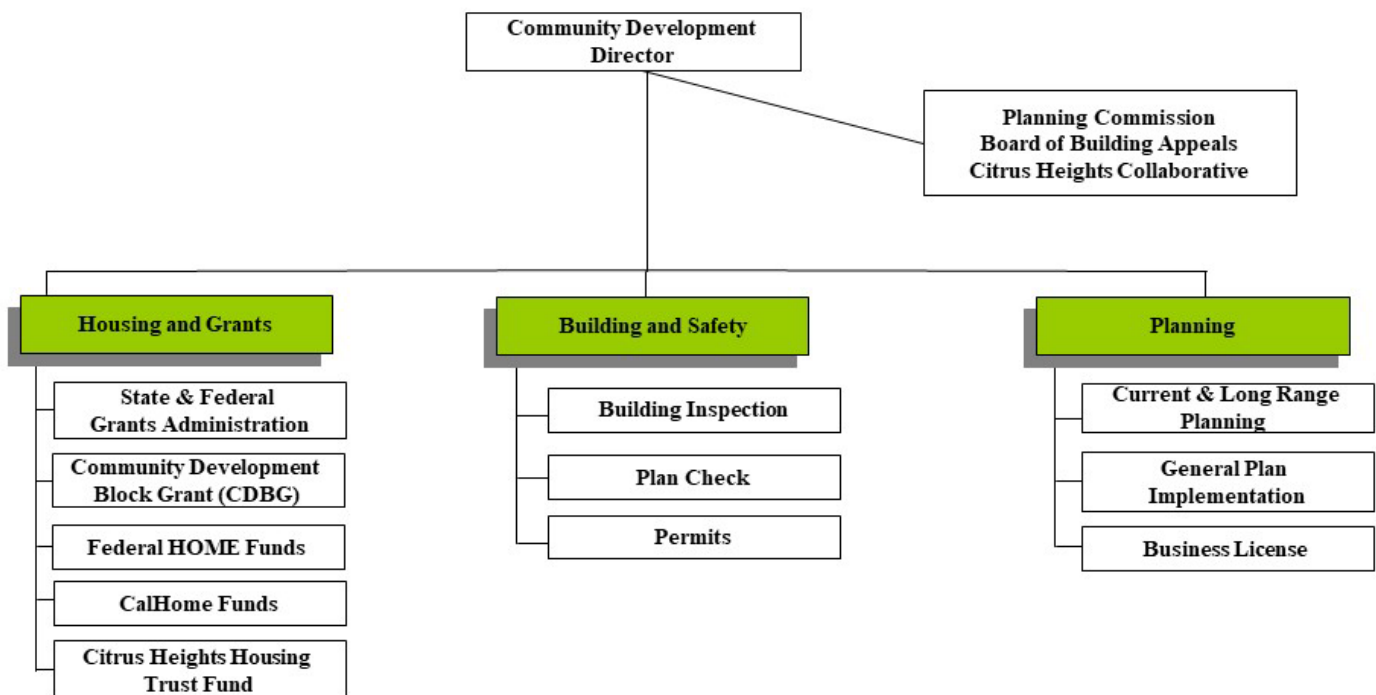
COMMUNITY DEVELOPMENT

Community Development

The Community Development Department encompasses three divisions: Building and Safety; Housing and Grants; and Planning. The department supports two City Commissions, the Construction Board of Appeals and Planning Commission, as well as the Citrus Heights Collaborative.

- **Building and Safety** is responsible for the administration and enforcement of the adopted local and state-mandated codes that regulate construction, property maintenance, and development. The purpose of these codes is to establish the minimum requirements to safeguard public health, safety, and general welfare. This division supports the Construction Board of Appeals.
- **Housing and Grants** administers a number of local, state, and federal housing and community development grants. These funds are used to assist first-time homebuyers and provide loans to modest-income homeowners to rehabilitate their homes. Additional uses for these funds include various public service projects such as: senior nutrition program, homeless services, housing crisis assistance, and youth projects. This division supports the Citrus Heights Collaborative.
- **Planning** comprises current and advanced planning activities as well as environmental analysis to ensure development is consistent with local and state land use regulations. This division supports the Planning Commission. The Planning Division also oversees the City's business license program.

COMMUNITY DEVELOPMENT DEPARTMENT *Organization Chart*



Building and Safety Division

The Building and Safety Division is responsible for reviewing plans, issuing permits, performing field inspections of buildings, ensuring compliance with local, state, and federal regulations related to building construction, maintenance, repair, accessibility and access requirements, as well as energy and green building standards. This division also enforces local standards by regulating the design, construction, use, occupancy, location, and maintenance of all buildings and structures within the City. Other responsibilities include assisting the Problem-Oriented Policing (POP) Code Compliance Division, Fire District, Engineering Division, and Planning Division. This division supports the Construction Board of Appeals.

BUILDING INSPECTION				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 577,292	\$ 581,213	\$ 651,834	\$ 671,346
Benefits	215,153	208,050	220,060	227,254
Services & Supplies	105,895	79,300	77,891	81,064
Capital Acquisition	-	-	-	-
Total	\$ 898,340	\$ 868,563	\$ 949,785	\$ 979,664

Accomplishments FY 2019-2020 and FY 2020-21

- Increased usage of Citizen Access Portal for online building permits now at 98% online.
- Maintained a Building Code Effectiveness Grading Classification of (2) through the Insurance Services Office, Inc. (ISO).
- Completed last phase of inspection program for exterior elevated elements (decks, balconies, stairs and associated waterproofing elements) for multi-family buildings in compliance with SB 721.
- Participated in and promoted the PASS program (Prequalified Architectural Submittal System).
- Implemented remote video inspection program due to COVID-19 pandemic.
- Updated Building Division permitting processes, forms, handouts and web content due to pandemic and closure of City Hall lobby.
- Completed staff training as required for staff to maintain their professional certifications.
- Worked collaboratively with Code Enforcement in regards to dangerous and/or substandard buildings.
- Issued 2500 permits remotely online.
- Performed over 6000 inspections.

Housing and Grants Division

The Housing and Grants Division is responsible for the administration of local, state, and federal housing funds and oversees programs funded with these revenues. These programs include Community Development Block Grant (CDBG) Program, federal HOME funds, CalHome funds, the Citrus Heights Housing Trust Fund, Housing Successor Agency, and several special purpose funds (i.e., housing loan repayments and housing mitigation fees). These funds are used to improve the quality of the City's housing stock, increase home ownership rates, and revitalize neighborhoods. The division's funds also pay for services that benefit low and moderate-income households, such as senior meals and youth programs, as well as homeless prevention and assistance. In addition, the division's funds may be used for special community needs, such as adding or modernizing parks, installing streetlights, improving handicapped access, and various economic development initiatives.

HOUSING AND GRANTS				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 121,832	\$ 54,336	\$ 56,856	\$ 58,614
Benefits	51,455	19,722	19,498	20,007
Services & Supplies	<u>6,035</u>	<u>7,352</u>	<u>7,573</u>	<u>7,799</u>
Total	\$ 179,322	\$ 81,410	\$ 83,927	\$ 86,420

Accomplishments FY 2019/20 and FY 2020/21

- Developed the City's 2020-2024 Consolidated Plan for the Community Development Block Grant Program.
- Conducted a regional Analysis of Impediments study, as required by the U.S. Department of Housing and Urban Development Department.
- Received and processed over \$180,000 in loan repayments for both the City's First-Time Homebuyer and Housing Repair programs.
- Served approximately 35,000 low- and moderate-income households in Citrus Heights through public service programs.
- Partnered with CHPD to create a Supplemental Navigator Fund to assist the City's Homeless Navigator in providing temporary housing and services to people experiencing homelessness in Citrus Heights.
- Partnered with the Sunrise Recreation and Park District to complete the Rusch Park Improvement Project.
- Created a Critical Repair Grant Program to provide grants up to \$20,000 to eligible homeowners in Citrus Heights for single-system repairs and accessibility modifications.
- Housing and Human Services Program Coordinator served as a member of Sacramento Steps Forward's advisory board, which administers Continuum of Care homeless funding on behalf of Sacramento County, in an effort to improve the performance and outcomes of the regional homeless response system.
- Collaborated with Jamboree Housing and federal, state and local partners to secure necessary financing to commence construction of the Sunrise Pointe affordable housing development as part of the City's effort to replace the low- and moderate-income housing units demolished on Sayonara Drive.

- Facilitated the Citrus Heights Collaborative, an informal network of community members, government agencies, non-profit organizations, faith-based groups, and others who gather monthly to share information and resources.
- Awarded approximately \$376,000 in federal CDBG-CV funds to provide essential services to low-income residents at-risk of COVID-19, including the Supplemental Navigator Fund.
- Submitted the City's 2020 Consolidated Annual Performance Evaluation Report (CAPER) to Housing and Urban Development (HUD).
- Transitioned the Citrus Heights Collaborative meetings to a virtual setting in response to COVID-19.
- Submitted the City's Permanent Local Housing Allocation Application to the California Department of Housing and Community Development (HCD).
- Transitioned operations in order to provide customer service to residents, co-workers, and service providers while working remotely during COVID-19.

Planning Division

The Planning Division is responsible for reviewing all proposed new residential, commercial, and office development projects. This analysis also includes completing the appropriate level of environmental review. In addition, the Planning Division conducts advanced planning activities related to the implementation of the *General Plan*, *Stock Ranch Guide for Development*, and *The Boulevard Plan*, special planning studies, including design guidelines and new ordinances. The division provides customer assistance at the “one-stop” counter and coordinates with other City departments as well as local, regional, state, and federal agencies. The division also provides staff support to the Planning Commission, special committees, and City Council regarding all matters relating to land use. The Planning Division also oversees the business license program.

PLANNING				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 442,604	\$ 540,087	\$ 411,434	\$ 423,717
Benefits	182,086	210,701	144,268	149,751
Services & Supplies	52,628	90,100	92,807	95,587
Total	\$ 677,318	\$ 840,888	\$ 648,509	\$ 669,055

Total includes Planning Commission beginning in FY 2020-21

Accomplishments FY 2019-20 and FY 2020-21

- Initiated a General Plan Amendment, Specific Plan, and Environmental Impact Report for the Sunrise Tomorrow (Sunrise Mall) site.
- Updated the General Plan Housing Element pursuant to State Law.
- Adopted the Electric Greenway Trail Project Mitigated Negative Declaration.
- Developed new guidelines for SB743 compliance.
- Approved Auburn Heights, the City's first Small Lot Housing Development.
- Approved Fair Oaks Senior Affordable Apartments, 110 new affordable apartments restricted to those at least 55 years old.
- Established Permit Ready Accessory Dwelling Unit Program (PRADU).
- Established Planning Principals for New Sylvan site and coordinated with potential developer.
- Created the Temporary Outdoor Program to assist businesses during COVID-19, by creating a simplified process to transfer indoor operations to sidewalks and parking lots.
- Conducted EIR Monitoring and implementation for the construction of the Mitchell Village Development.
- Processed 77 entitlements in Calendar Year 2019-20.

A stylized orange tree logo with a thick trunk and a dense, rounded canopy. A horizontal grey band passes through the middle of the tree, containing text.

CITY OF
CITRUS
HEIGHTS

GENERAL SERVICES DEPARTMENT

General Services Department

The General Services Department provides engineering services, maintains the City's infrastructure, and administers all public works contracts and capital improvement projects. The department's funding sources are local, state and federal transportation funds, grants, user fees, and general tax revenues. General Services provides administrative oversight for four operational divisions including Engineering, Citywide Operations and Maintenance, Fleet and Facilities Services, and Community Services.

Engineering Division Areas of Responsibility:

- ADA compliance in public ROW.
- Assessment district administration.
- Capital Improvement Program (CIP) development.
- Construction inspection.
- Development review.
- Street lighting.
- Stormwater management projects/programs.
- Traffic signals and system management.
- Transportation and traffic engineering.
- Transportation capital projects/programs.
- Transportation maintenance projects/programs.
- Transportation planning and construction grants.

Citywide Operations and Maintenance Areas of Responsibility:

- Capital right of way (ROW) acquisitions.
- CIP program administration.
- Encroachment permits.
- City staff liaison for Sacramento Transportation Authority (STA).
- City staff liaison for Sacramento Area Council of Governments (SACOG).
- Citywide creek, channel, and open space maintenance.
- Citywide infrastructure maintenance.
- Citywide landscape and maintenance.
- Citywide public tree maintenance.
- Curb, gutter, and sidewalk repair/maintenance.
- Landscape Maintenance Assessment Districts.
- Operating and capital budgets.
- Street sweeping.
- Streets and road maintenance including signage, and roadside drainage maintenance.

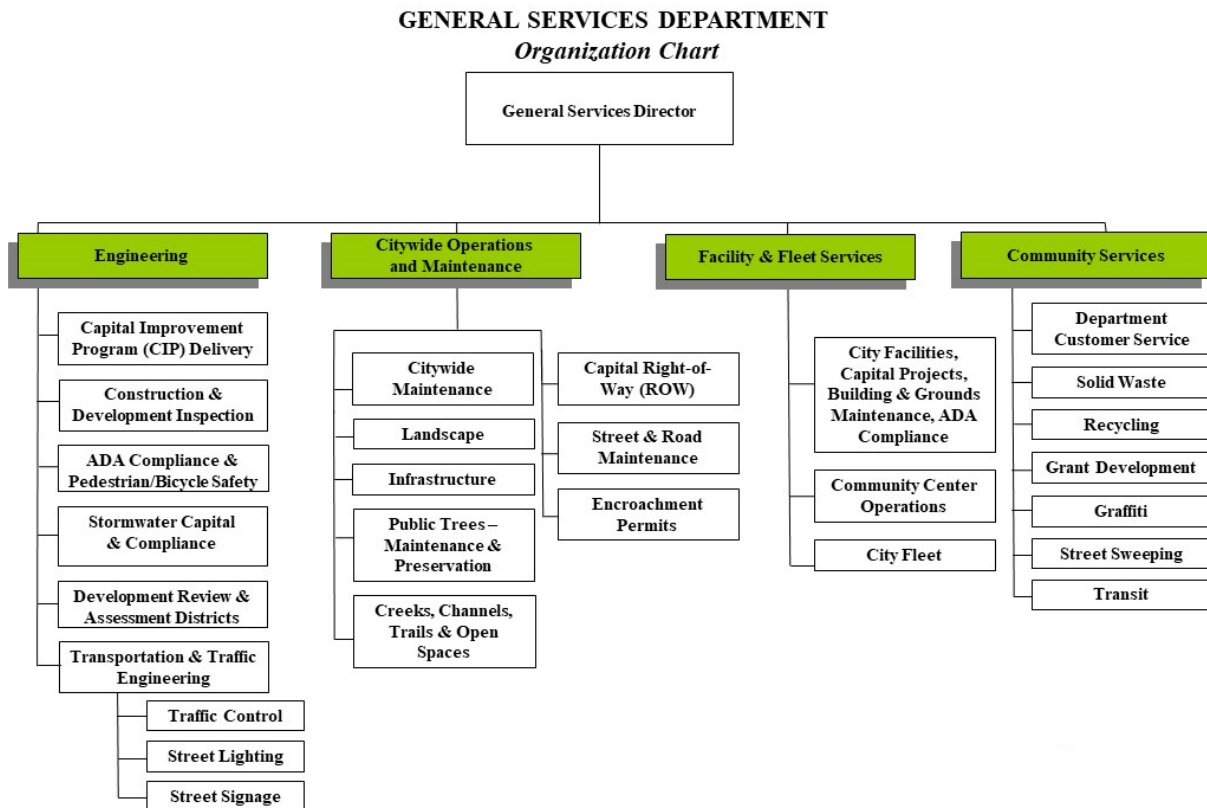
Facilities & Fleet Services Areas of Responsibility:

- ADA compliance of City facilities.
- City facilities capital projects planning/management.
- City facilities maintenance (buildings and grounds).
- City Fleet maintenance and administration.
- Energy efficiency and alternatives planning/implementation.
- Citrus Heights community centers administration/operations.
- City and community events.

Community Services Division Areas of Responsibility:

- Bicycle, pedestrian and mobility grant development.
- CalRecycle grants and funding program.
- Citywide integrated waste management planning and implementation.
- Commercial solid waste and recycling franchise agreements.
- Community outreach and engagement.
- Customer service administration.
- Graffiti abatement services.
- Grant development, support and administration.
- Residential solid waste and recycling services franchise agreement.
- State and regional recycling and diversion regulations compliance.
- Strategic planning.
- Representation/liaison on regional boards/committees.
- Transit services liaison and program administration.

The General Services Department budget is separated into ten areas based on funding sources: Administration and Engineering, Assessment District Administration, Community Center Management, Facility Management, Fleet Management, Solid Waste, Street Maintenance, Stormwater Utility Services, and Transit. A brief description of each area organized under the lead division within the department is provided below.



General Engineering/Public Works

Administration and Engineering provides administrative oversight for solid waste, transit, drainage administration, facility and fleet maintenance/purchasing, lighting district, and a full array of civil engineering services (e.g., development review, assessment district administration, CIP development and implementation, and traffic/transportation engineering). This division also provides liaison staff to the various transportation, drainage, and general public works related regional boards and associations. This division promotes and facilitates communications between residents and local businesses involved in and affected by construction projects. This division is also responsible for identifying and implementing ADA improvements within the City rights of way.

The Administration and Engineering budget reflects costs for a wide array of activities. Administration oversees the department budget, office operations, and manages contracts with service providers. Engineering is responsible for plan checks, development review, encroachment permits and inspections, development project review, and delivery of administration of the City's CIP, traffic/transportation engineering, and the Engineering Division general administration.

Funding Sources: *General Fund, Grant Funds, Local Transportation Funds*

ENGINEERING				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 265,195	\$ 341,131	\$ 258,602	\$ 266,367
Benefits	103,199	131,013	86,397	90,645
Services & Supplies	183,161	135,665	131,900	131,900
Total	\$ 551,555	\$ 607,809	\$ 476,899	\$ 488,912

Accomplishments FY 2019-20 and FY 2020-21

- Completed construction of the 2019 Residential Street Resurfacing.
- Completed construction of the 2020 Accessibility and Drainage Upgrades Project.
- Awarded final design contract for the Auburn Boulevard Complete Streets Project Phase 2.
- Secured \$2.86 million for Auburn Boulevard Complete Streets Project Phase 2 in Placer-Sacramento Gateway Corridor Plan in Cycle 2 of the state's Solutions for Congested Corridors Program (SCCP).
- Completed construction of the Highland Ave/Rinconada Dr. Drainage Improvements Project.
- Completed design for the Bonita Way Storm Drain Improvements.
- Adopted the Carriage/Lauppe Safe Schools Corridor Plan Reviewed.
- Adopted the Old Auburn Road Complete Streets Plan and submitted grant application for ATP to design and construct the first phase.
- Adopted the Multi-Modal Transportation Safety Program and began preparations to launch the program.
- Completed an aggressive community outreach program (including two community open houses), completed environmental documentation and 65% design plans for the Electric Greenway Project.
- Partnered with Sac County DOT to deliver a paving project to address large section of Madison Ave as well as Wachtel Way.
- Provided development review for multiple major commercial development projects.

- Provided development review for multiple major residential development projects, including Mitchell Village.
- Issued design contract for the Greenback Lane Complete Streets Project.
- Updated speed limits on 5 roadway segments and certified 6 others to align with current state regulations.
- Received grant and awarded consultant contract to develop a Local Road Safety Plan
- Completed construction of Mariposa Safe Routes to School Phase 3 project.
- Awarded construction contract for the Various Signalized Intersection Safety Improvement Project.
- Completed final design and right-of-way phase of the Mariposa Safe Routes to School Phase 4 project.

Citywide Operations and Maintenance

Assessment District Administration involves management and maintenance of properties within nine landscape maintenance assessment districts (LMADs) and one lighting assessment district located throughout the City. Maintenance work is accomplished utilizing contractor assistance.

- **Assessment District 97-01, Zone 1** – maintenance of open space and trees within a single-family subdivision, **Bridgemont/Van Maren**.
- **Assessment District 97-01, Zone 2** – maintenance of open space, trees and street frontage (corridor) landscaping within a single-family subdivision, **St Andrews Court**.
- **Assessment District 97-01, Zone 3** – maintenance of trees and street frontage landscaping within a single-family subdivision, **Verner Glen**.
- **Assessment District 97-01, Zone 4** – maintenance of trees and street frontage (corridor) landscaping within a single-family subdivision, **Oak Avenue/Village Creekridge**.
- **Assessment District 98-01** – maintenance of oak trees and street frontage landscaping within a single-family subdivision, **Stock Village**.
- **Assessment District 98-02, Zone 1** – maintenance of trees, street frontage (corridor) landscaping, and soundwall within a single-family subdivision, **Sorenson Ranch**.
- **Assessment District 98-02 Zone 2** – maintenance of trees, street frontage (corridor) landscaping, re-vegetation, drainage swales, and soundwall within one single-family subdivision, **Autumnwood**.
- **Assessment District 98-02 Zone 3** – maintenance of trees, street frontage (corridor) landscaping, drainage swales, soundwall, creek/open space, and pedestrian pathway lighting within a single-family subdivision, **Mariposa Creek**.
- **Assessment District 03-01, Zone 1** – maintenance of frontage, open space and pedestrian bridge within the Auburn Boulevard Commercial District, **Stock Ranch (commercial)**.
- **Assessment District 03-01, Zone 2** – maintenance of trees, street frontage landscaping, open space and a neighborhood park within single-family subdivision, **Stock Ranch (residential, SBA, City Hall)**.
- **Citrus Heights Lighting District** – operations and maintenance of the City's streetlights.

Funding Sources: Fees and Assessments; Gas Tax, Measure A Maintenance

Street Maintenance provides a variety of transportation infrastructure maintenance services including routine pavement maintenance, implementation of the City's Pavement Management System, maintenance of traffic signals and street signs, and coordination with other entities (e.g. special districts and utilities) as needed.

Funding Sources: Gas Tax including SB1/Road Maintenance Rehabilitation Account; Transportation Development Act and Measure A Maintenance.

Accomplishments FY 2019-20 and FY 2020-21

- Completed the updated Pavement Management Program, which conducted a full pavement analysis of all city-maintained streets, providing real time data on conditions, treatment options, and costs estimates.
- Awarded new contract for citywide landscape maintenance services.
- Provided citywide landscape and vegetation management services.
- Cleared approximately 138 acres of open space and 6 miles of creeks.
- Cleared approximately 5 miles of concrete lined channels.
- Provided citywide underground storm pipe maintenance services, helping to minimize localized flooding.
- Conducted over 200 arborist reviews of city-owned trees.

- Provided routine maintenance of over 1410 city-owned trees to ensure preservation of the City's tree canopy.
- Provided ongoing street sweeping services to meet National Pollutant Discharge Elimination System (NPDES) permit requirements.
- Launched new encroachment permit software to streamline permit process.
- Processed over 700 encroachment permits in 2020.
- Prepared and submitted of the Municipal Separate Storm Sewer System (MS4) Annual Report.
- Provided responsive repair and maintenance to the street light and traffic signal system.

Facilities & Fleet Services

Fleet Management includes management of the City's vehicle fleet, including all related administration of operations, fuel, maintenance, and replacement of all City vehicles.

The Fleet Management Division reflects costs for providing general maintenance, implementing preventive maintenance activities, overseeing vehicle purchases, monitoring service agreements, and promoting alternative energy vehicle services. This division actively promotes efficient and innovative management of the City fleet by exploring alternative fuel capacity, maintaining a pro-active preventive maintenance program, and conducting on-going research into clean air partnerships. The City fleet policy is based upon shared services and a "pool type philosophy" practice.

Funding Source: General Fund and Interfund Charges

FLEET MANAGEMENT				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 63,769	\$ 77,424	\$ 69,837	\$ 71,895
Benefits	21,187	29,926	21,460	22,552
Services & Supplies	40,659	67,500	69,525	71,611
Total	\$ 125,615	\$ 174,850	\$ 160,822	\$ 166,058

Facility Management involves strategic planning and implementation of the City's facility needs. This subdivision also manages the maintenance and construction of city buildings and facilities.

Funding Source: General Fund and Interfund Charges

FACILITY MANAGEMENT				
<u>City Hall</u>	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 129,281	\$ 147,748	\$ 137,883	\$ 141,995
Benefits	49,718	60,749	50,819	52,960
Services & Supplies	219,923	253,533	266,849	274,853
Total	\$ 398,922	\$ 462,030	\$ 455,551	\$ 469,808

FACILITY MANAGEMENT				
<u>Police Building</u>	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Services & Supplies	254,735	255,764	263,437	271,341
Total	254,735	\$ 255,764	\$ 263,437	\$ 271,341

Community Center Management includes costs for managing and maintaining the City's three community centers:

Citrus Heights Community Center: The City's 29,173 square foot state of the art LEED certified facility. Included are operating expenses necessary to provide residents with a rental facility capable of handling multiple functions on a daily basis as well as campus rentals for weddings and receptions. The facility operates 360 days per year and is staffed according to the needs of confirmed rental agreements.

Funding Source: User Fees and General Fund

COMMUNITY CENTER				
<u>Operations</u>	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Salaries	\$ 181,925	\$ 265,276	\$ 264,713	\$ 268,746
Benefits	72,547	63,563	59,781	61,550
Services & Supplies	260,408	225,244	308,173	315,131
Total	\$ 514,880	\$ 554,083	\$ 632,667	\$ 645,427

Citrus Heights Sylvan Community Center: Center management includes costs for managing and maintaining the 1,475 square foot facility. This facility was constructed to serve local community service groups, non-profits, and local residents at affordable rental rates. The Sylvan Community Center is capable of handling meetings, receptions, and events up to 50 people. The facility operates on a 365 day a year calendar and is staffed according to the needs of confirmed rental agreements.

Funding Source: General Fund

SYLVAN COMMUNITY CENTER				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Services & Supplies	14,801	16,270	18,447	19,002
Total	14,801	16,270	\$ 18,447	\$ 19,002

Citrus Heights Children and Youth Center: Center management includes costs for managing and maintaining the 2,200 square foot facility. The center, built on Sayonara Drive, holds up to 90 occupants and serves the neighborhood's youth after-school program. The building was constructed with solar panels, which cover the costs of the building's energy consumption.

Funding Source: General Fund

CHILDREN & YOUTH CENTER				
	2019-20	2020-21	2021-22	2022-23
	<u>Actual</u>	<u>Budget</u>	<u>Proposed</u>	<u>Proposed</u>
Services & Supplies	\$ 19,851	\$ 16,450	\$ 16,945	\$ 17,456
Total	\$ 19,851	\$ 16,450	\$ 16,945	\$ 17,456

Accomplishments FY 2019-20 and FY 2020-21

- Replaced Audio Visual System at the Community Center.
- Installed AV Components to provide new services to record and broadcast Webinars and Meetings/Trainings in the south rooms.
- Replaced failing security cameras at the Community Center.
- Administered the Great Plates Delivered Citrus Heights Program investing \$2.4 million into the local restaurant community.
- Repainted interior of Citrus Heights Community Center.
- Successfully designed and implemented new scheduling and invoicing Excel Program for the Community Center.
- Implemented updated fee schedule for all community centers.
- Opened Cooling Center with COVID guidelines in place.
- FY 2019-20, booked over 697 rentals at the Community Center.
- Maintained full staff of Facility Attendants.
- Created and maintained industry standard COVID custodial cleaning protocol for all facilities.
- Replaced Children & Youth Center east and west entrance doors.
- Replaced Sylvan Center north and south entrance doors.
- Repaired damaged automatic transfer switch at Police Department facility.
- Successful community events in 2019 – Sunday Funday, Hot August Bites and Holiday Tree lighting.

Community Services

Solid Waste involves management of residential solid waste/refuse collection services, recycling services, neighborhood clean-up services, commercial solid waste franchise agreements and state regulatory compliance, along with the household hazardous waste disposal contract. It also includes staff participation in the Regional Solid Waste Working Group and work with regional and state advisory, liaison, and technical committees.

Recycling grants and payment programs are also administered by this division. These grants and payment programs enable the City to provide public education and outreach, as well as install and maintain public recycling infrastructure throughout the City.

Funding Source: Solid Waste fees (Residential garbage and recycling service fees; commercial Franchise fees); Recycling grants and payment programs (CalRecycle).

Transit works in cooperation with the Sacramento Regional Transit District (SACRT) to address public transportation needs of Citrus Heights residents; this area also includes bus stop maintenance and capital improvement projects related to transit.

Transit services include bus services, SmaRT Ride (dial-a-ride) and para-transit services. The City is a member agency within the Sacramento Regional Transit District (SacRT). The City continues to work closely with SacRT on several program, system and infrastructure enhancements, including bus-stop improvements.

Funding Source Transportation Development Act Funds, Including Local Transportation Funds (LTF) and State Transit Assistance (STA) funds.

Accomplishments FY 2019-20 and FY 2020-2021

- Administered a \$50,000 Household Hazardous Waste (HHW) education grant.
- Included review of transit infrastructure and operational needs in development projects, including Auburn Boulevard Complete Streets, Greenback Lane Complete Streets, San Juan Avenue Complete Streets and Sunrise Tomorrow Specific Plan.
- Participated in the Sacramento Placer Gateway Project, resulting in Solutions for Congested Corridors grant funding to support Auburn Boulevard Complete Streets Project Phase 2 and long-term infrastructure planning for the region.
- Participated in the regional Public Works group to support streamlined regional funding through SACOG.
- Participated on grant application review team for SACOG regional funding program.
- Provided administrative and technical support services to all aspects of the department.
- Provided grant application support to transportation grant funding requests.
- Participated in planning and operations coordination for the Sacramento Regional Transit District (SacRT).
- Completed City ordinance updates adopted to ensure ongoing compliance related to unfunded state Mandatory Commercial Recycling (MCR) and Mandatory Organics Recycling (MORE) requirements.
- Entered into inspection services agreement with Sacramento County Environmental Management Department (EMD) to provide compliance assistance for unfunded state mandates related to commercial and organics recycling .





CITY OF
CITRUS
HEIGHTS

ENTERPRISE FUND

Enterprise Fund

Enterprise funds account for activities which operate as separate public enterprises. These funds receive revenues from fees charged to programs or other department users. The programs are largely cost-covering. The City has one enterprise fund, the Solid Waste Fund.

Solid Waste Fund: This fund accounts for revenues and expenses associated with garbage collection in the City. The City contracts solid waste services with Republic Services.

<u>Solid Waste</u>				
	2019-20	2020-21	2021-22	2022-23
	Actual	Budget	Proposed	Proposed
<u>Revenue</u>				
Fees & Charges	\$ 895,797	\$ 806,589	\$ 829,010	\$ 853,881
Total Revenue	\$ 895,797	\$ 806,589	\$ 829,010	\$ 853,881
<u>Use of Funds</u>				
Management & Support	\$ 283,587	\$ 215,707	\$ 222,179	\$ 228,845
General Services	392,262	643,027	788,714	819,139
Total Use of Funds	\$ 675,849	\$ 858,734	\$ 1,010,893	\$ 1,047,984





CITY OF
CITRUS
HEIGHTS

SPECIAL REVENUE FUNDS

Special Revenue Funds

Special Revenue Funds are used to account for the proceeds of specific revenue sources (other than expendable trusts and major capital projects) that are legally restricted to expenditures for specified purposes.

Transit Fund: This fund accounts for the revenue and expenses associated with the administration and maintenance of city-wide transit services in conjunction with the Sacramento Regional Transportation Authority (RT).

Stormwater Drainage Fund: This fund accounts for the revenues and expenses associated with the administration and coordination of stormwater drainage activities.

Street Maintenance: These funds account for Transportation Development Act (TDA), Gas Tax, SB1 (Road Repair and Accountability Act) and Measure A Street Maintenance funds. It represents the revenues and expenses associated with the maintenance and improvement of the City's roads and street landscaping.

Code Enforcement and Sacramento Abandoned Vehicle Program: These funds account for the revenues and expenses associated with code enforcement.

Grants and Housing (CDBG, Recycling Grants, Household Hazardous Waste Grants, Housing Grants, and other miscellaneous grants): These funds account for the revenues and expenses associated with specialized grant monies received by the City which may only be spent under specific guidelines.

Police Grants: These funds account for the revenues and expenses associated with police grant monies received by the City and may only be spent under specific guidelines.

Assessment Districts: These funds account for the revenues and expenses associated with the maintenance of ten city lighting and landscape assessment districts with the possible addition of two new landscape assessment districts in the fall of 2019. Lighting and landscape districts are created to finance common area landscape maintenance and lighting. Homeowners are assessed through their property tax bill.

Development Impact Fees (Transit, Roadway, Affordable Housing and Park Facilities): These funds account for the revenues and expenses associated with new development of infrastructure, affordable housing, and parks.

<u>Community Development</u>			
Special Revenue by Fund	FY 2020-21 Budget	FY 2021-22 Proposed	FY 2022-23 Proposed
Revenues			
Housing Grants	\$ 100	\$ 103	\$ 106
CDBG Entitlement	70,500	900,000	900,000
Total Revenues	\$ 70,600	\$ 900,103	\$ 900,106
Expenditures			
Housing Grants	\$ 171,078	\$ 236,355	\$ 78,621
CDBG Entitlement	646,084	1,703,977	1,348,266
Total Expenditures	\$ 817,162	\$ 1,940,332	\$ 1,426,887

<u>Police Department</u>			
Special Revenue by Fund	FY 2020-21 Budget	FY 2021-22 Proposed	FY 2022-23 Proposed
Revenues			
Code Enforcement	\$ 951,854	\$ 789,590	\$ 791,377
Police Special Revenue	82,671	83,471	84,295
Police Grants	85,142	334,072	243,642
SLES Funds	-	133,000	133,000
Total Revenues	\$ 1,119,667	\$ 1,340,133	\$ 1,252,314
Expenditures			
Code Enforcement	\$ 960,644	\$ 807,281	\$ 829,643
Police Special Revenue	98,254	127,190	130,322
Police Grants	87,492	258,261	230,912
SLES Funds	65,000	172,700	89,500
Total Expenditures	\$ 1,211,390	\$ 1,365,432	\$ 1,280,377

<u>General Services Department</u>			
Special Revenue by Fund	FY 2020-21 Budget	FY 2021-22 Proposed	FY 2022-23 Proposed
Revenues			
Gas Tax	2,297,300	2,278,399	2,312,574
RMRA-SB1	1,517,000	1,721,790	1,773,444
Stormwater	3,700,000	3,811,000	3,850,000
Measure A Road Maintenance	2,189,666	2,260,888	2,361,064
Transit	275,000	278,730	287,092
Transportation Development Act	7,200	74,160	76,385
Other Grants Special Revenue	35,000	46,050	37,132
Development Fees Special Revenue	215,165	260,252	353,496
Assessment District Special Revenue	641,425	651,239	651,671
Total Revenues	\$10,877,756	\$11,382,508	\$11,702,858
Expenditures			
Gas Tax	2,009,203	2,184,565	2,041,445
RMRA-SB1	1,750,000	2,600,000	2,100,000
Stormwater	5,057,174	5,551,230	4,665,709
Measure A Road Maintenance	2,148,974	1,749,578	1,808,681
Transit	345,066	283,192	303,673
Transportation Development Act	72,000	83,000	16,195
Other Grants Special Revenue	35,000	-	-
Development Fees Special Revenue	185,000	664,050	318,222
Assessment District Special Revenue	662,507	848,731	776,813
Total Expenditures	\$12,264,924	\$13,964,346	\$12,030,738



CITY OF
CITRUS
HEIGHTS

APPENDIX





CITY OF
CITRUS
HEIGHTS

BENEFIT OVERVIEW

Benefit Overview

As the policy board, the City Council approves all new, allocated positions and status (employee versus contract). In addition, the City Council reviews all staffing levels on an annual basis during the budget process. A position allocation system is the standard mechanism used by municipalities to track and monitor staffing levels and approvals.

Staffing Philosophy – The City Council desires to retain a mix of contract staff and employee staff in order to ensure a cost effective and flexible service delivery system. The City is committed to continual analysis of contract vs. employee staffing determinations. City employees create the oversight and administrative guidance necessary for contractual staff.

City Employee Benefits

Health Insurance

The City contributes a monthly amount towards health insurance for eligible employees. Health insurance is provided through the California Public Employees' Retirement System (CalPERS).

Retiree Medical Contribution

The City contracts with CalPERS for medical coverage. As part of this contract, employers are required to contribute towards retiree medical. The City's 2020 monthly contribution towards retiree medical is \$139.00. The 2021 contribution towards retiree medical is \$143.00 per month.

Dental Insurance

The City contracts with Delta Dental to provide dental insurance coverage for eligible employees and their families.

Vision Insurance

The City contracts with Vision Service Plan to provide vision insurance coverage for eligible employees and their families.

Flexible Spending Account

The City offers employees the ability to use pre-tax dollars for (1) reimbursement of eligible medical expenses not covered by the insurance programs and (2) dependent care. These two programs are called the "Medical Reimbursement Program" and the "Dependent Care Program".

Life Insurance

The City currently contracts with Prudential Insurance for life insurance benefits for eligible employees. The City pays the premium for basic life and accidental death and dismemberment (AD&D) – \$100,000 for management employees and \$50,000 for all other employees. **Age Reductions** – Under this plan, coverage reduces 35% at age 65 and 50% at age 70. Spouse or domestic partner coverage will be reduced as the employee ages, by 35% at age 65 and 50% at age 70.

Long Term Disability Insurance

The City contracts with Prudential Insurance to provide long term disability insurance for eligible employees.

Defined Benefit Retirement through CalPERS

The City has contracted with the California Public Employees' Retirement System (CalPERS) to provide retirement benefits for our employees. The CalPERS retirement program is a defined benefit program. The City has three different benefit tiers for miscellaneous members and three different benefit tiers for safety members as shown below. The specific benefit tier is determined by hire date and prior CalPERS membership.

Miscellaneous 2.7@55	Safety 3@50
Miscellaneous 2.0@55	Safety 3@55
Miscellaneous 2.0@62	Safety 2.7@57

The City provides the 4th level Survivor Benefits for all employees.

Deferred Compensation

Regular employees are eligible to participate in the City's designated 457 Deferred Compensation Plan at the start of employment. This program allows employees to save pre-tax dollars for their future retirement.

Annual Leave

The City's Annual Leave Program starts with 136 hours per year. The number of Annual Leave hours accrued each year is based on years of city service.

Long Term Medical Leave

The City's Long Term Medical Leave program provides 40 hours of leave per year.

Holidays

The City generally observes 12 holidays (96 hours) when the offices are closed. Two (2) floating holidays (16 hours) are also credited to eligible employees at the beginning of each calendar year. Holiday benefits are provided in different configurations based on the employee's shift schedule and assignment.

Bereavement Leave

Employees are eligible for 24 hours of Bereavement Leave due to the death of a family member per city policy.

California Family Rights Act (CFRA)

Eligible employees will receive up to 12 weeks of unpaid leave for covered events in a 12-month period.

Family Medical Leave Act (FMLA)

Eligible employees will receive up to 12 weeks of unpaid leave for covered events in a 12-month period.

Pregnancy Disability Leave (PDL)

Employees are eligible for pregnancy disability leave upon hire.

California Paid Family Leave (CPFL)

Employees may be eligible for six weeks of partial pay per year while taking time from work to care for a new child or sick family member. The payments are distributed by the Employment Development Department, a state agency, not the employer.

Enhanced Paid Leave Program (EPL)

The City has a six-week Enhanced Paid Leave program (wage continuation) for eligible employees for covered events.

Employee Assistance Program (EAP)

The City contracts with Managed Health Network (MHN) to provide an employee assistance program to city employees.

Tuition Reimbursement

New hires are eligible to participate in the Tuition Reimbursement Program after completing six-months of service with positive reviews.

Professional Development

The City encourages a variety of internal and external professional development training and opportunities for all levels of staff.

Personal Computer/Technology Purchase Program

The City offers a computer loan program to eligible personnel.

Alternative Work Schedule

The City offers alternative work schedules depending on job assignment.

Management Benefits

The City provides the following benefits to management personnel:

- 80 hours of Management Leave to Management staff.
- 120 hours of Executive Management Leave to Executive staff.
- \$100,000 fully paid life insurance program.
- Up to a 3% match on deferred compensation contribution for Management staff.
- A 5% match to a 401(a) retirement plan for Executive staff.

Uniforms

The City of Citrus Heights provides uniforms and uniform maintenance for positions required to wear uniforms at no cost to the employee. For eligible employees, the Uniform Benefit amount reported to CalPERS is determined by the employee's classification per the following table:

GROUP	ANNUAL	BI-WEEKLY
Sworn – Officers	806.00	31.00
Sworn – Command	806.00	31.00
CSI/CSO/NEO	494.00	19.00
Records/Dispatch	104.00	4.00
Animal Services Officers	390.00	15.00
Maintenance Worker I/II, Senior Maintenance Worker, Construction/Maintenance Inspector I/II, Construction/Maintenance Inspection Supervisor	598.00	23.00
Building Inspector I/II, Senior Building Inspector	598.00	23.00
Fleet Technician	286.00	11.00

Police Specific Benefits

There are four benefits provided to police specific personnel:

1. Educational Incentive Pay

Designated sworn personnel are eligible for a maximum of 10% education/certificate incentive pay. Designated sworn personnel receive an additional 5% for a Master's Degree.

Communication Center Personnel are eligible for a maximum of 5% education/certificate incentive pay.

2. Bilingual Pay

Designated personnel are eligible to receive \$100 per month upon certification of proficiency in either Spanish, Slavic languages, or American Sign Language.

3. Special Assignment Pay

The City offers up to a 5% Special Assignment Pay for eligible sworn personnel.

4. Professional Attire Pay

Professional Attire Pay of \$850 per year may be provided based on assignment.

Police Specific Recruitment Incentives

The City offers recruitment incentives for Police Department specific positions.





CITY OF
CITRUS
HEIGHTS

GOALS FOR FY 2021-22 AND FY 2022-23

Goals for FY 2021-22 & FY 2022-23



City Manager

City Manager

- Explore alternative revenue sources for city operating and capital costs.
- Research, analyze, and develop recommendations for utilization of American Rescue Act federal stimulus funding.
- Continue to develop initiatives to support and revitalize the sectors of the Citrus Heights economy heavily impacted by COVID-19.

City Attorney

- Minimize risk of litigation with proactive training while standing up for City's interests in the right cases.
- Proactively monitor and advise on quality of life issues for the City, including code enforcement, new land use requirements, and COVID-related laws and orders.
- Handle difficult code enforcement cases with receiverships (which are paid for by the property owner).

City Clerk

- Provide training for Electronic Document Management System for city personnel.
- Continue to complete back scanning of all closed/permanent city contracts and service agreements.
- Serve as the filing official for all Fair Political Practice filings.
- Oversee the redistricting process following the 2020 Census and provide redistricting and elections educational material.
- Administer the November 2022 General Municipal Election.
- Oversee the recruitment process for new board and commission members.
- Continue to oversee and maintain the Records Retention Program, the City's Municipal Code, and the City Council Procedures Manual.

Economic Development/Communications

- Market and steward Sunrise Mall specific plan and vision toward execution by working with owners and developers on potential development opportunities.
- Work with Citrus Heights Chamber of Commerce, Sunrise Marketplace and local business associations to retain and attract businesses.
- Continue the City's Business Visitation program.
- Effectively leverage city communication channels to inform & engage residents and increase community pride.

Administrative Services Department

Information Technology

- Expand services for the new Electronic Document Management System (EDMS).
- Continue expansions to Permit and GIS feature/use.
- Continue to migrate computers from Windows 7 to Windows 10.
- Support Work from Home needs due to COVID19.
- Continue to replace personnel computers, monitors, and printers as needed.
- Server replacements: either extending warranties or replace hardware.

Finance

- Explore ability to offer additional services with on-line payment options.
- Prepare mid-year and mid-cycle budget updates to City Council.
- Utilize the City's electronic financial system to closely monitor FY 2021-22 & FY 2022-23 budgets.
- Administer secured short-term financing to bridge gap prior to receiving property tax in 2022-23.

Human Resources

- Implement city-wide Coaching & Performance Program.
- Implement Compensation Step System.
- Coordinate Succession Plan.
- Release Request for Proposal for benefits administrator.

Risk Management

- Review and update City Personnel Rules & Regulations.
- Review and update City Administrative policies.
- Coordinate city-wide mandated training.
- Continue to monitor legislation affecting the City.

Police Department

Office of the Chief

- Update the Police Department's Strategic Plan.
- Provide quality policing services to the community with reduced staffing.
- Leverage technology, such as sUAS and bait, to augment police services.
- Continue to ensure the safety of our employees and our community throughout the ongoing pandemic

Operations

- Provide increased quality training to our Patrol Officers and reinstating our robust training program to include tactical scenario based training with the use of force simulator.
- Develop creative strategies to utilize volunteer staff to assist in the essential functions of Patrol, and nuisance related traffic problems.
- Develop and train an auxiliary team to help and or enhance our capabilities with call taking in our communications center.

Special Services

- Transition Unified Crime Reporting (UCR) to a National Incident Based Reporting System (NIBRS).
- Implement program to gather traffic stop data compliant with the Racial Identity and Profiling Act (RIPA).
- Leverage grant funding and collaborative regional partnerships to maintain effective domestic violence, sex assault, and child exploitation investigations.
- Evaluate Fleet options for replacement patrol vehicles, motorcycles, and mobile data computers.
- Develop creative strategies to bring back volunteer staff to assist in the essential functions of Patrol, Nuisance related vehicle and public blight when COVID restrictions allow.



Community Development Department

Building and Safety Division

- Continue to improve on our online permitting and electronic plan review processes to increase efficiency and become 100% paperless.
- Expand remote video inspection program.
- Upgrade inspection request process to be 100% electronic and remote.
- Provide training for Building staff as needed to obtain education credits in order to maintain professional certifications.

Housing and Grants Division

- Facilitate completion of the Sunrise Point Affordable Housing development, which will contribute 23 units toward the City's replacement housing obligation on Sayonara Drive.
- Complete the Sayonara Drive replacement housing project.
- Continue to support non-profits who provide public services to the Citrus Heights community.
- Issue 10-14 housing repair loans/accessibility grants to low income households.
- Continue to collaborate with Sacramento Steps Forward/partners to improve the performance and outcomes of the region's homeless response system.

Planning Division

- Adopt Sunrise Tomorrow Specific Plan and EIR and assist in its implementation.
- Implement new permit tracking software that will allow for online submittals.
- Process the entitlements for Sylvan Corners and Birdcage properties.
- Continue monitoring of Mitchell Village subdivision to ensure all mitigation measures are met.

General Services Department

Citywide Operations and Maintenance

- Organize and lead up to 3 community volunteer clean up (landscape, trees, vegetation) events annually.
- Continue to provide creek and vegetation maintenance services citywide, with a goal of completing up to 8500 lf of creek clearing.
- Continue to maintain and enhance citywide tree canopy, focusing on preservation of mature species.
- Complete the LED conversion of 500 streetlights.

Community Services

- Provide education and outreach to local businesses regarding organics recycling, edible food recovery opportunities, and commercial recycling compliance.
- Conduct residential recycling and organics compliance assessments.
- Finalize Organics Compliance Plan for Senate Bill 1383 (mandatory organics collection for residential customers and related requirements) and begin implementation.
- Provide citywide grant application and implementation support.
- Work with Sacramento Regional Transit to facilitate improvements to transit infrastructure and amenities.
- Partner with SacRT to educate residents about transit services available in the City and throughout the region.

Engineering

- Launch Multi Modal Transportation Safety Program and implement up to five top priority level countermeasures.
- Complete Final Design for the Auburn Boulevard Complete Streets Project Phase 2 and initiate construction.
- Complete construction of the Various Signalized Safety Improvements Project.
- Complete construction on the 2020 and 2021 Accessibility and Drainage Improvement Projects.
- Complete construction of the 2021 Residential Resurfacing Project.
- Complete design and construct the Greenback Lane Complete Streets Project.
- Implement updated permitting procedures for Encroachments, Street Use, Transportation and Development permits.
- Utilize the City's updated Pavement Maintenance System to select and evaluate pavement projects.
- Complete construction of the Bonita Way Drainage Improvements.
- Complete construction of the Mariposa Ave Safe Routes to School Phase 4.
- Award professional services agreement for the San Juan Avenue Complete Streets Project Phase 1.

Facility & Fleet Services

- Develop and launch new event staffing model for the Community Center.
- Plan and implement programming for the Senior Center at the Community Center.
- Engage the community to increase volunteer base at the Community Center.
- Post COVID redefine the marketing plan for Community Center to increase revenue/bookings.
- Market the new in-house webinar and recording features of the South Rooms at the Community Center.
- Perform in-house security infrastructure maintenance.
- Provide preventive maintenance, administration, fuel services, and safety checks for all city-owned fleet including replacement planning for alternative vehicles.
- Continue to maintain all city-owned facilities effectively and efficiently emphasizing cost savings and efficiency and longevity.

A stylized tree graphic with a dark green canopy and a brown trunk and roots. A horizontal band, consisting of a black section on the left and a grey section on the right, passes through the middle of the tree.

CITY OF
CITRUS
HEIGHTS

GLOSSARY

Glossary of Budget Terminology

Appropriation: An authorization by City Council to make expenditures and incur obligations for a specific purpose within a specific timeframe.

Assessed Valuation: A dollar value placed on real estate or other property by Sacramento County as a basis for levying property taxes.

Assessment District: Defines area of land that is benefited by the acquisition, construction, or maintenance of a public improvement. An assessment is levied and collected in the regular property tax bill to fund the improvements.

Audit: Scrutiny of the City's accounts by an independent auditing firm that determines whether the City's financial statements are fairly presented in conformity with generally accepted accounting principles. An independent audit is performed annually.

Beginning Balance: Unencumbered resources available in a fund from the prior fiscal year after payment of prior fiscal year expenditures.

Bond: Capital raised by issuing a written promise to pay a specified sum of money, called the face value or principal amount, with interest at predetermined intervals.

Budget: A fiscal plan of financial operation, listing an estimate of proposed applications or expenditures and the proposed means of financing them.

Capital Improvement Program (CIP): The five-year financial plan for improving assets and integrating debt service and capital assets maintenance, adopted in a separate budget document and updated annually.

Community Development Block Grant (CDBG): a program designed by the U.S. Department of Housing and Urban Development to revitalize low-and moderate-income areas within a city.

Deficit: An excess of expenditures over revenues (resources).

Department: An organizational unit comprised of divisions of functions. It is the basic unit of service responsibility encompassing a broad mandate or related activities.

Expenditure: The actual payment for goods and services.

Fiscal Year (FY): The period designated by the City for the beginning and ending of financial transactions. The City of Citrus Heights fiscal year begins July 1st and ends June 30th of each year.

Gas Tax: Administered by the State Board of Equalization, this is an 18 cents per gallon tax on fuel used to propel a motor vehicle or aircraft. Use of the revenue is for research, planning construction, improvement, maintenance, and operation of public streets and highways, or public mass transit.

General Fund: The fund used to account for all financial resources except those required to be accounted for in another fund.

General Plan: California State law requires each city to adopt a General Plan that describes the direction the city will take concerning its future development.

Healthy Cities Grant: A State grant received by the City to help in its efforts to enhance neighborhood livability in Citrus Heights by the involvement of residents and businesses in clean-up and beautification programs and efforts.

Infrastructure: Facilities on which the continuance and growth of the community depend on, such as roads, water lines, sewers, public buildings, etc.

Measure A: This funding source was approved by Sacramento County voters in the form of a ½ cent sales tax in the late 1980s to fund specific transportation projects. When Citrus Heights incorporated, Measure A funds programmed for County transportation projects within Citrus Heights became available to the City as a funding source for those projects previously under County jurisdiction.

Motor Vehicle-in-Lieu: A State vehicle fee imposed on motorists for the privilege of operating a motor vehicle on the public highways. It is imposed “in-lieu” of a local property tax.

Property Tax: Property tax is imposed on real property (land and permanently attached improvements such as a building) and tangible personal property located within the City. The tax compromises one percent of the assessed value of the property.

Residents’ Empowerment Association of Citrus Heights (REACH): A coalition of Citrus Heights Neighbors working together.

Redevelopment Property Tax Trust Fund: As of February 1, 2012 the Community Redevelopment Agency of the City of Citrus Heights has been dissolved. The City of Citrus Heights as Successor Agency and its Oversight Board are responsible for the wind down of the agency. Property tax revenue remitted from the County into the Redevelopment Property Tax Trust Fund will be used to pay off obligations and complete projects that have enforceable obligations and are listed on the Recognized Obligation Payment Schedule.

Reserved Fund Balance: Accounts used to record a portion of the fund balance legally segregated for a specific use and not available for appropriation.

Resolution: A special order of the City Council which has a lower legal standing than an ordinance.

Revenue: Annual income received by the City.

Revenue Neutrality: The Cortese-Knox local Government Reorganization Act (“The Cortese-Knox Act”), which establishes procedures for changes in local government organizations, including city incorporation, was amended by the State Legislature in 1992.

The 1992 Amendment was the “revenue neutrality” statute (Government Code 56845). Revenue neutrality addresses the process and outcome of how revenues and expenditures will be allocated to the

newly incorporated agency (the new city) and the larger agency that the incorporated area came from (the county). For more information on revenue neutrality, refer to Government Code Section 56842.

Request for Proposal for Services (RFP): A document that solicits proposal, often made through a bidding process by an agency or company interested in procurement of a commodity, service, or valuable asset, to potential suppliers to submit business proposals.

Risk Management: The process for conserving the earning power and assets of the City by minimizing the financial effect of accidental losses.

Salaries and Benefits: A budget category which generally accounts for full-time and temporary employees, overtime, and all employee benefits, such as medical, dental, and retirement.

Sacramento Regional County Solid Waste Authority (SWA): SWA is a joint powers authority that oversees commercial waste management in the City of Sacramento and the unincorporated areas of Sacramento County.

Taxes: Compulsory charges levied by a government for the purpose of financing service performed for the common benefit.

Transient Occupancy Tax (TOT): A tax imposed on travelers who stay in temporary lodging facilities within the City. Also referred to as a Hotel/Motel Tax.

A stylized blue tree logo with a thick trunk and a dense, rounded canopy of leaves. The tree is centered on the page.

CITY OF
CITRUS
HEIGHTS

FIVE YEAR CAPITAL IMPROVEMENT
PROGRAM

Five Year Capital Improvement Program

The complete Five Year Capital Improvement Program (CIP) is scheduled to be presented to the Planning Commission on April 28th and to the City Council on May 13, 2021. A draft listing of CIP projects is presented on the following page.

PROJECT	Primary Funding Sources	FY 2021/2022	FY 2022/2023	FY 2023/2024	FY 2024/2025	FY 2025/2026
Advanced Traffic Management (annual)	Measure A Traffic Control/Safety	\$ 50,000.00	\$ 50,000.00	\$ 50,000.00	\$ 50,000.00	\$ 50,000.00
Amsell Court Storm Drain Improvements (2-year project)	Stormwater Utility			\$ 400,000.00		
Annual ADA Accessibility and Drainage Improvements (annual, pending CDBG allocation approval)	CDBG, Stormwater Utility					
Antelope Road-I/80 Safety Improvements Project (multi-year project, future funds tbd)	STA, Future Funds TBD	\$ 568,000.00	\$ 568,000.00			
Arcade-Cripple Creek Trail Project (formerly Electric Greenway; multi-year project)	ATP Grant, Stormwater Utility, Measure A, Per Capita funds	\$ 1,784,500.00	\$ 4,160,391.00			
Auburn Boulevard Complete Street Revitalization (estimated expenditures based on anticipated full funding; phased work)	Multiple SACOG Grants, STA Smart Growth Matching Funds, Placer Gateway Corridor Grant (CTC), City LOC, Stormwater Utility	\$ 700,000.00	\$ 5,460,000.00	\$ 10,531,000.00	\$ 6,144,000.00	
Blayden Court Storm Drain Improvements (2 year project)	Stormwater Utility	\$ 600,000.00	\$ 600,000.00			
Bonita-Old Auburn Storm Drain Improvements (1 year project)	Stormwater Utility	\$ 1,600,000.00				
Chula Vista Storm Drain Improvements (1 year project)	Stormwater Utility			\$ 800,000.00		
Greenback Lane Complete Streets (1 year project)	Measure A Capital and Traffic Safety, SB1, Roadway Impact, Stormwater Utility, Transit Impact	\$ 1,700,000.00				
Mariposa Avenue Safe Routes to School Ph. 4 (1 year project)	SACOG ATP Grant, Measure A Bike/Ped	\$ 2,011,750.00				
Mariposa-Sylvan Valley Storm Drain Improvements (1 year project)	Stormwater Utility					\$ 400,000.00
Minnesota, Anderson and Canady Infiltration Basins and Storm Drain Improvements (2 year project)	Stormwater Utility			\$ 900,000.00	\$ 900,000.00	
Multi-Modal Transportation Safety Improvements Project Implementation (annual improvements)	Measure A Traffic Control/Safety, and Ped Safety, Reflex Funds	\$ 60,000.00	\$ 60,000.00	\$ 80,000.00	\$ 80,000.00	\$ 80,000.00
Neighborhood Creek Bank and Riparian Corridor Restoration (annual improvements)	Stormwater Utility	\$ 100,000.00	\$ 100,000.00	\$ 100,000.00	\$ 100,000.00	\$ 100,000.00
Neighborhood Areas Drainage Master Plans (non-infrastructure)	Stormwater Utility	\$ 250,000.00				
Old Auburn Complete Street Project Ph 1 (multi-year design/row project)	SACOG ATP Grant, Measure A Traffic Safety	\$ 350,000.00	\$ 350,000.00	\$ 1,213,000.00	\$ 517,000.00	
Pavement Restoration Program (includes annual Residential Resurfacing, and contribution to County Shared Projects for Madison and Wachtel Segments)	SB1, Measure A Maint, Gas Tax	\$ 2,600,000.00	\$ 2,500,000.00			
San Juan Complete Streets (3 year project - design to construct)	State of Good Repair Grant, Measure A Bike/Ped and Traffic Safety, SB1, Stormwater Utility, and TDA Bike/Ped	\$ 267,000.00	\$ 1,250,000.00	\$ 2,872,000.00		
Storm Drain Pipe Rehabilitation Program (annual improvements)	Stormwater Utility	\$ 150,000.00	\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 250,000.00
Sunrise Blvd Complete Streets (multi-year project, future funds tbd)	STA, Future Funds TBD			\$ 300,000.00		
Various Signalized Intersection Safety Improvements (1 year project)	CDBG, HSIP Grant, Measure A Bike/Ped and Traffic Safety, Stormwater Utility, TDA Bike/Ped and Transit Impact	\$ 1,201,521.00				
Wonder Street Storm Drain Improvements (1 year project)	Stormwater Utility					\$ 600,000.00
	Totals:	\$ 13,992,771.00	\$ 15,348,391.00	\$ 17,796,000.00	\$ 8,041,000.00	\$ 1,480,000.00



GREATER SACRAMENTO
ECONOMIC COUNCIL

February 23, 2021

Christopher W. Boyd
City Manager
City of Citrus Heights
6360 Fountain Square Drive
Citrus Heights, CA 95621

RE: Fiscal Year 2021/2022 Annual Commitment Estimate

Dear Christopher,

I am inspired by the rapid evolution of our region and the organization. Over the past six years, GSEC has been a catalyst in economic development, forming a true public-private partnership in California and setting the standard for measuring economic development success.

To date, our public sector members contributed over \$5.5 million to help grow the local economy. Leveraging your investment with our private sector investors who have renewed for another five years, together we achieved the following in the region:

- **16,098 jobs created**
- **Produced economic output of \$6.54B**
- **Added \$79.4M to the local tax benefits**
- **Return on Investment for community: \$155.12**

Greater Sacramento is here to be a resource for you – to provide the information, tools and platform to make our region successful. During the past year we have added a regional business retention and expansion program. In partnership with your economic development team's effort, we will visit with 100 companies throughout the region. We are looking to expand this partnership next year. Additionally, we are looking forward to adding the following efforts over the next three years to ensure an expedited recovery as we put the economic impacts of the pandemic behind us:

- Continue to grow our **business retention and expansion** efforts so that our existing tradable sector businesses are supported and continue to choose our region as a place to invest
- Launch a **foreign direct investment (FDI)** initiative that will bring in capital and companies from outside country. This will add another channel to grow our economy and as California remains the top location for this type of investment and this effort provides a global opportunity to grow our economy.
- Greater Sacramento is completing its pilot project "Digital Upskill Sacramento" that has provided digital skills to some of our most underserved and COVID19 impact members of the community. This **workforce initiative** program has provided an opportunity for upward social mobility that not only will change the trajectory of the economy but the trajectory of these learners for their lifetime.

The Greater Sacramento Economic Council is committed to ensuring that population estimates, and contribution per capita rates remain unchanged for the upcoming fiscal year. **To assist you in budget preparation and planning for Fiscal Year 2021/2022, your annual contribution will remain \$35,238 even while our commitment and added services continue to grow.**



GREATER SACRAMENTO
ECONOMIC COUNCIL

The support of our communities is paramount to our success. Only through a true public-private partnership will we achieve the results this region deserves. We are looking forward to your involvement and focusing on economic recovery.

Sincerely,

Barry Broome
President & CEO

CITY OF CITRUS HEIGHTS

CITY COUNCIL STAFF REPORT MEMORANDUM

DATE: April 22, 2021

TO: Mayor and City Council Members
Christopher W. Boyd, City Manager

FROM: Colleen McDuffee, Community Development Director
Casey Kempenaar, Planning Manager
Leslie Blomquist, City Engineer

SUBJECT: Transportation Impact Study and SB743 Implementation Guidelines

Summary and Recommendation

The city is required to comply with the California Environmental Quality Act (CEQA) for all actions considered by the city, including land development projects. State law, court cases, and changes in technology often require local agencies to adopt standards, update processes, and modify how they review projects under CEQA.

Recent changes have resulted in the need to update the city's methodology and requirements to evaluate impacts to the city's transportation network. These changes, largely in response to Senate Bill 743 (SB743), necessitate the development of SB743 Implementation Guidelines and update of the city's Transportation Impact Study (TIS) Guidelines to ensure development review is consistent with both SB743 and CEQA.

SB743 has transformed the way local government reviews transportation related impacts as part of CEQA. The city is required to adopt standards and modify its processes to comply with the law. This staff report is intended to provide a broad overview of the law and the proposed guidelines in response to the law. Due to the inherent complexities of the law and associated analysis a comprehensive presentation will be presented to the City Council at the City Council meeting.

Staff recommends the City Council follow the Planning Commission's recommendation and take the following actions:

1. Review the Transportation Impact Study Guidelines and SB743 Guidelines;
2. Adopt Resolution 2021-__ a Resolution of the City Council of the City of Citrus Heights, adopting the Transportation Impact Study and SB743 Guidelines; finding that the Thresholds of Significance have been promulgated pursuant to the California Environmental Quality Act (CEQA) Guidelines 15064.7; and determining the adoption not a "project" pursuant to CEQA Guidelines Sections 15060(c)(3) and 15378

Fiscal Impact

There is no fiscal impact associated with this action. The preparation of the TIS and SB743 Guidelines was funded by the Local Early Action Planning (LEAP) grant funded by California Housing and Community Development. The adoption of the guidelines will streamline development review, reducing delays and costs for development consistent with the proposed guidelines.

Background and Analysis

In 2013, the State of California passed SB743, which altered how transportation impacts from new development are measured under the CEQA. Traditionally, transportation impacts have been assessed in terms of Level of Service (LOS), a measure of automobile delay along roadway segments and at key intersections. SB743 shifts from an LOS methodology to metrics aligned with state goals around greenhouse gas reduction, land use diversity, and development of multi-modal travel networks.

The law stipulated that starting on July 1, 2020, automobile delay and LOS may no longer be used as the performance measure to determine the transportation impacts of land development projects under CEQA. Instead, an alternative metric that supports the goals of the SB743 legislation is required.

Although there is no requirement to use any particular metric, the use of Vehicle Miles Traveled (VMT) has been recommended by the Governor's Office of Planning and Research (OPR). This requirement does not modify the discretion lead agencies have to develop their own methodologies or guidelines, or to analyze impacts to other components of the transportation system, such as walking, bicycling, transit, and safety.

General Plan Overview

In 2011, the city adopted an updated General Plan, which included a focus on improved mobility and renewed focus on evaluation of impacts to the transportation network. Notably, General Plan Goal 29 states:

Plan, design, construct, and manage a Complete Streets transportation network that accommodates the needs of all mobility types, users and ability levels

Several related Policies were included to implement Goal 29 (Attachment B). In particular, Policy 29.2 recognized the conflict between achieving high Levels of Service (LOS) and the potential impact requisite road widening to maintain LOS would have on the environment, safety, and associated impacts. Policy 29.2 recognized that widening the city's major corridors would negatively impact the community; therefore, exceptions to the LOS standards are allowed under the General Plan for projects impacting the following five corridors:

- Sunrise Boulevard – south City limits to north City limits
- Greenback Lane – west City limits to east City limits
- Old Auburn Road – Sylvan Road to Fair Oaks Boulevard
- Antelope Road – I-80 to Auburn Boulevard
- Auburn Boulevard – Old Auburn Road to northern City limits

Transportation Impact Study Guidelines

Policy 29.2 included an action to require the development of TIS Guidelines to provide guidance when evaluating the potential impact of projects in the city. In 2013, the city adopted these standards, which have not been updated since.

Exhibit 1A (clean version) and Attachment A (redline/strikeout) identify the proposed changes to the 2013 TIS Guidelines. These changes are necessary to reflect changes in laws (including SB743), reflect changes in the traffic analysis practice, and to reflect changing conditions in the city.

No changes are proposed to the General Plan; LOS standards will continue to apply for all future projects. General Plan Policy 29.2 will continue to apply to the city's General Plan and associated review; however, consistent with SB743, LOS analysis is no longer considered as part of the CEQA process.

SB743 Guidelines

The Draft SB743 Implementation Guidelines are included as Exhibit 1B. The guidelines were prepared to provide parameters for transportation engineers and planners to conduct CEQA transportation analyses for land development and transportation projects in compliance with SB743. The intent of the guidelines is to:

1. Provide an overview of SB743
2. Provide technical guidance for VMT Analysis
3. Adopt Thresholds of Significance for VMT
4. Identify streamlining opportunities when VMT analysis is not required
5. Provide case studies of VMT analysis
6. Identify mitigation parameters for projects that exceed VMT Thresholds of Significance

The SB 743 Implementation Guidelines outline the technical process and background about VMT in Citrus Heights as well as outline the thresholds of significance when reviewing projects under CEQA. The Guidelines also specify smaller projects are exempt from VMT analysis. These thresholds are described in more detail in Exhibit 1B.

The Implementation Guidelines are intended to implement SB743 and related guidance from OPR. It is important to note, not every project will require a VMT analysis to be performed. First, certain projects are not required to conduct a VMT analysis based on their small size or low VMT generating characteristics. These projects include:

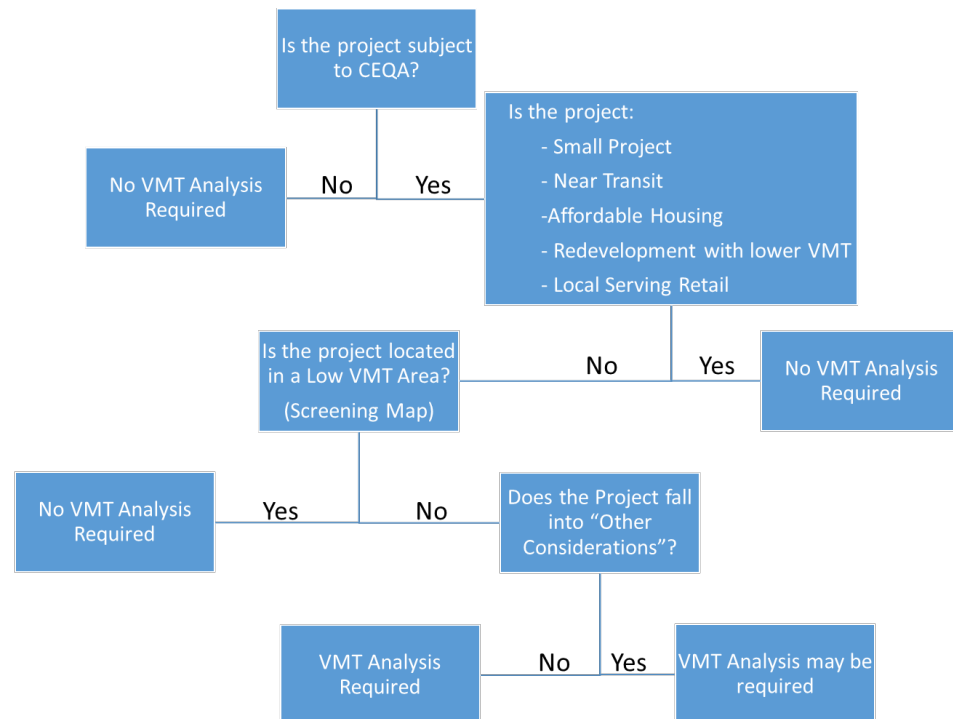
1. Small Projects
 - a. 19 Single Family Homes or less
 - b. 32 Multifamily Homes or less
 - c. 14,000 SF of Office or less
2. Affordable Housing
3. Projects near High Quality Transit
4. Redevelopment Projects resulting in reduction in VMT
5. Local Serving Retail (Under 50,000 SF)

In addition to the aforementioned exempt projects, projects located within predefined low VMT generating areas are not required to conduct a VMT analysis. The Guidelines include screening maps for each land use typology (residential, employment, and visitor) identifying the

predetermined low VMT areas (areas that generate 15% less VMT compared to the regional average). These screening maps identify the portions of the city that are considered low VMT areas and areas with high quality transit corridors (as outlined in the OPR guidance). These screening maps reduce or eliminate the need for VMT analysis under SB743 for projects that occur in these areas, which encompass a large portion of the city.

As described above, a variety of criteria apply to determine if a project is subject to a VMT analysis under SB743. Figure 1 illustrates the process to determine if projects are subject to VMT analysis.

Figure 1 – SB743 Guidelines Flowchart



Planning Commission Review

On March 24, 2021, the Planning Commission reviewed the Draft Guidelines and had minor comments and questions. After the presentation, the Planning Commission unanimously recommended the City Council review and adopt the TIS and SB743 Implementation Guidelines.

Environmental Review

The TIS and SB743 Implementation Guidelines are intended to be utilized to conduct environmental analysis under CEQA and SB743. The TIS Guidelines identify the thresholds triggering a TIS analysis and outline the methodology necessary to prepare Transportation Impact Studies when required. The proposed changes to the TIS Guidelines are necessary to comply with state law and the state of the Transportation Engineering practice.

The SB743 Implementation Guidelines include thresholds triggering VMT analysis and outline the requisite methodology to conduct VMT analysis consistent with SB743. The guidelines also

include CEQA thresholds of significance for VMT impacts, consistent with SB743 as well as the Technical Advisory released by OPR.

The adoption of CEQA thresholds is not considered a project per CEQA Guidelines Section 15060(c)(3) and 15378; no further environmental review is required.

Exhibits

1. Resolution 2021- ____ A Resolution of the City Council of the City of Citrus Heights, adopting the Transportation Impact Study and SB743 Implementation Guidelines; finding that the thresholds of significance have been promulgated pursuant to the California Environmental Quality Act (CEQA) Guidelines 15064.7; and determining the adoption not a "project" pursuant to CEQA Guidelines sections 15060(C)(3) and 15378
 - 1A. Draft Transportation Impact Study Guidelines (Clean Copy)
 - 1B. Draft SB743 Implementation Guidelines

Attachments

- A. Draft Transportation Impact Study Guidelines (Redline/Strikeout Copy)
- B. General Plan Goal 29

RESOLUTION NO. 2021 - ____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CITRUS HEIGHTS,
ADOPTING THE TRANSPORTATION IMPACT STUDY AND SB743
IMPLEMENTATION GUIDELINES; FINDING THAT THE THRESHOLDS OF
SIGNIFICANCE HAVE BEEN PROMULGATED PURSUANT TO THE CALIFORNIA
ENVIRONMENTAL QUALITY ACT (CEQA) GUIDELINES 15064.7; AND
DETERMINING THE ADOPTION NOT A "PROJECT" PURSUANT TO CEQA
GUIDELINES SECTIONS 15060(C)(3) AND 15378**

WHEREAS, in 2011, the City Council of the City of Citrus Heights adopted a focused General Plan Update and Environmental Impact Report;

WHEREAS, the 2011 General Plan included Goal 29.2, which states “Plan, design, construct, and manage a Complete Streets transportation network that accommodates the needs of all mobility types, users and ability levels”;

WHEREAS, General Plan Policy 29.2 called for the adoption of Transportation Impact Study Guidelines to guide the development of requisite Transportation Impact Studies;

WHEREAS, in 2013, the first Transportation Impact Study Guidelines were developed for the City of Citrus Heights;

WHEREAS, in 2013, the California Legislature enacted SB743 altering how transportation impacts were considered by local agencies under the California Environmental Quality Act (CEQA);

WHEREAS, SB743 stipulated starting July 1, 2020, vehicle delay (level of service) would no longer be considered an environmental impact under CEQA;

WHEREAS, from time to time the City is required to update the Transportation Impact Study Guidelines to reflect changes in law and transportation engineering practice;

WHEREAS, guidelines for the implementation of SB743, including thresholds of significance under CEQA are necessary to evaluate environmental impacts under SB743;

WHEREAS, on March 24, 2021, the Planning Commission reviewed the Transportation Impact Study and SB743 Guidelines; and

WHEREAS, consideration of the Transportation Impact Study and SB743 Guidelines, including Thresholds of Significance under CEQA have been publicly reviewed and considered.

NOW, THEREFORE, the Citrus Heights City Council hereby resolves as follows:

1. The City Council hereby adopts the Transportation Impact Study Guidelines as shown in Exhibit 1A

2. The City Council hereby adopts the SB743 Implementation Guidelines as shown in Exhibit 1B
3. The City Council hereby finds the adoption of Thresholds of Significance in the Transportation Impact Study and SB743 Implementation Guidelines have been promulgated pursuant to CEQA Guidelines 15064.7; and
4. The adoption of the Transportation Impact Study and SB743 Implementation Guidelines is not a project pursuant to CEQA Guidelines Sections 15060(c)(3) and 15378.

PASSED AND ADOPTED by the City Council of the City of Citrus Heights, California, this 22nd day of April 2021, by the following roll call vote:

AYES: **Council Members:**
NOES: **Council Members:**
ABSTAIN: **Council Members:**
ABSENT: **Council Members:**

Steve Miller, Mayor

ATTEST:

Amy Van, City Clerk

Exhibit 1A: Transportation Impact Study Guidelines

Exhibit 1B: SB743 Implementation Guidelines

Transportation Impact Study Guidelines for City of Citrus Heights

March 2021

Prepared By:

FEHR  PEERS

Prepared for:



TABLE OF CONTENTS

1.	INTRODUCTION	1
	Background	1
	Preparers of Studies	2
	Project Considerations	2
	Intent of Study Guidelines	3
	Relationship to Recent Caltrans Policy Documents.....	3
	General Plan Context.....	4
2.	TRIGGERS REQUIRING AN IMPACT STUDY.....	5
	Project Definition	5
	Trip Generation	6
3.	SCOPE OF THE STUDY	9
	Study Area.....	9
	Transportation Analysis Scenarios.....	9
	Transportation Analysis Time Periods	10
	Consultation with Other Jurisdictions	11
	Major Components of the Study.....	11
4.	RELEVANT POLICIES	13
	Vehicle Level of Service.....	19
5.	ANALYSIS METHODOLOGY	20
	Transportation Data Collection.....	20
	Multimodal Analysis.....	21
	Traffic Operations Analysis	22
	On-site Transportation Review.....	24
	Traffic Forecasts.....	25
6.	IMPACT ASSESSMENT.....	28
	Scenario Evaluation	28
	Significance Criteria	28
	Cumulative Impacts.....	30
7.	MITIGATION MEASURES.....	31
8.	RECOMMENDED PROCESS AND DOCUMENTATION.....	33

1. INTRODUCTION

Transportation impact study (TIS) guidelines are routinely established by jurisdictions to provide guidance on how to properly analyze the potential transportation impacts of proposed projects. The following guidelines have been developed to provide a clear and consistent technical approach to transportation impact analysis for projects within the City of Citrus Heights.

BACKGROUND

The First Citrus Heights TIS Guidelines were adopted in 2011 as part of the General Plan Update. The Guidelines were update in 2021 not only because of changes in analysis techniques and methods in the 10 years since they were last prepared, but moreover because of Senate Bill (SB) 743. This landmark law requires that environmental analyses performed under the California Environmental Quality Act (CEQA) do not use level of service (LOS) as the basis for identifying impacts of a proposed project to the transportation system¹.

SB 743, passed in 2013, required the California Governor's Office of Planning and Research (OPR) to develop new CEQA guidelines that address traffic metrics under CEQA. In December 2018, OPR published the *Technical Advisory on Evaluating Transportation Impacts in CEQA*, which provided guidance for implementing SB 743. Under this guideline, Vehicle Miles Traveled (VMT) is the primary metric used to identify transportation impacts. On July 1, 2020, the provisions of Section 15064.3 became effective statewide.

Citrus Heights will evaluate land use proposals and transportation projects in a manner consistent with SB 743 and guidance contained in the *Technical Advisory*. To this end, the City has prepared its *SB 743 Implementation Guidelines for Citrus Heights* (2021). Readers should refer to that document for details on VMT analysis methods, significance criteria, and mitigation measures.

Although no longer permitted within CEQA documents, the LOS analysis (and identification of locations whose operations would be adversely affected) is still prepared to provide helpful information to decisionmakers and the public, to assist staff in understanding what types of improvements should be considered as a Condition of Approval for the project, and to evaluate the project's consistency with the City's General Plan LOS policy.

As a result of SB 743, the format of transportation impact studies prepared in Citrus Heights will be different than in the past, and will likely vary depending on the type of environmental document that is prepared:

¹ CEQA Guidelines Section 15064.3 states that "automobile delay, as described solely by level of service or similar measures of vehicular capacity or traffic congestion shall not be considered a significant impact on the environment pursuant to this division, except in locations specifically identified in the guidelines, if any."

- (Mitigated) Negative Declarations² – a single comprehensive TIS document (either report or technical memorandum will be prepared containing all components of the analysis.
- Environmental Impact Reports (EIRs) – will consist of the transportation chapter, which will include the VMT analysis and analysis of other topical areas (e.g., impacts to bicycle, pedestrian, transit facilities and services, hazardous conditions, emergency response, construction impacts, etc.). A separate Local Transportation Analysis (LTA) will be prepared, which presents the LOS analysis.

The above approach enables City Community Development Department staff who are preparing a (Mitigated) Negative Declaration to copy/paste relevant information from the TIS directly into that document. The TIS itself will typically not be attached to the (Mitigated) Negative Declaration, but will be included in the staff report and posted on the City's website for review by the general public.

The LTA prepared in support of an EIR will not be included as an appendix to the Draft or Final EIR. The LTA will be included in the staff report and posted on the City's website for review by the general public. This approach was successfully first undertaken on the Sunrise Tomorrow Specific Plan Draft EIR.

The City expects these guidelines to continue resulting in studies that provide comprehensive and accurate analysis of potential transportation impacts to City facilities and services.

PREPARERS OF STUDIES

The City of Citrus Heights has established on-call transportation consulting agreements with consulting firms with demonstrated expertise in preparing high-quality and unbiased TISs. The City will not accept TISs prepared by consultants directly for an applicant. Refer to Section 6 for more details.

PROJECT CONSIDERATIONS

The following types of projects may require a TIS as determined by the City Engineer:

- Transportation infrastructure modification or expansion, including capital improvement projects (CIP) on city roads and state facilities.
- Land use entitlements requiring discretionary approval by the City of Citrus Heights, which includes annexations, general plan amendments, specific plans, zoning changes, conditional use permits, design review permits, and tentative maps.
- Land use activity advanced by agencies other than the City of Citrus Heights that is subject to jurisdictional review under state and federal law.

Section 2 identifies specific project parameters or "triggers" that may necessitate a TIS.

² Also includes Addendums to EIRs and other types of environmental documents that do not require public circulation and review.

INTENT OF STUDY GUIDELINES

These guidelines address key elements required for preparing and reviewing transportation impact studies in Citrus Heights. This document is intended to be a resource applied in concert with professional judgment. The following major issues are addressed in this document:

- Situations and thresholds that commonly trigger the need for a TIS.
- Scope and extent of the required study.
- Transportation impact analysis methods.
- Criteria to determine if the transportation-related impacts of a proposed project are significant under the California Environmental Quality Act (CEQA).
- Mitigation measure requirements.
- Guidelines for documentation of the findings, conclusions, and recommendations.

The City of Citrus Heights will review transportation studies and reports based on the guidelines presented in this document. However, each project is unique, and TIS guidelines are not intended to be prescriptive beyond practical. Not all criteria and analyses described in this document will apply to every project. Early and consistent communication with the Community Development and General Services Departments is encouraged to confirm the appropriate type and level of analysis required on a case-by-case basis.

RELATIONSHIP TO RECENT CALTRANS POLICY DOCUMENTS

In May 2020, the California Department of Transportation (Caltrans) published the *Vehicle Miles Traveled-Focused Transportation Impact Study Guide* (TISG), which replaced its Guide for the Preparation of Traffic Impact Studies (2002). The TISG generally endorses the policies, technical approaches, and recommendations from OPR's *Technical Advisory*. It also indicates that Caltrans intends to "transition away from requesting LOS or other vehicle operations analyses of land use projects", instead placing the focus on VMT and safety.

Caltrans published the *Interim Local Land Development and Intergovernmental Review (LDIGR) Safety Review Practitioners Guidance* in December 2020. This document provides guidance for conducting safety reviews of land use projects and plans that may affect the State Highway System. Although the LDIGR Safety Review Practitioners Guidance stops short of including specific thresholds of significance or providing specific recommendations for how safety evaluations should be included in CEQA documents, it does clearly indicate the State's expectation that, when appropriate, CEQA studies of land use projects should include safety investigations of the State Highway System. Furthermore, that document specifies that mitigation measures for identified safety impacts should avoid increasing roadway capacity, which may induce VMT or affect conditions for vulnerable users, such as bicyclists or pedestrians.

Citrus Heights will follow applicable analysis methods and general guidance provided in these recent Caltrans documents, to the extent they are applicable to the land use proposal or transportation project being evaluated.

GENERAL PLAN CONTEXT

The Community Development Element of the Citrus Heights General Plan specifically identified the development and adoption of transportation impact study guidelines that consider all modes of travel and establish clear guidance for analysis and significance criteria (Action 29.2.B and Policy 29.3.).

The General Plan was updated in 2011 to guide future decision-making in the City. The common vision is to be a highly livable place that is safe, has a strong sense of identity, and offers economic opportunity. As part of this vision, the City values a transportation network that supports mobility for all users, including drivers, transit users, bicyclists, and pedestrians of all ages and abilities.

For projects that are consistent with the General Plan, the impact analysis is generally limited to an evaluation of the project access points (including nearby intersections) and connectivity to the existing adjacent bicycle, pedestrian, vehicle, and transit facilities (see Sections 2 and 3 for details). Projects consistent with the General Plan that are large and/or unique (e.g., located on a corridor that is not part of the City's LOS-exempted corridor list) may trigger an expanded analysis subject to the determination of the City engineer. ***If a project is inconsistent with the General Plan, these guidelines do not apply, and a consultation with the Community Development and General Services Departments is required.***

The General Plan vision is supported by eight planning principles, with the two most relevant listed here:

Mobility: Increasing traffic, much of it from outside the City, will exacerbate congestion on the City's major roadways and also result in cut-through travel through residential neighborhoods, higher vehicle speeds and increased noise levels. Solutions could include street improvements, fixed-route transit (i.e., connecting key commercial districts), and improved bicycle and pedestrian routes. Where appropriate, streets should be completed and connected. In the past, roadways were viewed primarily for automobile travel. This viewpoint has evolved to one where roads are seen within a complete streets context, where the needs of all travel modes, users, and ability levels are equally important.

Sustainability: The City should promote efforts to improve communitywide sustainability for both the existing built environment and new development. Building and site design and construction practices should include energy, water, and other conservation techniques that reduce the consumption of natural resources. In addition, the City should support a transition to cleaner, more renewable energy sources. The City should implement measures to improve air quality and reduce greenhouse gas emissions.

Section 4 contains specific references to relevant transportation and mobility policies of the General Plan.

2. TRIGGERS REQUIRING AN IMPACT STUDY

Unless waived by the City Engineer, a TIS is required when any one of the following conditions is met:

- The project has the potential to create a significant environmental impact under CEQA (check Table 6 for a list of significance thresholds for all modes).
- The proposed project has the potential to generate 200 new (i.e., accounts for pass-by trips attracted to certain retail uses, see Section 3) passenger vehicle trips per day.
- The project requires a permit application, which is subject to discretionary approval.
- The project will substantially alter physical or operational conditions on a City roadway, bikeway, sidewalk, or other transportation facility.
- The project adversely affects transportation safety.

In general, a prepared TIS report is applicable for two years. After two or more years of inactivity, an updated TIS may be required.

PROJECT DEFINITION

The applicant shall provide a project description that, at a minimum, includes the following:

- Specific land uses intended for the site including a detailed project site plan.
- Size or intensity of the proposed development and uses (e.g., square footage, acreage, dwelling units, tonnage, number of employees or residents, etc.).

City staff will then normally determine whether the project generates 200 or more new passenger vehicle trips per day. Refer to Table 1 for typical project trip generation estimates that reach this threshold. City staff will also determine whether the project may have adverse effects on off-site transportation facilities or services including transit, roadways, bikeways, and sidewalks.

This detailed and accurate information is critical to determine if a TIS is required based on potential significant environmental impacts or trip generation.

Does my project require a transportation impact study?

Actions That May Be Subject to CEQA

- *Infrastructure construction*
- *Adoption of an ordinance or resolution*
- *Land use changes*
- *Funding from public agency contracts, subsidies, and loans*
- *Issuance of a lease, permit, license, certificate, or other entitlement*

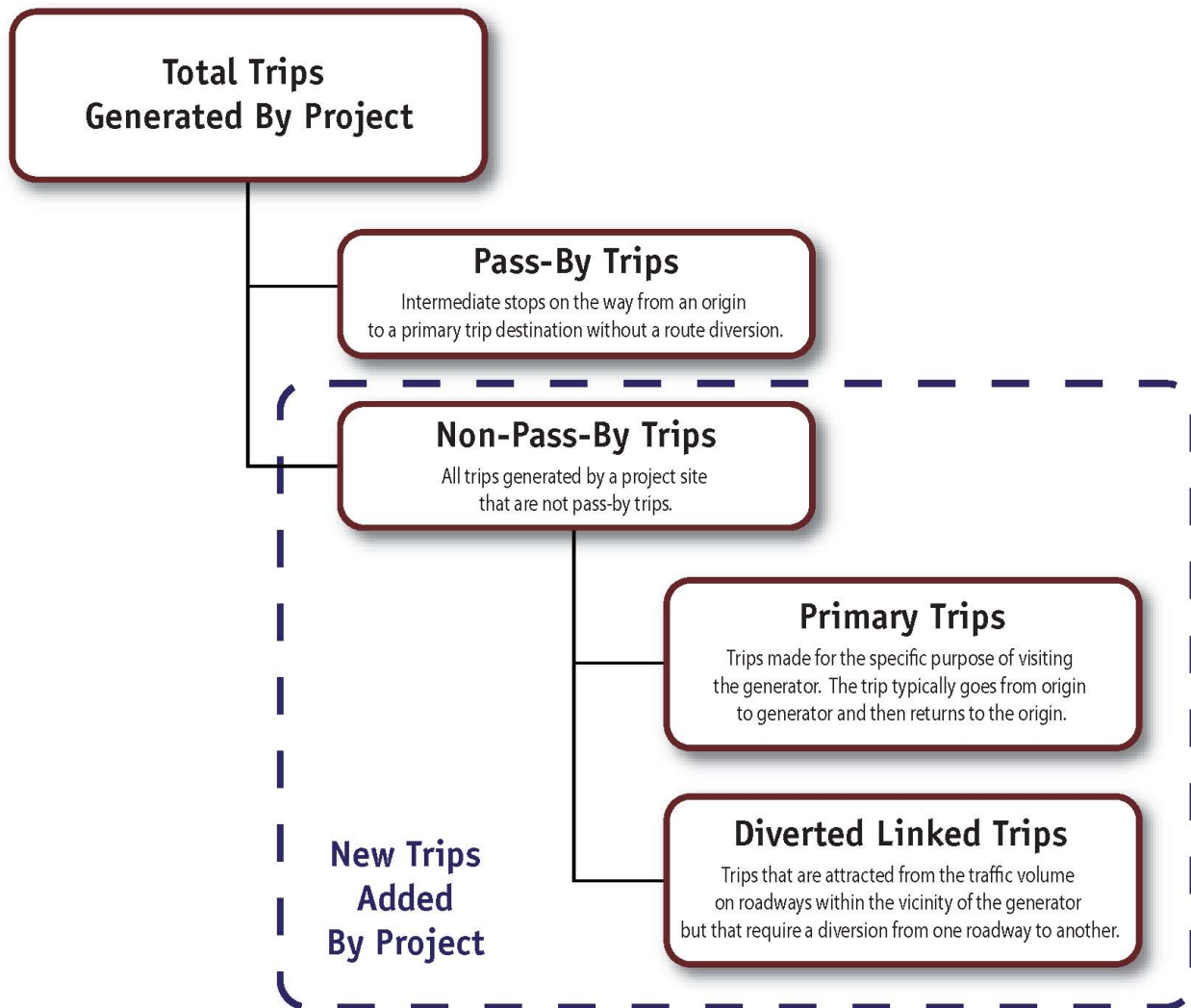
If the proposed project includes any of the above actions, an Initial Study (IS) should be prepared to determine the appropriate environmental clearance documents, such as an Environmental Impact Report (EIR).

TABLE 1: QUANTITY OF LAND DEVELOPMENT THAT TRIGGERS 200 DAILY TRIP THRESHOLD

Proposed Development Example (ITE Land Use Code)	New Daily Trips Generated ^{1,2}
Single Family Detached Housing (210) – 22 dwelling units	208
Multifamily Housing Mid-Rise (221)– 37 dwelling units	201
Shopping Center (820) – 7 KSF leasable area	219
General Office Building (710) – 21 KSF gross floor area	205
High-Turnover (Sit-Down) Restaurant (932) – 2.5 KSF gross floor area	218
Medical-Dental Office Building (720) – 6 KSF gross floor area	209
¹ Trip rates based on data published in <i>Trip Generation Manual, 10th Edition</i> (ITE, 2017). ² Assumes pass-by percentages of 17% for shopping center and 22% for high-turnover sit-down restaurant, based on the premise that the daily pass-by rate is half of the PM peak hour pass-by rate contained in the <i>Trip Generation Handbook, 3rd Edition</i> (ITE, 2017). KSF = 1,000 square feet	

TRIP GENERATION

The trip generation analysis shall identify the number of new daily and peak hour vehicle-trips added by the proposed project. The trip generation estimation for all new or proposed development projects shall include the summation of primary trips, pass-by trips, and diverted linked trips. The figure on the following page describes trip types relevant to trip generation and the difference between the total trips generated by the project versus new trips added by the project.



The estimation of new trips generated by the proposed development project may include credit for trips associated with existing uses on the site. Existing uses are typically those actively present on the project site at the time data is gathered for the traffic impact study.

The final estimate of new daily and peak-hour trips associated with a proposed development project should represent the net contribution of the proposed project. The City will review the trip generation analysis and determine if additional analysis is required.

Trip generation analysis should be primarily based on trip generation rates derived from local empirical data. Recognizing that this is not always possible, the most recent version of the Institute of Transportation Engineers (ITE) *Trip Generation Manual*³ and recommendations provided in the *Trip Generation Handbook* should be used.⁴ If multiple trip generation rate sources exist, the study shall provide a comparison and use the rates that best reflect local conditions and applicable regulatory constraints.

The project trip generation rate cannot be based solely on one nearby or similar land use facility. The sample used for non-standard trip generation rates shall include at least three similar facilities in Citrus Heights or neighboring jurisdictions with similar characteristics.

If the study involves comparable sites located in other communities, chosen sites and uses to be studied should be reasonably equivalent to the site and use proposed within the City.

The final trip generation rates used for the project should be a weighted average of the various trip generation rates available. A tabular summary of the final trip generation rate calculation shall be provided. Appendix A provides sample trip generation calculations.

Establishing Trip Generation for an Unknown Use

Option 1:

In the case of "shell" buildings with unidentified uses or where the ultimate tenant use of the building cannot be restricted, the City Engineer will likely recommend the use of the highest traffic intensity among all permitted uses to establish traffic impacts and to calculate project impact fees.

Option 2:

Traffic impacts may be assessed based on a use with lower traffic intensity if the City and the developer establish a trip budget threshold. The trip budget will be monitored by the Planning Department. Additional evaluation is required if the trip budget will be exceeded by a proposed project.

¹ *Trip Generation Manual*, 10th Edition, Institute of Transportation Engineers, 2017.

² *Trip Generation Handbook*, 3rd Edition; *An ITE Recommended Practice*, Institute of Transportation Engineers, 2017.

3. SCOPE OF THE STUDY

The contents and extent of a transportation impact study depend on the location and size of the proposed development, the prevailing conditions in the surrounding area, and the technical questions being asked by decision makers and the public.

STUDY AREA

Defining a study area needs to be developed through consultation with City staff that results in substantial evidence (facts, analysis, etc.) supporting the study area delineation. The boundary should extend as far as any potential CEQA impact might occur, including across jurisdictional boundaries. The City must approve study locations before traffic data collection and analysis commences. Careful consideration of all modes and facilities (i.e., transit, pedestrian, bicycle, vehicle, rail crossings, etc.) is required when selecting the study area boundary. The study area should be viewed as the “area of influence” of a specific project. The extent of the study area should be determined based on consultation with City staff and the following guidance:

- For potential impacts to pedestrian facilities, the study area should be a minimum of ½ mile.
- For potential impacts to bicycle facilities, the study area should be a minimum of two miles.

Additional facilities may be studied based on circumstances unique to the site, especially those related to pedestrians, bicycles, and transit.

TRANSPORTATION ANALYSIS SCENARIOS

The potential transportation analysis scenarios are listed below. Projects consistent with the General Plan will be required to complete the Baseline Conditions analysis. Future Conditions would typically be required for projects that are proposing general plan changes or at the discretion of the City engineer depending on the project type and location.

BASELINE CONDITIONS

- **Baseline Conditions** represents transportation conditions for all travel modes in the study area based on recent field observations. The baseline condition is often the existing condition unless there is an impending planned transportation or land use projects that will be in place prior to the proposed project is constructed. Traffic volumes for roadway analysis should be based on recent count data⁵. For CEQA compliance, the transportation impact analysis should include a description of the physical environmental condition in the vicinity of the project, as they exist at the time of the notice of preparation is published, or if no notice of preparation is published, at the time environmental

⁵ At the time this set of guidelines was being prepared, the COVID pandemic was still causing substantial changes in travel behaviors. Consequently, analyses are continuing to rely on “Pre-COVID” traffic volumes, which are available from the City’s online count database, historic counts from vendors, and “big data” from Streetlight, Data, Inc.

analysis is commenced, from both a local and regional perspective (CEQA Guidelines Section 15125(a)).

- **Baseline Plus Project Conditions** represented by project changes to baseline transportation conditions for all travel modes in the study area. For re-use or conversion projects, this will involve accounting for any existing use of the site that remains or will be discontinued. Per SB 743, any analysis of roadways or intersections performed as part of the study would not be included in the environmental document, but may be included in a staff report so that the public and decision-makers are aware of the project's effects.

FUTURE CONDITIONS

- **Cumulative No Project Conditions** represented by transportation conditions for all travel modes in the study area reflecting all approved projects plus pending projects or expected development of other areas of the City designated for growth. In most cases, the project site will likely be vacant under this scenario. In some cases though, this scenario may need to account for any existing uses on the site that could continue and potential increases in development allowed by ministerial approvals only.
- **Cumulative Plus Project Conditions** represented by Cumulative Conditions plus changes to these conditions caused by the proposed project. This scenario needs to account for whether the project is changing any existing or planned land uses on the site.

Additional analysis scenarios may be required in the traffic impact analysis dependent on project conditions and setting. For example, other scenarios may be needed to test phasing or other interim conditions, at the discretion of the City.

TRANSPORTATION ANALYSIS TIME PERIODS

The determination of analysis time periods will depend on the travel modes being evaluated. For non-auto travel modes, the analysis may include daily, peak period, or peak hour conditions. Final determination shall be made in consultation with City staff. For roadway analysis, General Plan Policy 29.2 states that peak hour traffic volumes will be used in determining compliance with the vehicle LOS standard. These may include weekday AM, Mid-Day, and PM hours as well as Saturday Mid-Day or PM peak hours.

Based on the land use of the proposed project and upon consultation with City, the study should analyze traffic operations during the peak hour of the following time periods:

- Weekday morning peak (7:00 – 9:00 AM)
- Weekday evening peak (4:00 – 6:30 PM)

For some projects, the City may substitute or require additional peak hour analysis for the following time periods:

- Weekday afternoon or school peak (2:00 – 4:00 PM)
- Friday evening peak (5:00 – 7:00 PM)
- Weekend midday peak (11:00 AM – 1:00 PM)
- Weekend evening peak (4:00 – 7:30 PM)

The determination of study time periods should be made separately for each proposed project based upon the peaking characteristics of project-generated traffic and peaking characteristics of the adjacent street system and land uses. The time period(s) that should be analyzed are those that exhibit the maximum combined level of project-generated traffic and adjacent street traffic.

CONSULTATION WITH OTHER JURISDICTIONS

If the study area overlaps with other jurisdictions, the other jurisdictions must be consulted to verify study locations and to specify the impact significance criteria that should be used in the TIS for these locations. Section 15086 of the CEQA Guidelines shall be followed as the basis for satisfying consultation requirements. Although no longer required by CEQA, a limited/focus amount of intersection operations analysis in another jurisdiction may be warranted under certain conditions subject to the final determination of the City engineer.

MAJOR COMPONENTS OF THE STUDY

The extent and complexity of a transportation impact study can vary greatly. Table 2 provides basic transportation and circulation elements that shall be acknowledged in every project requiring a TIS. Table 3 in Section 4 identifies relevant policies by each element. Specific analysis methodologies and significance criteria for each of the listed elements are described in further detail in Sections 5 and 6. Communicating the transportation impact study results is as important as the analysis itself. Effective graphics, charts, and simulations are often necessary to successfully communicate analysis results to decision makers and the public.

TABLE 2: TRANSPORTATION AND MOBILITY ELEMENTS ADDRESSED IN AN IMPACT STUDY

Elements	Evaluation
On-site Circulation	Review and evaluate site access locations, driveway throat depths, size of major circulation features with respect to operations and safety, turning movement volumes at site access points, queuing at site access driveways, adequacy of passenger drop-off and pick-up areas, dimensions of truck loading areas, truck delivery routes, and emergency access. Address and accommodate pedestrian and bicycle access. See Appendix B for a sample.
Off-Site Traffic Operations	Study all roadway facilities using methods and procedures contained in the latest version of the <i>Highway Capacity Manual</i> (HCM).

Elements	Evaluation
Bicycle Facilities	Identify any existing or planned bicycle facilities that may be affected by the project. Focus on maintaining or enhancing connectivity and completing network gaps.
Pedestrian Facilities and Americans with Disabilities Act (ADA) compliance	Identify any existing or planned pedestrian facilities that may be affected by the project. Focus on maintaining or enhancing connectivity, completing network gaps, and removing barriers. Disclose evaluation and documentation of project features (e.g., road widening) with likely disparate impact on pedestrians (e.g., longer crossing time).
Parking	Compare the project parking plan with City standards.
Trucks (or other heavy vehicles)	For truck traffic generating projects, identify the number of truck trips that will be generated, and design facilities necessary to accommodate truck traffic.
Transit	Identify any existing or planned transit facilities that may be affected by the project. Focus on maintaining or enhancing connectivity, completing network gaps, and avoiding increases in transit vehicle travel times.
Intersection Traffic Control	The City allows roundabouts instead of traffic signals or all-way stop control. Evaluate unsignalized intersections located within the study area to determine appropriate traffic control with or without the project.
General and Related Plan Consistency	Evaluate the project against transportation-related goals, policies, and actions set forth in the General Plan, Pedestrian Master Plan, and Bikeway Master Plan. Depending on project type and location, evaluation may be needed for the Old Auburn Road Complete Streets (OARCS) Plan, Carriage/Lauppe Safe Schools Corridor Plan, Multi-Modal Transportation Safety Program (MMTSP) and City Intelligent Transportation System (ITS) Master Plan.
Other Subject Areas	Consider other subject areas on a case-by-case basis.
Other Jurisdictional Requirements	In situations where several agencies must approve a development or are responsible for affected roadways, coordination with multiple lead and responsible agencies may be necessary to finalize scope of work, analysis methods, and assumptions.
Safety	Projects subject to CEQA review should be analyzed for potential safety impacts to the state highway system in accordance with the <i>Interim Local Land Development and Intergovernmental Review (LDIGR) Safety Review Practitioners Guidance</i> (Caltrans, December 2020). Safety evaluations may also be performed for City-maintained roadways using the following available documents: <i>City of Citrus Heights Bikeway Master Plan</i> (2015) and <i>City of Citrus Heights Pedestrian Master Plan</i> (2016). By the end of 2021, a Local Road Safety Plan (LRSP) will be completed for the City's transportation system, which may also be utilized in safety evaluations.
VMT	Projects subject to CEQA review should be analyzed for VMT impacts in accordance with the <i>SB 743 Implementation Guidelines for City of Citrus Heights</i> (2021). This document includes an extensive discussion that defines VMT, describes how per capita and per employee VMT is estimated, presents recommended significance criteria for VMT impacts, and identifies feasible mitigation measures for significant VMT impacts.

4. RELEVANT POLICIES

An important aspect of a TIS is to provide sufficient information for the City to determine that a project is consistent with the General Plan and other applicable City plans. As such, individual projects must be reviewed against relevant policies contained in the General Plan. Applicants with a project within the Sunrise Tomorrow Specific Plan, Stock Ranch Guide for Development, or Boulevard Plan areas should also confirm applicable policies from these plans with the City.

Table 3 lists the most common policies associated with each element of a TIS in an abbreviated fashion. Applicants should review the full policy statement in the General Plan.

TABLE 3: PRIMARY TRANSPORTATION AND MOBILITY GENERAL PLAN POLICIES

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
On-site Circulation	<ul style="list-style-type: none">➤ Policy 29.4: Support safe, complete and well-connected neighborhood street, bicycle, and pedestrian access and connections that balance circulation needs with the neighborhood context.➤ Policy 29.8: Minimize the number of access points along arterial roadways, including by consolidating or relocating driveways to provide for more efficient traffic movement.➤ Policy 30.3: Discourage the construction of private streets to ensure full public access to the City circulation system.➤ Policy 6.5: Consider the neighborhood context when developing traffic calming devices for established rural neighborhoods➤ Policy 13.1: Improve mobility in the Sunrise MarketPlace area to provide adequate access for vehicles, transit, bicycles and pedestrians.➤ Policy 13.2: Create convenient connections across Sunrise Boulevard for vehicles, bicycles, pedestrians and transit.	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Off-Site Traffic Operations	<ul style="list-style-type: none"> ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 3.5 and 7.6: Plan, design, and construct neighborhood and rural residential streets to encourage walking and bicycling while discouraging high vehicle speeds and volumes consistent with Policy 29.1. ➤ Policy 6.1: Provide public improvements that are appropriate and compatible with the unique qualities of the City's rural neighborhoods. ➤ Policy 6.5: Consider the neighborhood context when developing traffic calming devices for established rural neighborhoods. ➤ Policy 29.2: Measure customer satisfaction related to vehicle travel using level of service (LOS) according to procedures in the latest version of the Highway Capacity Manual published by the Transportation Research Board. The City will strive to achieve LOS E or better conditions for City roadways and intersections during peak hours, with exception of the exempt locations identified in this policy. ➤ Policy 53.2: Minimize the impacts of vehicle emissions on air quality. ➤ Policy 29.4: Support safe, complete and well-connected neighborhood street, bicycle, and pedestrian access and connections that balance circulation needs with the neighborhood context. ➤ Policy 30.2: Require public street right-of-way dedications and improvements as development occurs. 	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Bicycle Facilities	<ul style="list-style-type: none"> ➤ Policy 30.1: Improve aesthetic features along the City's roadways and maintain landscaping in an efficient and timely manner especially when it enhances the walking and biking environment. ➤ Policy 19.1: Promote improvements to major corridors to make them more distinctive and inviting. Encourage installation and maintenance of landscaping in median and street frontages along arterial roadways. ➤ Policy 19.2: Establish a street tree planting program for major corridors. ➤ Policy 19.3: Require landscaping on commercial, residential, and institutional uses adjacent to all public street frontages. ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 29.1 – Action B: Evaluate projects to ensure that the safety, comfort, and convenience of pedestrians and bicyclists are given equal level of consideration to drivers. ➤ Policy 29.1 – Action C: Consider ways to increase and improve travel choices when reviewing development or transportation infrastructure projects. ➤ Policy 29.1 – Action E: Improve the existing street network to minimize travel times and improve mobility for transit, bicycle, and walking trips between new projects and surrounding land uses to reduce vehicle trips. ➤ Policy 3.5 and 7.6: Plan, design, and construct neighborhood and rural residential streets to encourage walking and bicycling while discouraging high vehicle speeds and volumes consistent with Policy 29.1. ➤ Policy 13.1: Improve mobility in the Sunrise MarketPlace area to provide adequate access for vehicles, transit, bicycles and pedestrians. ➤ Policy 13.2: Create convenient connections across Sunrise Boulevard for vehicles, bicycles, pedestrians and transit. ➤ Policy 54.1: Encourage alternative modes of transportation and trip-reducing strategies. <p>Policies and planned improvements contained within the <i>City of Citrus Heights Bikeway Master Plan</i> (2015) should also be reviewed.</p>	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Pedestrian Facilities and Americans with Disabilities Act (ADA) compliance	<ul style="list-style-type: none"> ➤ Policy 30.1: Improve aesthetic features along the City's roadways and maintain landscaping in an efficient and timely manner especially when it enhances the walking and biking environment. ➤ Policy 19.1: Promote improvements to major corridors to make them more distinctive and inviting. Encourage installation and maintenance of landscaping in median and street frontages along arterial roadways. ➤ Policy 19.2: Establish a street tree planting program for major corridors. ➤ Policy 19.3: Require landscaping on commercial, residential, and institutional uses adjacent to all public street frontages. ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 29.1 – Action B: Evaluate projects to ensure that the safety, comfort, and convenience of pedestrians and bicyclists are given equal level of consideration to drivers. ➤ Policy 29.1 – Action C: Consider ways to increase and improve travel choices when reviewing development or transportation infrastructure projects. ➤ Policy 29.1 – Action D: Require sidewalks on all arterial and collector streets. Where feasible, separate sidewalks from streets on arterials and collectors with landscaping including a tree canopy to create shade ➤ Policy 29.1 – Action E: Improve the existing street network to minimize travel times and improve mobility for transit, bicycle, and walking trips between new projects and surrounding land uses to reduce vehicle trips. ➤ Policy 3.5 and 7.6: Plan, design, and construct neighborhood and rural residential streets to encourage walking and bicycling while discouraging high vehicle speeds and volumes consistent with Policy 29.1. ➤ Policy 6.6: Support development of "safe routes" to school for children residing in rural neighborhoods. ➤ Policy 13.1: Improve mobility in the Sunrise MarketPlace area to provide adequate access for vehicles, transit, bicycles and pedestrians. ➤ Policy 13.2: Create convenient connections across Sunrise Boulevard for vehicles, bicycles, pedestrians and transit. ➤ Policy 13.4: Facilitate the development of new buildings in areas currently devoted to parking to shorten distances between buildings and foster better pedestrian connections between shopping centers. ➤ Policy 30.4: Maintain street and sidewalks in rural residential areas that balance circulation needs and compatibility within the surrounding neighborhoods. ➤ Policy 54.1: Encourage alternative modes of transportation and trip-reducing strategies <p>Policies and planned improvements contained within the <i>City of Citrus Heights Pedestrian Master Plan</i> (2016) should also be reviewed.</p>	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Parking	<ul style="list-style-type: none"> ➤ Policy 13.4 – Action A: Consider establishment of a maximum parking ratio for the MarketPlace area that recognizes the value of land for additional development that can increase shopping and employment opportunities while also improving the convenience of walking, bicycling, and using transit. 	
Trucks	<ul style="list-style-type: none"> ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 29.5: Encourage movement of goods by truck on freeways and other appropriate designated routes. 	
Transit	<ul style="list-style-type: none"> ➤ Policy 31.1: Strive to increase fixed-route and demand responsive (i.e., paratransit) transit service coverage and frequency to Citrus Heights residents and employees. ➤ Policy 31.2: Strive to provide public transit that is an attractive, convenient, dependable and safe alternative to the automobile. ➤ Policy 31.4: Require new development to provide transit enhancements, (including, but not limited to bus pull-outs and bus shelters) where appropriate, that decrease transit travel times, improve access to transit stops, or improve the amenities, security, or travel information at transit stops. ➤ Policy 13.3: Promote installation of additional, distinctive transit stops at key activity areas and encourage covered shelters at existing and new stops. ➤ Policy 8.3: Support the creation of transit centers near Greenback Lane/Sunrise Boulevard and Greenback Lane/Auburn Boulevard. ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 29.1 – Action C: Consider ways to increase and improve travel choices when reviewing development or transportation infrastructure projects. ➤ Policy 29.1 – Action E: Improve the existing street network to minimize travel times and improve mobility for transit, bicycle, and walking trips between new projects and surrounding land uses to reduce vehicle trips. ➤ Policy 13.1: Improve mobility in the Sunrise MarketPlace area to provide adequate access for vehicles, transit, bicycles and pedestrians. ➤ Policy 13.2: Create convenient connections across Sunrise Boulevard for vehicles, bicycles, pedestrians and transit. ➤ Policy 13.5: Promote transit-oriented development through reuse and redevelopment of opportunity sites near the Greenback Lane/Sunrise Boulevard intersection. ➤ Policy 54.1: Encourage alternative modes of transportation and trip-reducing strategies 	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Intersection Traffic Control	<ul style="list-style-type: none"> ➤ Policy 32.1: Evaluate and utilize technologies that can improve the performance, reliability, and safety of the transportation system (such as signal coordination, centralized traffic control, red-light cameras, and real-time travel information). ➤ Policy 53.1 – Action D: Synchronize traffic signals on roads susceptible to high emission levels from idling vehicles. ➤ Policies and planned improvements contained within the <i>City of Citrus Heights ITS Master Plan</i> should also be reviewed. 	
General Plan Consistency	<ul style="list-style-type: none"> ➤ Review other elements of the General Plan for applicable policies, especially the Community Development and Community Health chapters. 	
Other Subject Areas	<ul style="list-style-type: none"> ➤ Policy 10.8: Discourage concentration of auto intensive facilities (such as drive through and gas station uses) and ensure that drive-through businesses are allowed only where compatible with the surrounding areas. ➤ Policy 3.5 – Action A: Regulate development to limit traffic on new local residential streets to 3,000 vehicles per day. 	
Other Jurisdictional Requirements	<ul style="list-style-type: none"> ➤ Policy 29.6: Collaborate with neighboring jurisdictions when updating the General Plan and preparing the Capital Improvement Program to work toward providing a regional Complete Streets transportation network for all modes. ➤ Policy 33.2: Establish formal and informal processes with regional agencies, the City of Roseville, Sacramento County, and Placer County to review and provide input on proposed development within one-half mile of the City limits. ➤ Policy 33.1: Maintain open communication and cooperation with all public agencies that serve residents and businesses in Citrus Heights. 	

VEHICLE LEVEL OF SERVICE

Historically, vehicle LOS thresholds have been the prevailing criteria applied to transportation projects. The City of Citrus Heights recognizes that vehicle LOS is one performance measure that needs to be carefully weighed against other City objectives to balance the preservation of community neighborhood values with a safe and efficient circulation system. The City's vehicle LOS standard is described in detail in the Community Development element of the General Plan (Policy 29.2) and is summarized to the right. Objectives or thresholds for other transportation modes have also been established and are addressed in these guidelines in Table 6.

If the TIS study area extends into an adjacent jurisdiction, their LOS threshold shall also be used to determine deficient operations for locations in that jurisdiction. The General Plan also states that LOS exceptions may be allowed on a case-by-case basis, where improvements are infeasible or would conflict with other community values.

City of Citrus Heights Vehicle LOS Policy

Measure customer satisfaction related to vehicle travel using LOS according to procedures in the latest version of the Highway Capacity Manual published by the Transportation Research Board. The City will strive to achieve LOS E or better conditions for City roadways and intersections during peak hours (these may include weekday AM, Mid-Day, and PM hours as well as Saturday Mid-Day or PM peak hours). The intent of the policy is to effectively utilize the roadway network capacity while balancing the desire to minimize potential adverse effects of vehicle travel on the environment and other modes.

Exceptions to LOS E are allowed at certain locations as specified in Policy 29.2 (see pages 35 and 36 in these guidelines for more details).

Source:

*Citrus Heights General Plan
Policy 29.2*

5. ANALYSIS METHODOLOGY

This section provides data collection and analysis procedures for conducting transportation impact studies in Citrus Heights. The City is committed to a balanced analysis for all modes of travel. The methodology presented in this section includes robust data collection and analysis techniques for pedestrian, bicycle and transit networks, in addition to vehicle circulation.

TRANSPORTATION DATA COLLECTION

Accurate data is essential to achieve a high level of confidence in transportation analysis results. Existing traffic conditions data should be collected using the guidelines set forth in Table 4.

TABLE 4: BASELINE CONDITIONS DATA COLLECTION PROTOCOL

Data Set	Procedure
Peak period turning movement counts	<p>Collect data for all study intersections on a Tuesday, Wednesday, or Thursday during weeks without holidays, large special events, or heavy construction in the study area that results in temporary travel pattern shifts. Fall or Spring days without rain and when school is in session are preferred.</p> <ul style="list-style-type: none"> ➤ Care should be taken to collect data on days when schools are in session. ➤ Consult with the City to determine if adjustments are necessary to account for seasonal variation in traffic volumes. ➤ Due to the effects of the COVID pandemic on travel, existing traffic counts should be representative of pre-COVID conditions so long as travel continues to be affected. This is often accomplished by purchasing historic counts from vendors or by purchasing anonymous "big data" from Streetlight Data, Inc, or other vendors. Once a new post-COVID condition is reached, new traffic counts may be used to represent then current conditions. Careful coordination with City staff is required to determine the appropriate count timeframe. ➤ Bicycles and pedestrians should be included in all counts. ➤ Some projects may require vehicle classification or occupancy counts. Consult with the City on a case-by-case basis.
Daily traffic counts	Collect data for all study roadway segments using the parameters described above for peak period turning movement counts with the exception of collecting bicycle and pedestrian data.
Roadway geometrics	Establish existing geometrics from a combination of aerial photography, as-built plans, and site visits.
Travel time and speed	Collect as necessary (e.g., for microsimulation validation or to document how travel speeds may change). Collect data using a floating car survey.
Signal timing	Request timing from the City and other operating agencies such as Caltrans or Sacramento County. Verify timing in the field.
Collision data	Obtain collision data from City staff and other adjacent agencies as applicable (i.e. City of Roseville). If collision analysis is required on state highway system, obtain from Statewide Integrated Traffic Records System (SWITRS) or similar database (depending on study purpose).

Data Set	Procedure
Mode split	Summarize daily and peak hour mode split from study area or communities adjacent to study area. Data sources could include the Census journey-to-work survey, the SACOG household travel survey, or other available surveys.
Transit routes and use	Map existing transit routes and stops serving the study area and identify service hours. Document amenities (benches, shelters, bicycle parking, etc.) available at transit stops and centers within ¼-mile of non-residential projects and a ½-mile of residential projects.
Bicycle and pedestrian facilities	Map existing bicycle and pedestrian facilities within the study area (include sidewalks, crosswalks, signal heads, push buttons, related signing and striping). Document barriers, deficiencies and high-pedestrian demand land uses including schools, parking, senior housing facilities, and transit stops or centers.

MULTIMODAL ANALYSIS

Evaluate the project's potential adverse effects on transportation facilities and services related to transit, bicycles, and pedestrians. The evaluation could include identification of any disruption to existing facilities and services or interference with the implementation of planned facilities and services. This effort will require identifying and mapping existing facilities. Particular attention should be made to roadway or intersection widening improvements and the addition of new driveway locations, that would increase pedestrian/bicycle crossing times or increase the potential for vehicle and pedestrian/bicycle conflicts. Consideration should also be given to how a project affects accessibility between each travel mode and the surrounding land uses.

For some projects, more detailed multi-modal analysis may be required. In these cases, the methodology shall be selected in consultation with City staff and should consider new tools, procedures, and performance measures such as those listed below.

- **Multimodal LOS** – The HCM contains multi-modal LOS analysis methods. Simulation models can also be used to measure performance (i.e., person-delay) for all modes using the transportation network.
- **Activity Connectedness** – Travel time for each mode (vehicles, transit, bicycles, and walking) between the project and surrounding land uses can be used to gauge the degree of accessibility for a project. The City desires to minimize travel time to necessary destinations while minimizing unnecessary vehicle travel. The main idea is to evaluate activity centers and destinations around projects to ensure that walk times to necessary destinations are minimized and the walking experience is comfortable.
- **Speed Management** – Desired travel speeds for each mode should be considered in project evaluation where new transportation facilities are being constructed. The City desires roadways to be designed for 40 miles per hour or less on arterial streets and 30 miles per hour or less on collector streets to reduce the severity of collisions and minimize air pollution and greenhouse gas emissions.

TRAFFIC OPERATIONS ANALYSIS

Traffic conditions shall be analyzed using standard or state-of-the-practice professional procedures for trip generation, trip distribution, and traffic assignment, which can generally be found through organizations such as Institute of Transportation Engineers (ITE), Caltrans, Federal Highway Administration (FHWA), and American Planning Association (APA).

General Plan Policy 29.2 states that vehicle LOS definitions and calculation methods must be consistent with the latest edition of the *Highway Capacity Manual* (HCM). The HCM is published by the Transportation Research Board. The most recent version is the 6th Edition, published in 2016.

Analysis Parameters

Table 5 provides guidance for use of specific analysis parameters (e.g., signal phasing, conflicting pedestrian volumes, etc.).

TABLE 5: ANALYSIS PARAMETER RECOMMENDATIONS

Parameter	Recommendation
Peak hour factor (PHF)	A PHF of 1.0 should be used for all analyses that involve City-owned and operated facilities. This approach has also been adopted by the Cities of Sacramento and Roseville, and County of Sacramento. By applying a 1.0 PHF, the reported LOS is based on conditions for the entire peak hour, versus the busiest 15-minutes of the peak hour. In selecting a 1.0 PHF, the City concluded that evaluating and reporting conditions (and sizing infrastructure) for the busiest 15-minutes of the day is overly conservative. If analyses are being performed for a Caltrans facility, coordination with District 3 traffic operations staff is recommended to confirm PHF and other assumptions.
Saturation flow rate	Prior analyses within Citrus Heights have demonstrated that use of the default ideal saturation flow rate (i.e., 1,900 vehicles per hour per lane) contained in the latest version of the HCM allows for microsimulation models to be validated to prescribed specifications. Under certain circumstances, however, field measurement of saturation flow rates may be warranted.
Yellow phase	If a traffic signal is present under baseline conditions, use existing yellow phase from city provided timing sheets. For future conditions, consult with the City and consider the most recent version of the California Manual on Uniform Traffic Control Devices (MUTCD).
All red phase	If traffic signal is present under baseline conditions, use existing all red phase from city provided timing sheets. For future conditions, consult with the City and consider the most recent version of the California MUTCD. All red phase may be greater on high-speed roadways.
Conflicting pedestrians for signalized intersections and roundabouts	Should be based on existing pedestrian counts or observations. Otherwise, refer to the most current version of the HCM to determine the amount of pedestrian activations per cycle into appropriate categories.

Parameter	Recommendation
Traffic signal cycle lengths and phasing	Replicate existing cycle length and phasing (e.g., leading left turns) from city provided timing sheets. For new signalized locations, coordinate with City staff regarding assumed signal phasing, cycle lengths, and coordination with other intersections along the study corridor.
Heavy truck percentages	Based on the existing heavy-truck percentage and adjusted to account for future planned development. In general, heavy-truck percentages should be greater on truck routes and main thoroughfares than on local streets. Minimum recommended value is 3%.
Lane utilization factor	If applicable, adjust lane utilization factors based on field observations. Otherwise, refer to the most current version of the HCM.

Analysis Tools and Methods

Traffic operations analysis should be conducted using tools and methods approved by the City of Citrus Heights.

Intersection Analysis – Traffic Operations

Microsimulation models, such as SimTraffic or VISSIM, should be used at the majority of intersections in the City. These models better consider the effects of signal coordination, upstream/downstream bottlenecks, turn lane overflow effects, imbalanced lane utilization, congested conditions, and other factors than deterministic models such as Synchro. They also produce 95th percentile queue lengths that are more accurate than deterministic models. In instances, where the intersection is isolated (i.e., not coordinated), under-capacity, and does not have a critical turn movement whose storage is to be evaluated, deterministic models are suitable to use.

Microsimulation analysis requires at least 10 runs to be performed, statistical outliers to be removed, and reported results to be the average of 10 runs. For oversaturated conditions, it may be necessary to perform a 15-minute or 30-minute, pre-peak hour seeding runs so that congested conditions are present at the beginning of the peak hour of analysis. Microsimulation models should be validated to existing conditions using a variety of performance standards such as average travel time, queue lengths, and vehicles served. The level of variance between the ten runs should be checked to ensure that it is not excessive (e.g., the average delay is 90 seconds and variance is 60 seconds would be considered excessive). High variance results are typically indicative of including a run where “a vehicle gets stuck” or randomness inherent to this type of analysis has a material effect on the outcome.

Intersection Analysis – Signal Phasing / Swept Path Analysis

When changes are being proposed at a signalized intersection that would result in changes in signal phasing, the AutoTurn software program should be used to determine whether simultaneous left-turn movements are possible in the event protected left-turn phasing is being considered. Exhibits should be prepared as part of the TIS to illustrate these swept paths (e.g., for a bus or WB-50 type truck). Coordination with City staff is required to determine the appropriate design vehicle(s).

Roundabout Analysis – Traffic Operations

Roundabouts may be analyzed using the Sidra software program or microsimulation analysis. The volume-to-capacity (V/C) ratio should be checked on each approach to the roundabout to verify that they are less than 0.85. Values over 0.85 suggest the potential for lengthy queuing and delays and require supplemental review to determine its appropriateness. Microsimulation would be particularly appropriate if a roundabout is proposed at a busy (with many vehicles, bicyclists, and pedestrians) intersection on an existing four-lane arterial.

Roundabout Analysis – Geometric Review

The geometric configuration of a proposed roundabout should be analyzed to determine its ability to accommodate buses or a truck using the AutoTurn software program. Coordination with City staff is required to determine the appropriate design vehicle(s). Other considerations in the geometric review are the placement of crosswalks, and the required amount of lateral transition to maintain slow speeds through the roundabout.

ON-SITE TRANSPORTATION REVIEW

A detailed site plan review is required for every project. The following site plan elements should be reviewed in detail. Other elements may also need to be studied depending on the type of project.

- Driveway spacing, width, permitted turning movements, curb return radii, and sight distance.
- Minimum required throat depth (for inbound and outbound travel) at project driveways based on minimum requirements as described in Section 106 of the City's Municipal Code. For special circumstances, throat depth may be calculated using either microsimulation or the method described in *Estimation of Maximum Queue Lengths at Unsignalized Intersections* (ITE Journal, 2001). Use of 95th percentile queue lengths from Synchro is discouraged due to known limitations in accuracy.
- Need for deceleration lanes at driveways.
- Truck turning movement adequacy including refuse, delivery trucks, etc.
- Presence/adequacy of pedestrian paths from the project site to adjacent sidewalks.
- Need for median treatments along adjacent roadways to accommodate project access.
- Review of on-site circulation including width of drive aisles, parking lot layouts, drive-through lane storage, pedestrian pathways, and other circulation-related considerations.

Appendix B includes several sample illustrations of site plan review recommendations.

Parking

Coordination with City staff is required to determine if analysis of the project's parking supply is required. In many instances, City staff will verify that the project's parking supply is consistent with requirements from the City's Municipal Code. However, in special cases, a focused parking supply and demand analysis may be

warranted as part of the TIS. This could include projects that propose less parking than the code requires or projects that propose a mix of complementary mix of land uses that provide opportunities for shared parking. In the latter case, the latest version of the Shared Parking spreadsheet (jointly published by the Urban Land Institute and National Parking Association in 2020) should be used.

TRAFFIC FORECASTS

Most TISs require one or both of the following types of traffic forecasts of the roadway system:

1. "Plus Project" Traffic Forecasts
2. Cumulative Traffic Forecasts

These forecasts may be used for peak hour intersection operations or for daily roadway segments to be used as inputs into air or noise analyses. The need for cumulative forecasts and analysis is subject to the discretion of the City engineer based on the project type and location.

1. "Plus Project" Traffic Forecasts

Most "Plus Project" forecasts rely on the standard four-step method in transportation planning: trip generation, mode split, trip distribution, and traffic assignment. Each of these steps is discussed below:

Trip Generation

Chapter 2 of these guidelines provided a detailed recommended approach for estimating a project's trip generation.

Mode Split

Mode split refers to the method of travel a resident, employee, customer, etc. selects to travel to/from a proposed project. Mode split can be estimated in a number of ways, such as the following:

- Mode of travel observed at nearby, similar facilities.
- Estimates derived from the US Census, American Community Survey, or SACOG 2018 Household Survey.
- Use of a mixed-use trip generation model that has been demonstrated to be validated to local conditions.

Note that if ITE trip generation estimates are used for the trip generation estimate, those rates already have a certain (but not discernable) amount of non-auto travel built into them. This is because those rates were collected primarily in suburban settings with limited transit options, and free parking. Despite this, some (modest) levels of external travel by walking, biking, and transit likely occurred.

Trip Distribution

Trip distribution refers to the directionality of external trips made by a resident, employee, customer, etc. to/from a proposed project. Trip distribution can be estimated in a number of ways, such as the following:

- Review of existing turning movements and complementary land uses.
- Use of “big data” (using anonymous cell phone records) to measure the trip distribution of similar land uses located near the proposed project.
- Use of a travel demand model to track trips from a specific Traffic Analysis Zone (TAZ) that represents the project site.
- Other trip distribution specific data, such as school district boundaries, ZIP codes of students/employees, general population distribution, and presence of similar/competing facilities (e.g., coffee shop) nearby.

Trip distribution patterns often differ by land use type. They may also differ by time-of-day or for the inbound/outbound travel directions. These and other factors should be considered when developing trip distribution estimates.

Trip Assignment

Trip assignment refers to the specific path of travel expected to be used by project vehicle trips. Trip assignment is typically determined in one of the following ways:

- Review of existing turning movements (e.g., relative amount of left-turn traffic from two parallel routes).
- Travel time comparisons among potential routes (collected via in-person travel time runs or based on Google Maps travel time comparisons).
- Permitted driveway and adjacent intersection turning movements. Trip assignment will likely change if left-turns into or out of the project driveway are permitted versus prohibited. The availability of u-turns at a nearby intersection may also influence trip assignment.

In some instances, a project may result in changes in background travel. This could occur if new roadways are being constructed that provide new travel options. The introduction of certain land uses to an area that currently have none may background travel patterns.⁶

Consultants should submit a Project Travel Characteristics memo to City staff for review/approval prior to initiating next steps in the analysis. Doing so avoids having to redo work should there need to be updates to parts of the trip generation and distribution elements.

⁶ As an example, the City of Citrus Heights does not current have any hotels within its boundaries. A proposed hotel could be evaluated by using “big data” to reveal travel patterns from nearby hotels outside the City limits to understand how much and where travel from these facilities is occurring within the City.

2. Cumulative Traffic Forecasts

The City of Citrus Heights does not have its own travel demand model that has been calibrated to local conditions. This is a fairly common situation for cities that are nearly built out and not expecting large amounts of new development. The City's roadway network and land uses are contained in both the SACMET and SACSIM versions of the Sacramento Area Council of Governments (SACOG) travel demand models. The SACSIM model is SACOG's officially adopted model as it was utilized for its now adopted 2020 MTP/SCS. The SACMET model has been used for several project-level applications in the City over the past decade. The SACMET model is trip-based while the SACSIM model is activity-based. The SACSIM model is very complicated and time-consuming to use, requiring many hours to make land uses changes and to run.

Consultants working on TISs in the City should coordinate with City staff regarding the most appropriate model to use. This will depend on the type of project, location of project, and any prior studies within the study area that may have been performed.

The difference method forecasting process should be used to develop cumulative traffic forecasts. This approach generally adds the growth in traffic between base and future year models to existing counts. This approach is used because it effectively accounts for base year model errors (e.g., underprediction of traffic on a certain roadway) that could otherwise translate to the future year forecasts if not accounted for by this method.

Consultants are recommended to coordinate with City staff regarding the most appropriate means for developing Cumulative Plus Project traffic forecasts. Several options are available as described below:

- Manually add project trips on top of Cumulative No Project forecasts.
- Add project to the travel demand model and develop Cumulative Plus Project forecasts. Manually subtract project trips to obtain Cumulative No Project forecasts.
- Allow the travel demand model to develop both Cumulative No Project forecasts and Cumulative Plus Project forecasts.

As there are distinct advantages and disadvantages to each method, collaboration with City staff is required to determine the most appropriate method.

6. IMPACT ASSESSMENT

The main intent of the TIS is to determine potential transportation impacts of proposed projects. This information is essential for decision makers and the public when evaluating individual projects. This section explains what operating conditions shall be used when determining an impact. These guidelines also establish criteria for when a project impact is considered significant.

SCENARIO EVALUATION

Transportation impact determination for a proposed development project shall be based upon the comparison of the following scenarios using the significance criteria cited below:

- Baseline Conditions vs. Baseline Plus Project Conditions
- Cumulative No Project Conditions vs. Cumulative Plus Project Conditions

SIGNIFICANCE CRITERIA

A project impact is considered significant when it meets the criteria listed in Table 6. Several of these criteria warrant further discussion, as provided below:

1. Pursuant to SB 743, significance criteria are not provided for potential roadway network LOS impacts. Instead, a new VMT impact significance criterion is provided. Refer back to the *SB 743 Implementation Guidelines for Citrus Heights* (2021) for more information about the significance criteria.
2. The third bullet under On-Site Circulation pertains to hazardous conditions including insufficient storage in turn lanes. The City has determined that on arterial roadways, a left- or right-turn lane whose 95th percentile vehicle queue exceeds the available turn lane storage as a result of a proposed project would be deemed to substantially increase hazards. This would be considered a significant impact. The above interpretation is more typically applied at intersections with collector streets or driveways, but not necessarily at the intersection of two arterials. This is because greater delays and queuing are typical at these arterial-arterial intersections and drivers expect more frequent stop-and-go traffic. The City Engineer will make the final determination with regard to the significance of turn pocket queuing exceedances.
3. Disruptions to transit service could include measurements of project-related increases in bus running time along high-frequency bus routes, along with the reported on-time performance of that route from SacRT. Studies show that late running buses can contribute to declining ridership.

TABLE 6: SIGNIFICANCE CRITERIA

Elements	The project would cause a significant impact if it would:
On-Site Circulation	<ul style="list-style-type: none"> ➤ Include designs for on-site circulation, access, and parking areas that fail to meet City or industry standard design guidelines. ➤ Fail to provide adequate accessibility for service and delivery trucks on-site, including access to truck loading areas. ➤ Substantially increase hazards due to geometric design features (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).
Off-Site Traffic Operations	Pursuant to SB 743, off-site traffic impacts are no longer considered significant under CEQA. Therefore, no significance criteria are provided.
Bicycle Facilities	<ul style="list-style-type: none"> ➤ Disrupt existing or planned bicycle facilities or conflict with adopted City bicycle plans, guidelines, adopted complete streets plans, policies, or standards. ➤ Add trips to an existing bicycle facility which adversely affects the operation or safety of the transportation system.
Pedestrian Facilities	<ul style="list-style-type: none"> ➤ Fail to provide accessible and safe pedestrian connections between buildings and to adjacent streets and transit facilities. ➤ Disrupt existing or planned pedestrian facilities or conflict with adopted City pedestrian plans, adopted complete streets plans, guidelines, policies, or standards. ➤ Add trips to an existing pedestrian facility, which adversely affects the operation or safety of the transportation system.
Parking	The direct effect of a given supply of parking is not considered a potential area of significance. However, the extent to which a given supply of parking results in searching for parking may be considered an indirect significant impact.
Trucks (or other heavy vehicles)	<ul style="list-style-type: none"> ➤ Fail to provide safe accommodation of forecast truck traffic or temporary construction-related truck traffic.
Transit	<ul style="list-style-type: none"> ➤ Disrupt existing or planned transit facilities and services or conflicts with adopted City transit plans, guidelines, policies, or standards.
Emergency Vehicle Response	<ul style="list-style-type: none"> ➤ Result in inadequate emergency access during construction and/or operation.
General Plan Consistency	<ul style="list-style-type: none"> ➤ Conflict or create an inconsistency with General Plan policies.
Construction-Related Impacts	<ul style="list-style-type: none"> ➤ Create a temporary but prolonged impact due to lane closures, need for temporary signals, emergency vehicles access, traffic hazards to bikes/pedestrians, damage to roadbed, or truck traffic on roadways not designated as truck routes.
Other Jurisdictional Requirements	<ul style="list-style-type: none"> ➤ Exceed established significance criteria thresholds for locations under the jurisdiction of other agencies.
VMT Impacts	<ul style="list-style-type: none"> ➤ Not qualify under one of several potential exemption categories (small projects, affordable housing, transit priority, etc.), and its VMT exceeded 85 percent of the regional per capita, per employee, or per visitor average VMT.¹
¹ Refer to Senate Bill 743 Implementation Guidelines for Citrus Heights (2021) for details.	

CUMULATIVE IMPACTS

Cumulative impact analysis must comply with the California Environmental Quality Act (CEQA). Land use development and infrastructure projects that are consistent with the General Plan, are expected to rely on the General Plan cumulative traffic analysis and EIR conclusions. Specific Plans will require updated cumulative traffic analysis consistent with the following definitions:

- The cumulative scenario is required per CEQA Guidelines Section 15130.
- The general definition of cumulative as a scenario is that it represents past, present, and reasonably foreseeable actions regarding land use development and the transportation network (see CEQA Guidelines Section 15355).

The General Plan environmental impact report (EIR) was based on a full build out of the City's land use designations and will generally cover the cumulative traffic effects of consistent development projects. However, over time, it is likely that general plan amendments or regional growth will influence background traffic volumes. If this occurs, individual projects may be required to conduct a project-specific cumulative analysis based on the determination of the City Engineer.

7. MITIGATION MEASURES

All significant project impacts should be mitigated consistent with the policies of the Citrus Heights General Plan. Table 7 shows the appropriate CEQA mitigation actions for each analysis scenario.

Each mitigation measure will require detailed review to assess resulting significance after mitigation. Table 7 provides a list of common mitigation measures that may be applicable to a proposed project. Other mitigation strategies may also be applied if supported by substantial evidence demonstrating their effectiveness.

TABLE 7: EXAMPLE MITIGATION MEASURES

Mitigation measures may include, but are not limited to, the following examples:	
Roadway Capacity Improvements	<p>The following would be considered improvements to improve upon deficient conditions, but not mitigations for significant impacts under CEQA.</p> <ul style="list-style-type: none"> ➤ Optimize location of access driveway(s) ➤ Provide additional through traffic lane(s), right-turn lane(s), and left-turn lane(s) if they don't adversely impact other modes or are prohibited per Policy 29.2 (see following page) ➤ Improve sight distances at intersections and driveways to acceptable standards
Traffic Control Modifications (warrants must be met)	<ul style="list-style-type: none"> ➤ Provide for yield or stop control ➤ Install roundabouts ➤ Provide coordination/synchronization or modified signal phasings at traffic signals along a corridor ➤ Provide turn-lane channelization through raised islands ➤ Restrict certain turn movements ➤ Optimize location of access driveway(s) and cross-parcel access
Transit Facilities	<ul style="list-style-type: none"> ➤ Provide bus turn-outs, bus shelters, additional bus stops, park-and-ride lots, and/or prioritized transit treatments (e.g., bus queue jump lanes)
Pedestrian and Bicycle Facilities	<ul style="list-style-type: none"> ➤ Provide for access to, from, and through the development for pedestrians and bicyclists ➤ Designate Class I bicycle paths, Class II bicycle lanes, Class III routes, or Class IV protected facilities
Land Use Changes	<ul style="list-style-type: none"> ➤ Alter density or diversity of uses to achieve vehicle trip reductions
Project Access and On-Site Circulation	<ul style="list-style-type: none"> ➤ Relocate/design driveways and/or restrict certain turn movements ➤ Lengthen turn pockets ➤ Construct right-turn deceleration lanes at driveways ➤ Modify site to provide adequate driveway throat depths ➤ Reconfigure on-site parking, drive-through lanes, or other on-site amenities
Transportation Demand Management (TDM) for VMT Impacts	<p>Refer to <i>SB 743 Implementation Guidelines for Citrus Heights</i> (February 2021) for discussion of potential TDM strategies.</p>
Construction-Related	<ul style="list-style-type: none"> ➤ Implement Construction Traffic Management Plans

In accordance with Policy 29.2, no roadway widening to provide additional vehicle capacity to the streets listed below will be permitted:

- Sunrise Boulevard – south City limits to north City limits
- Greenback Lane – west City limits to east City limits.
- Old Auburn Road – Sylvan Road to Fair Oaks Boulevard
- Antelope Road – I-80 to Auburn Boulevard
- Auburn Boulevard – Old Auburn Road to northern City limits

Development projects that impact these locations according to these transportation impact study guidelines would require mitigation, including, but not limited to, the following options:

- Actions that reduce vehicle trips or provide non-auto improvements to the transportation network or services
- Signal timing and/or phasing modifications
- Lengthening of turn pockets

For non-auto mitigation improvements, the mitigation measures should consider the planned bikeway, pedestrian, and transit needs as identified in the Citrus Heights General Plan, the *City of Citrus Heights Bikeway Master Plan (2015)*, and the *City of Citrus Heights Pedestrian Master Plan (2016)*.

In addition, Policy 29.2 allows the City Council to provide additional exceptions to the LOS E policy where physical mitigation is infeasible or would conflict with other community values, such as:

- Impacts on general safety, particularly pedestrian, bicycle, and transit safety.
- The right-of-way needs and the physical impacts on surrounding private or public properties.
- The visual aesthetics of the required improvement and its impact on community identity and character.
- Environmental impacts including air quality and noise impacts.
- Impacts on quality of life as perceived by residents.

Based on Policy 29.2 and these guidelines, identifying improvements for locations where roadway widening is prohibited includes the following key analysis steps:

- Identify locations where the project will not comply with the General Plan LOS policy.
- Consult with City staff to determine which mitigation option(s) should be applied.
- If actions to reduce project vehicle trips is one of the desired mitigation options, determine the number of project vehicle trips that are feasible to remove from the impacted location to reduce or mitigate the project impact. This step should be conducted in consultation with City staff.
- Identify vehicle trip reducing measures and quantify the trip reduction by referring to a study or source that confirms the vehicle trip reduction by citing or relying on substantial evidence.

8. RECOMMENDED PROCESS AND DOCUMENTATION

The transportation impact analysis shall be performed under the supervision of either a professional traffic or civil engineer, or a certified professional planner specializing in transportation. It is recommended that the work occur in a phased manner and seek City acceptance before initiating the next task.

- **Transportation Study Scope of Work** detailing project description, site location, analysis method, area-wide assumptions, study intersections and/or roadways, peak hours for analysis, and traffic data collection.
- **Project Trip Generation and Trip Distribution** documenting all key technical assumptions, data sources, and references.
- **Administrative Draft Transportation Study Report** prepared according to the Scope of Work, Project Trip Generation, and Trip Distribution approved by the City.
 - As discussed in Section 1 of these guidelines, the type of project and environmental review will dictate the format of this report. Refer to Section 1 for details.
- **Draft Transportation Study Report** addressing the City's comments on the Administrative Draft Report (see Appendix C for recommended outline).
- **Final Transportation Study Report / Response to Public Comments** addressing comments from the City, Caltrans, neighboring jurisdictions, etc.

For small projects that require a limited scope of study, it may be suitable to prepare a Technical Memorandum in lieu of a full report.

Appendix A: Sample Trip Generation Calculations

The following table exemplifies how trip generation information and assumptions should be prepared and documented for submittal to the City of Citrus Heights.

TABLE A-1: SAMPLE ESTIMATED PROJECT TRIP GENERATION

Land Use	Size	Unit	Daily		Trip Rates						Trips					
			Rate	Trips	AM Peak Hour			PM Peak Hour			AM Peak Hour			PM Peak Hour		
					In	Out	Total	In	Out	Total	In	Out	Total	In	Out	Total
Single-Family ¹	200	du	9.44	1,888	0.18	0.56	0.74	0.62	0.37	0.99	37	111	148	125	73	198
Commercial ²	100	ksf	60.12	6,012	1.25	0.77	2.02	2.60	2.83	5.43	125	77	202	260	283	543

Notes: du = dwelling units; ksf = 1,000 square-feet

1. Trip generation based on *Trip Generation, 10th Edition* (Institute of Transportation Engineers ITE, 2017) weighted average trip rates for Single-Family Detached Housing (Land Use Code 210)
2. Trip generation based on *Trip Generation, 10th Edition* (Institute of Transportation Engineers ITE, 2017) fitted curve equations for Shopping Center (Land Use Code 820):
 Daily : $\ln(T) = 0.68 \ln(X) + 5.57$ (50% Inbound, 50% Outbound)
 AM Peak Hour: $T = 0.5X + 151.78$ (62% Inbound, 38% Outbound)
 PM Peak Hour: $\ln(T) = 0.74 \ln(X) + 2.89$ (48% Inbound, 52% Outbound)
 Where: T = trips generated, X = 1,000 square-feet, Ln = natural log

Additional Notes:

- Survey data or the most recent version of ITE should be used to calculate trip generation.
- Pass-by reductions should also be considered for commercial uses where applicable.
- For mixed use developments, an internalization estimate should be included based on proven methods or models such as the US EPA MXD model, MXD+ model developed by Fehr & Peers, or other tools that are supported by substantial evidence.

Appendix B: Sample Site Plan Review

Sunrise Tomorrow Specific Plan

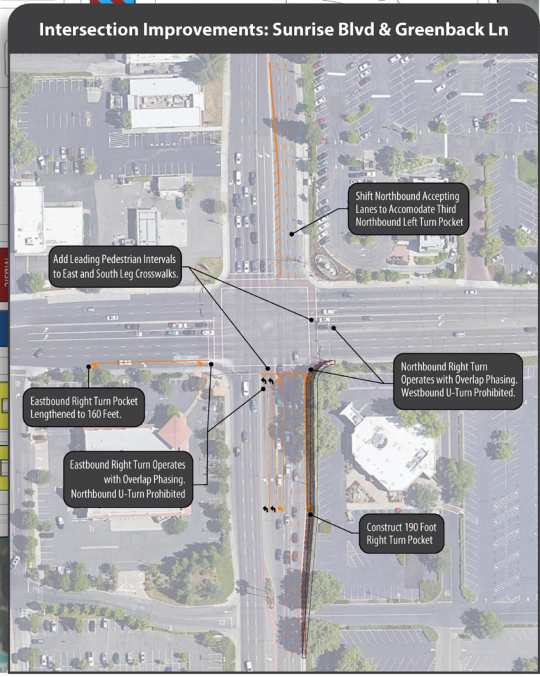
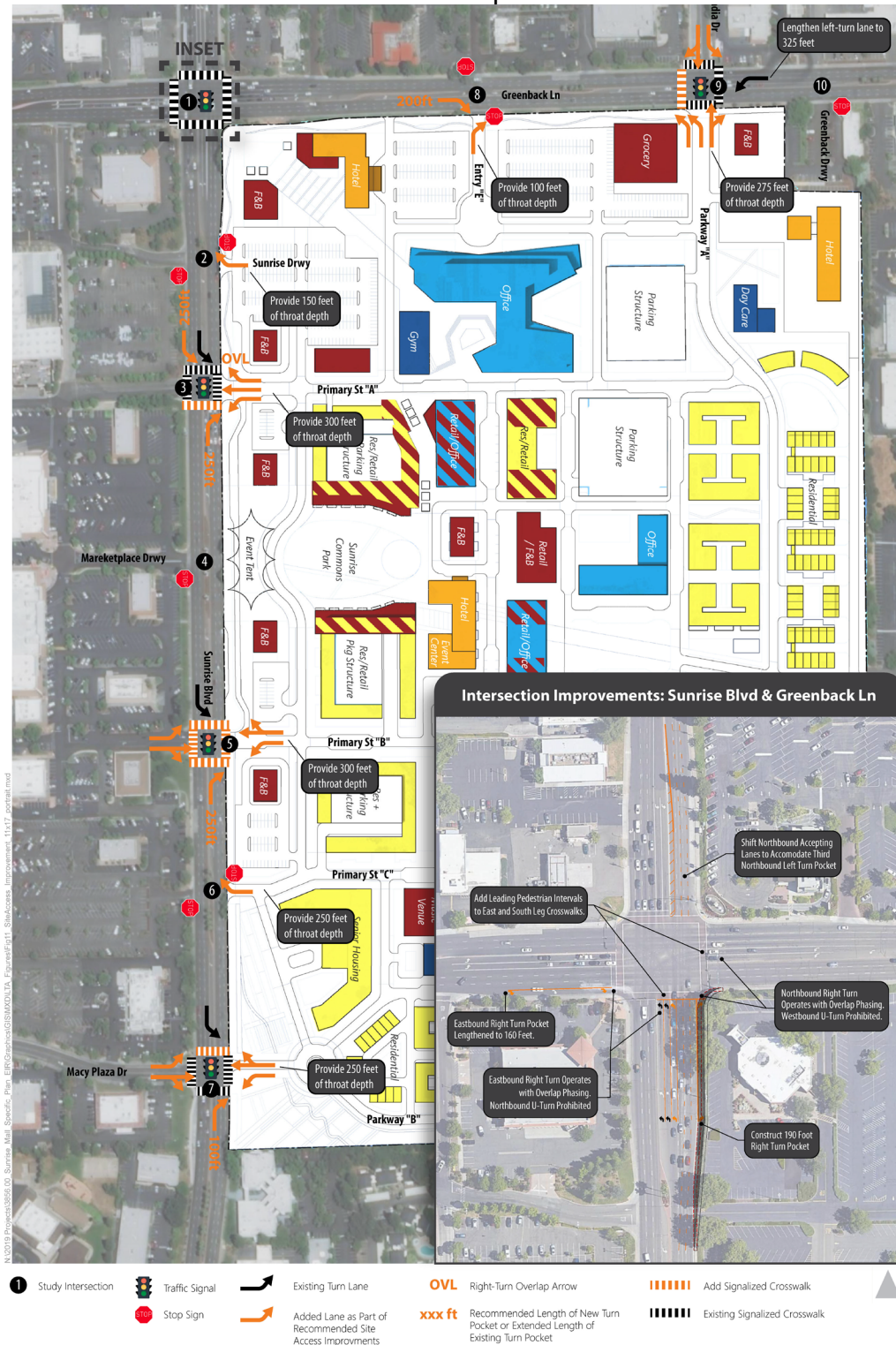
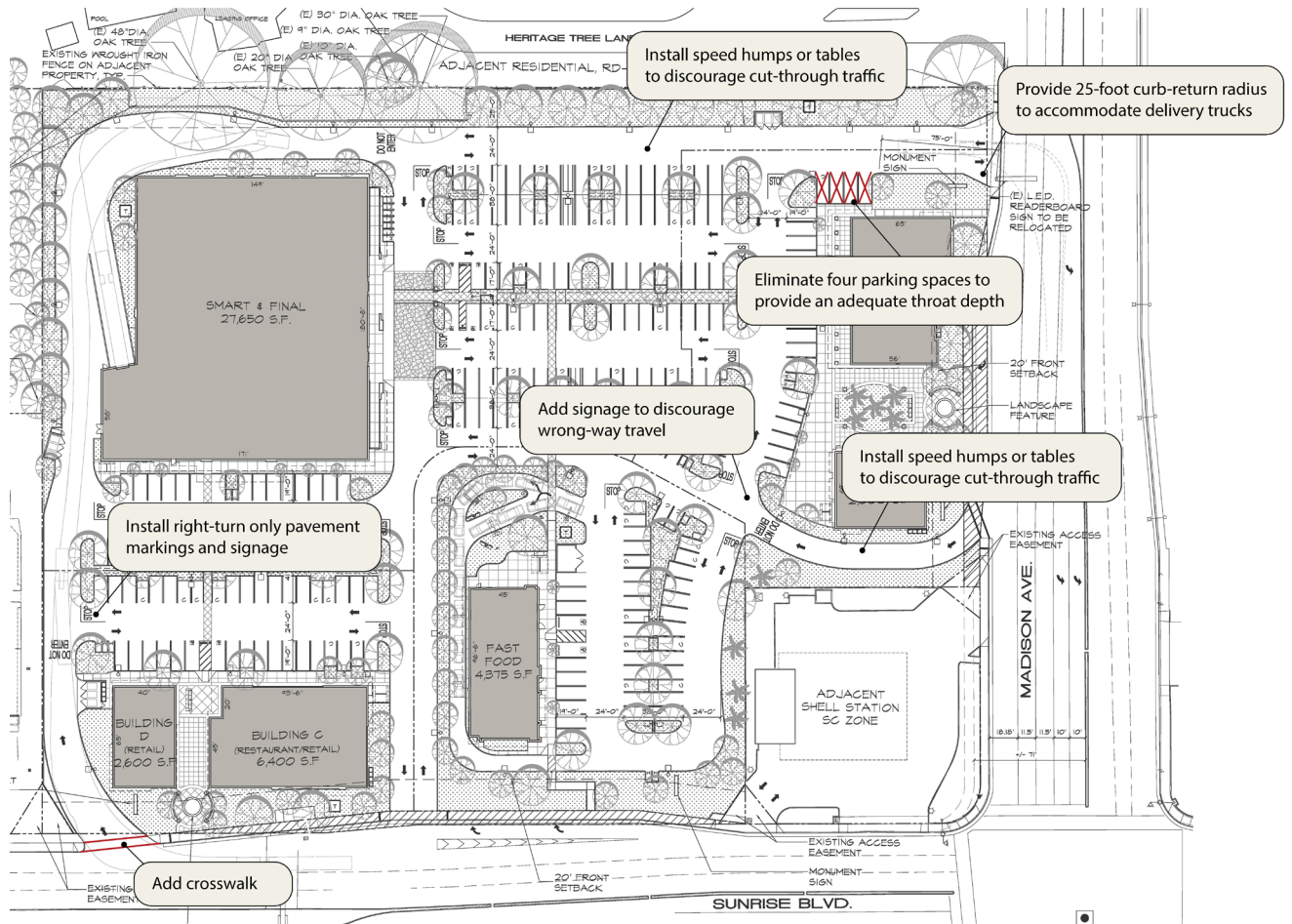


Figure 11
Project Site Recommendations

Retail Center in Northeast Quadrant of Sunrise/Madison Intersection



RECOI

Appendix C:

TIS Report Format Outline

1. Introductory Items

- Front Cover/Title Page
- Table of Contents, List of Figures, and List of Tables
- Executive Summary

2. Introduction/Background

- Project description
- Type and size of development
- Site plan (include proposed driveways, roadways, traffic control, parking facilities, emergency vehicle access, and internal circulation for vehicles, bicyclists, and pedestrians)
- Location map (include major streets, study intersections, and neighboring zoning and land uses)

3. Baseline Conditions

- Roadway system within project site and surrounding area
- Location and routes of nearest public transit system serving the project
- Location and routes of nearest pedestrian and bicycle facilities serving the project
- Figure of study intersections with peak hour turning movement counts, lane geometries, and traffic control
- Map of study area showing ADT of study roadways
- Table of baseline peak hour average vehicle delay and LOS

4. Baseline Plus Project Conditions

- Table of trip generation for project
- Figure/map of trip distribution (in percent)
- Maps of study area with applicable peak hour turning movements (Project Only and Baseline Plus Project)
- Table of Baseline and Baseline Plus Project intersection peak hour average vehicle delay and LOS
- Traffic signal and other warrants
- Findings of project impacts
- Mitigation measures for project impacts (include a map showing physical mitigation)
- Scheduling and implementation responsibility of mitigation measures
- Impacts of mitigation measures

5. Cumulative and Cumulative Plus Project Conditions

- Map of study area with Cumulative No Project peak hour turning movements
- Map of study area with Cumulative Plus Project peak hour turning movements
- Table of Cumulative and Cumulative Plus Project intersection peak hour average vehicle delay and LOS
- Traffic signal and other warrants
- Findings of project impacts
- Mitigation measures for project impacts (include a map showing physical mitigation)
- Scheduling and implementation responsibility of mitigation measures
- Impacts of mitigation measures

6. Construction Impacts

7. Phasing Impacts (for large projects only)

8. Appendices

- List of references
- Traffic counts
- Technical calculations for all analyses

SB 743 Implementation Guidelines for City of Citrus Heights

February 2021

Prepared By:

FEHR  PEERS

Prepared for:



I. INTRODUCTION

This report presents recommendations for implementing Senate Bill (SB) 743 in the City of Citrus Heights.

Report Organization

This report is organized into the following chapters:

- *Chapter I (Introduction)* – describes background information on SB 743, relevant CEQA Guidelines, and a simple definition of Vehicle Miles Traveled (VMT).
- *Chapter II (Citrus Heights VMT Characteristics)* – describes the process undertaken to quantify the VMT of residents, workers, and visitors within the City.
- *Chapter III (Proposed VMT Thresholds of Significance for Land Use Projects)* – presents specific thresholds of significance the City may consider using when evaluating land use projects under CEQA including project types and locations that are presumed to cause less than significant impacts.
- *Chapter IV (Proposed VMT Thresholds of Significance for Transportation Projects)* – describes the process the City may consider taking when evaluating the VMT impacts associated with transportation projects.
- *Chapter V (Land Use Project Case Studies)* – presents a case study that demonstrate how the VMT impacts of land use projects (that do not fit neatly into an exempted area) may be evaluated under SB 743.
- *Chapter VI (Mitigation Measures and Plan Updates)* – discusses mitigation measure opportunities to offset the significance of significant land use or roadway project transportation impacts, and the need to update the significance criteria and VMT thresholds.

Background

On September 27, 2013, Governor Jerry Brown signed SB 743 into law and started a process intended to fundamentally change transportation impact analysis as part of CEQA compliance. These changes include elimination of *auto delay*, *level of service (LOS)*, and *other similar measures of vehicular capacity or traffic congestion* as a basis for determining significant impacts. The law directed the Governor's Office of Planning and Research (OPR) to update the CEQA Guidelines to include new criteria (e.g., metrics) for determining the significance of transportation impacts.

OPR selected VMT as the transportation impact metric, recommended its application statewide, and submitted updates to the CEQA Guidelines that were certified by the Natural Resources Agency in December 2018. The requirements of SB 743 became effective statewide on July 1, 2020.

To help aid lead agencies with SB 743 implementation, OPR produced the [*Technical Advisory on Evaluating Transportation Impacts in CEQA*](#) (December 2018). The *Technical Advisory* helps lead agencies think about the variety of implementation questions they face with respect to shifting to a VMT metric. The guidance is not a recipe for SB 743 implementation since lead agencies must still make their own specific decisions about methodology, thresholds, and mitigation.

OPR hosted a series of webinars in Spring 2020, in which they provided verbal interpretations and clarifications of the *Technical Advisory*. Fehr & Peers regularly attends these webinars and notes these staff interpretations such that their latest guidance is reflected in reports such as this.

Intent of SB 743

The following two legislative intent statements are contained in the SB 743 statute:

- 1) Ensure that the environmental impacts of traffic, such as noise, air pollution, and safety concerns, continue to be properly addressed and mitigated through the CEQA.
- 2) More appropriately balance the needs of congestion management with statewide goals related to infill development, promotion of public health through active transportation, and reduction of greenhouse gas emissions.

These statements are important because they provide direction to OPR and to lead agencies. For OPR, the direction is largely about what the new metrics should achieve. For lead agencies, the direction is about expected changes in transportation analysis plus what factors to consider for significance thresholds.

SB 743 does not prevent a city or county from continuing to analyze delay or LOS as part of other plans (i.e. the general plan), fee programs, or on-going network monitoring, but these metrics will not form a determination of significant impacts under CEQA. Cities or counties can still use vehicle LOS outside of the CEQA process if they determine it is an important part of their transportation analysis process. The most common applications will likely occur for jurisdictions wanting to use vehicle LOS to size roadways in their general plan or determine nexus relationships for their impact fee programs. Jurisdictions can also continue to condition projects to build transportation improvements through the entitlement process (i.e., conditions of approval) in a variety of ways, such as using general plan policy consistency findings.

Relevant CEQA Guidelines

This section presents the precise language (source: https://www.califaep.org/docs/2020_ceqa_book.pdf) contained in the most recent CEQA guidelines pertaining to this topic.

CEQA SECTION 15064.3 (DETERMINING THE SIGNIFICANCE OF TRANSPORTATION IMPACTS)

(a) Purpose.

This section describes specific considerations for evaluating a project's transportation impacts. Generally, vehicle miles traveled is the most appropriate measure of transportation impacts. For the purposes of this section, "vehicle miles traveled" refers to the amount and distance of automobile travel attributable to a project. Other relevant considerations may include the effects of the project on transit and non-motorized travel. Except as provided in subdivision (b)(2) below (regarding roadway capacity), a project's effect on automobile delay shall not constitute a significant environmental impact.

(b) Criteria for Analyzing Transportation Impacts.

- (1) Land Use Projects. Vehicle miles traveled exceeding an applicable threshold of significance may indicate a significant impact. Generally, projects within one-half mile of either an existing major transit stop or a stop along an existing high quality transit corridor should be presumed to cause a less than significant transportation impact. Projects that decrease vehicle miles traveled in the project area compared to existing conditions should be presumed to have a less than significant transportation impact.
- (2) Transportation Projects. Transportation projects that reduce, or have no impact on, vehicle miles traveled should be presumed to cause a less than significant transportation impact. For roadway capacity projects, agencies have discretion to determine the appropriate measure of transportation impact consistent with CEQA and other applicable requirements. To the extent that such impacts have already been adequately addressed at a programmatic level, such as in a regional transportation plan EIR, a lead agency may tier from that analysis as provided in Section 15152.
- (3) Qualitative Analysis. If existing models or methods are not available to estimate the vehicle miles traveled for the particular project being considered, a lead agency may analyze the project's vehicle miles traveled qualitatively. Such a qualitative analysis would evaluate factors such as the availability of transit, proximity to other destinations, etc. For many projects, a qualitative analysis of construction traffic may be appropriate.
- (4) Methodology. A lead agency has discretion to choose the most appropriate methodology to evaluate a project's vehicle miles traveled, including whether to express the change in absolute terms, per capita, per household or in any other measure. A lead agency may use models to estimate a project's vehicle miles traveled, and may revise those estimates to reflect professional judgment based on substantial evidence. Any assumptions used to estimate vehicle miles traveled and any revisions to model outputs should be documented and explained in the environmental document prepared for the project. The standard of adequacy in Section 15151 shall apply to the analysis described in this section.

Section 15064.3 defines VMT as “the amount and distance of automobile travel attributable to a project”. It describes certain conditions (e.g., proximity to a transit stop) for land use projects that should be presumed to cause a less than significant transportation impact. It concludes that projects that decrease VMT compared to existing conditions should be presumed to have a less than significant transportation impact. Lastly, it establishes that agencies have discretion to choose the most appropriate methodology to evaluate a project’s VMT provided that documentation and explanations are offered for methods and assumptions that were used in the estimation.

CEQA GUIDELINES SECTION 15064.7 (THRESHOLDS OF SIGNIFICANCE)

This section encourages public agencies to develop and publish thresholds of significance to be used in determining the significance of environmental effects.

- (a) A threshold of significance is an identifiable quantitative, qualitative or performance level of a particular environmental effect, non-compliance with which means the effect will normally be determined to be significant by the agency and compliance with which means the effect normally will be determined to be less than significant.
- (b) Each public agency is encouraged to develop and publish thresholds of significance that the agency uses in the determination of the significance of environmental effects. Thresholds of significance to be adopted for general use as part of the lead agency’s environmental review process must be adopted by ordinance, resolution, rule, or regulation, and developed through a public review process and be supported by substantial evidence. Lead agencies may also use thresholds on a case-by-case basis as provided in Section 15064(b)(2).

Technical Advisory on Evaluating Transportation Impacts in CEQA

The 26-page *Technical Advisory* provides guidance for how professional planners and CEQA practitioners should approach SB 743 implementation including recommendations regarding assessment of VMT, thresholds of significance, and mitigation measures.

Page 1 of the document states the following:

- The *Technical Advisory* does not alter lead agency discretion in preparing environmental documents subject to CEQA.
- The *Technical Advisory* should not be construed as legal advice.
- OPR is not enforcing or attempting to enforce any part of the recommendations.

Given the length, technical depth, and wide range of topics addressed in the *Technical Advisory*, it is not summarized here. However, it is cited frequently in the following chapters.

VMT Basics

This subsection presents a high level overview of what VMT is and what it is not.

1. By definition, one (1) VMT is defined as one mile driven by a vehicle (regardless of the number of occupants).
2. VMT is commonly expressed as a daily value (in miles).
3. Consistent with the *Technical Advisory*, VMT estimates are not truncated at political boundaries.
4. While VMT is a useful metric for quantifying the efficiency of a given mix of land uses and roadway network enhancements, it is not a direct measure of congestion or delay.

The following link provides a brief instructional video that defines typical trip purposes and how they translate into VMT: <https://youtu.be/UE4TJItVdJ8>.

II. CITRUS HEIGHTS VMT CHARACTERISTICS

This chapter describes the data collection process, analysis, and findings of an effort focused on quantifying the VMT of residents, workers, and visitors within Citrus Heights.

Rationale for Technical Approach

At the outset of this project, Fehr & Peers and City staff met to discuss the benefits and drawbacks of two distinct options for analyzing VMT within the City. Since the City does not have its own travel demand model, the two options are to rely on SACOG's model¹ or utilize big data.

Reliance on SACOG's model would mean several things including a choice by the City to attach itself to the modeling framework, which has both benefits and drawbacks. A major benefit would be consistency with SACOG modeling approaches. But a major drawback is the model itself². Selection of the model would mean that the City would be anticipating consultants will apply the model on projects within the city, which would increase the cost and schedules to complete studies and limit the number of consultants who could prepare such studies given the model's complexity.

What is "Big Data"?

"Big Data" is used in a variety of disciplines such as retail/market studies, transportation, and economics. It uses anonymous location records from smart phones and in-vehicle navigation devices to understand travel behaviors, patterns, and user types.

Alternatively, reliance on big data (described to the right and in detail below) would link the City to this form of data/analysis and its lesser-known results. Although big data has been used for countless study purposes throughout the nation and beyond for over a decade, its use on this specific type of project has been relatively limited. However, the upside of this approach is

¹ The Sacramento Area Council of Governments (SACOGs) model is known as SACSIM, which is an activity-based model that estimates travel behavior throughout the entire region. It was used for SACOG's recently adopted 2040 MTP/SCS. The model is very sophisticated, requiring analysts to spend dozens of hours in training to understand how it works. The model is also quite time-intensive, requiring at least two days of staff time to make a simple land use change and then requiring another two days for the model to run on a high-speed computer. Since the model covers the entire SACOG six-county region, it does not contain local level granularity. Within Citrus Heights, it consists of 27 traffic analysis zones (TAZs), whereas a locally-calibrated model would have ten times as many zones.

² While the model has been proven to generate reasonable results under many circumstances, there are also cases where its VMT predictions do not match expectations or other data sources (e.g., Census data, California Household Travel Survey). More on these comparisons can be found at: <https://www.streetlightdata.com/sb-743-vmt-solutions/>

considerable: a very large dataset, greater geographic disaggregation within the City, and less time and costs to analyze future projects.

Upon weighing the pros and cons of each approach, the City and Fehr & Peers jointly decided that the big data option would be the most appropriate approach for the City. The chosen data source was Streetlight Data, Inc, whose data collection and processing are described in the following section.

Overview of StreetLight Data

StreetLight Data combines Location-Based Services (LBS) data with complex machine learning algorithms to understand travel behavior across the country³. However, for SB 743 purposes, a much more straightforward dataset is needed, which can be summarized as follows:

- Average VMT per device for Residents, Workers, and Visitors, disaggregated to the Census Block Group (CBG) level within the City of Citrus Heights.

Frequently Asked Questions (FAQs)

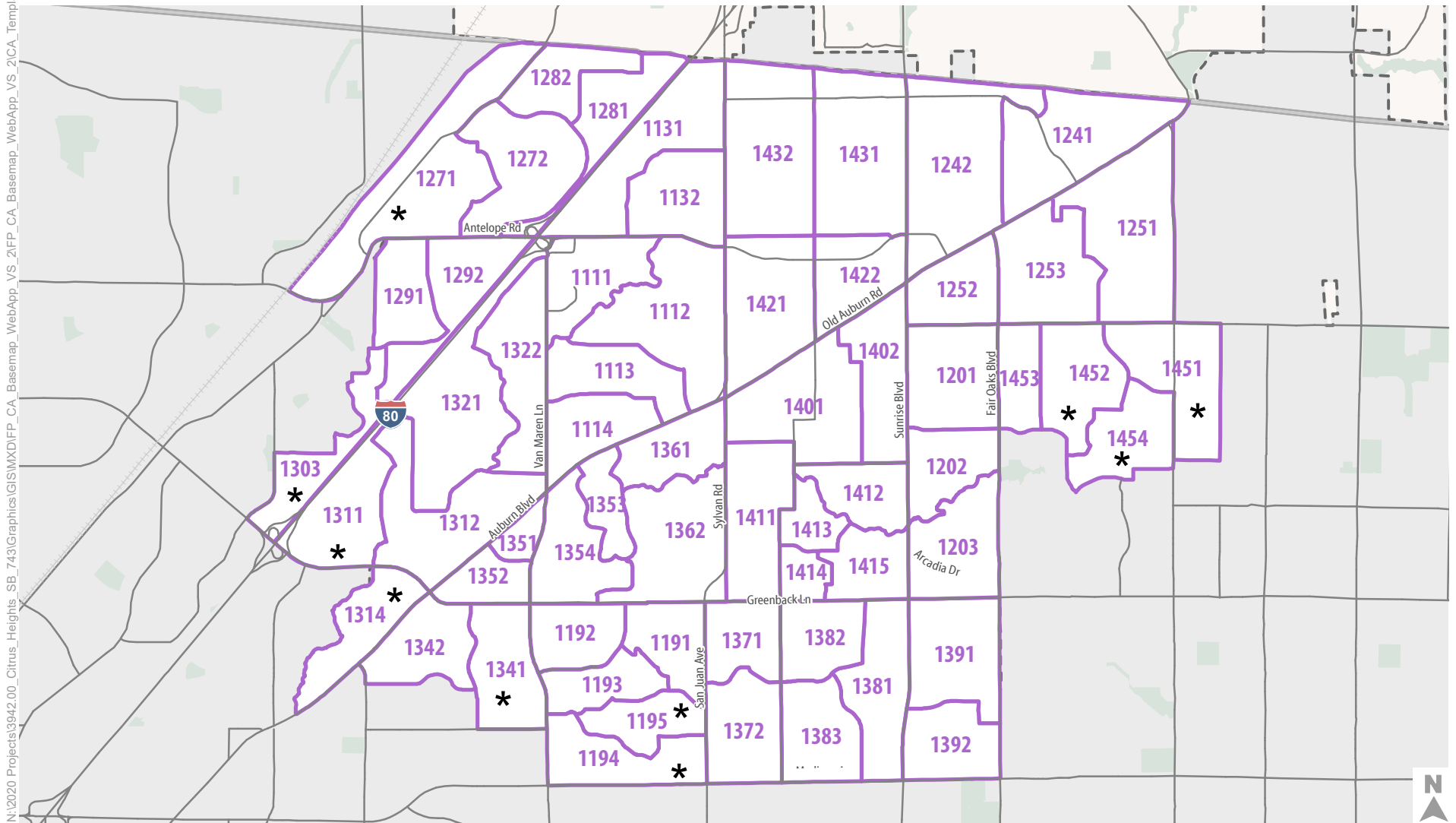
- ❖ ***What time periods do the data cover?*** For the Resident and Worker datasets, the time period covered is all Tuesdays, Wednesdays, and Thursdays of 2019. Reported results are the average of all days. For the Visitor dataset, reported results are based on the average of monthly averages for all 12 months in 2019.
- ❖ ***How is/was the start/end location of a trip determined?*** The trip starts (based on location records) once a device is traveling at a reasonable speed and ends once the device has not moved 5 meters within 5 minutes.
- ❖ ***How is the length of a trip determined?*** Trip length is calculated based on LBS data from GPS satellites, with the trip length based on the most likely route taken.
- ❖ ***How is a "Resident" device determined?*** Within a given CBG, a device is deemed to be associated with a resident if it is in the same location during most overnight hours.




³ StreetLight Data processes approximately 40 billion anonymized location records per month from smart phones and navigation devices in connected cars and trucks and uses machine learning to transform these records into aggregated and normalized route-based travel patterns. Data is validated using permanent traffic counters and embedded sensors, and normalized with multiple data sources, including parcel data, digital road network data, and census information. This data is used in many different disciplines ranging from retail, economics, transportation, etc. In 2019-2020, Fehr & Peers used Streetlight Data to analyze travel behavior at and surrounding Sunrise Mall as part of the Sunrise Tomorrow Specific Plan.

- ❖ **How is a “Worker” device determined?** Within a given CBG, a device is deemed to be associated with a worker if it is in the same location during most of the device’s reported weekday hours.⁴
- ❖ **How is a “Visitor” device determined?** A device that is recorded as traveling to a given CBG that is otherwise not classified as either a Resident or Worker device is then classified as a Visitor device.
- ❖ **What is a Census Block Group and how many are in Citrus Heights?** A Census Block Group is one of several forms of geographic classification used by the US Census.⁵ Citrus Heights has 61 CBGs (see **Figure 1**). Within the City, they average about 150 acres in size and contain about 1,400 persons.
- ❖ **What type(s) of VMT are counted?** Resident VMT considers all trips that have a trip origin or destination at a residence. Worker VMT considers strictly those trips that travel between the worksite and home (i.e., a stop at the gym or gas station on the way home would result in that trip being excluded). Visitor VMT includes all trips that have a trip origin or destination within the CBG.
- ❖ **How is it determined whether the VMT is occurring in a vehicle versus another mode of travel (e.g., bus or biking)?** Streetlight Data has internal procedures built into their calculation processes to flag these different modes of travel (e.g., slow walking trips, systematic starts/stops associated with bus/rail vehicles). Because Citrus Heights does not have the type of diverse transit system found in a major city, this is not an issue for VMT in the city.
- ❖ **How are trips that start/end within the same CBG treated?** These trips are included, though they are typically quite short and represent a small percentage of overall trip-making.
- ❖ **Are there any known technical shortcomings of the data source?** Two potential areas of concerns were flagged. One pertains to a trip being double-counted if multiple passengers have apps in operation. The second pertains to household composition, specifically the effects of lesser versus greater ratios of devices per household member. Holding all else constant, a family of four with two driving age children, versus a family of four with no driving age children will have different VMT per device characteristics. However, as noted later, the VMT metrics are comparative in nature; thus, these issues are present in both the local data as well as the SACOG region data to which the local data is compared. After discussing this with Streetlight Data, it was concluded that no adjustments to the data were warranted to address these items given the comparative nature of the evaluation.

⁴ Streetlight Data, Inc. employed a process whereby persons that typically work at a worksite most weekdays for at least four to six hours are included in the dataset. Special care was taken to not accidentally classify students in this dataset (by removing middle and high schools from the data acquisition process).

⁵ Each CBG has an 11-digit code associated with it. Within Citrus Heights, the first seven digits of all CBGs are identical, and thus all CBG references moving forward are made only to the last four digits which vary.



-  County Boundaries
-  City Boundary
-  Major Roads

* = CBG is comprised of land located both within Citrus Heights and unincorporated Sacramento County or Roseville. Maps that follow show only portions of these CBGs located within the City limits

Figure 1



- ❖ **How do we know that the Streetlight Data is accurate?** This is a very important question, and one whose answer cannot be summarized in a single paragraph. The next section presents the Streetlight Data database and evaluates its summarized VMT estimates against various local data sources for reasonableness.

Citrus Heights VMT Characteristics

Appendix A contains the Streetlight dataset. **Table 1** compares the Citywide averages (weighted by number of residents and workers in each CBG) against SACOG regional averages.

Table 1: Summarized VMT Results for Citrus Heights		
Traveler Type	Weighted Average VMT (Citrus Heights) ¹	SACOG Region Average
Residents	17.4 miles per day	20.1 miles per day
Workers	12.8 miles per day	18.1 miles per day
Visitors	16 miles per day (relational) ²	20 miles per trip (relational) ²
Note: ¹ Weighted average is among all CBGs based on relative number of residents and workers within each CBG. ² Visitors do not travel to a given CBG every day. Thus, a calculation of a visitor device VMT would reflect their frequency of visits, which is small when represented on an average daily basis. To provide a more intuitive statistic, the daily VMT per device was factored up such that the average SACOG region visitor made two daily trips (one inbound and one outbound). The results for each Citrus Heights CBG were then calculated by multiplying the average trip length to/from the CBG by the factored visitation frequency of the specific CBG.		

The following evaluation was performed to assess the reasonableness of the SACOG region results against other published data sources. If the SACOG region results are reasonable, then it follows that the VMT data for the City would also be reasonable since it is based on the same data source and calculation procedures. Following are the results of this evaluation:

Resident VMT Considerations

1. The *Sacramento Region Transportation Study Analysis Report* (RSG, 2018) surveyed over 4,000 households in Spring 2018 and found that the SACOG region averaged 23.9 miles of vehicle travel per day per person (calculated based on the total household size). The Streetlight Data follows a similar methodology because vehicle trips made on behalf of persons who cannot drive are accounted for in another person's (e.g., parent) device. The following conclusions are drawn based on this data:
 - Results in Table 1 represent averaged conditions on Tuesdays, Wednesdays, and Thursdays throughout 2019 whereas the SACOG survey reflects only conditions with school in session.

- The data in Table 1 only include home-based trips, while the SACOG survey also considers non-home-based (e.g., from the office to the gym) travel, which adds more VMT.
 - Based on these considerations (i.e., Streetlight Data excludes non-home-based travel, and dampens the VMT effects of school-related travel by considering months in which school is not in session), **it is reasonable for Streetlight's VMT per Resident estimate to be 16 percent less than the SACOG result.**

Worker VMT Considerations

2. The California Household Travel Survey (CHTS) contained results that may be used to compare Worker VMT estimates. That survey, which was performed in 2012, found that home-based-work travel in the SACOG region generated an average of 13.2 VMT per worker, which is 27 percent less than Streetlight's SACOG region average of 18.1 VMT. The CHTS estimate represented conditions in 2012 near the end of the Great Recession. Travel behaviors have changed considerably since 2012, as evidenced by the following:
 - Longer distance travel for work has become more common. The 2018 SACOG survey indicated that 11 percent of Yolo County and 6 percent of El Dorado County work trips were destined to the Bay Area, a considerable distance from Sacramento. ⁶
 - Some short/medium distance work trips likely shifted to a Transportation Network Company (TNC) such as Uber or Lyft, which were not present in 2012. This effectively reduces the weighting effect of shorter distance trips, thereby causing average trip lengths and VMT to increase.
 - **The above factors explain why it is reasonable for Streetlight's SACOG region average of about 18 miles per day of travel per worker to be marginally greater than the 2012 CHTS estimate for the region.**

Visitor VMT Considerations

3. The Streetlight Data revealed an average SACOG region trip length of 10 miles per visitor trip. This is somewhat greater than the 2012 CHTS estimate of about 7 miles per trip⁷. This may be explained by the relatively infrequent, but much longer distance trips to a variety of destinations within the region such as gambling venues, cemeteries, ski resorts, golf courses, wine tasting, and various recreational destinations (e.g., lakes, campgrounds, etc.). Because the SACOG survey only posed questions to

⁶ It takes only a small percentage of 100+ mile commute trips from the SACOG region to the Bay Area to meaningfully increase the average VMT per worker. This trend is very common when reviewing home-based work trip length datasets – the average trip length is often considerably greater than the median due to this effect.

⁷ Based on the average of home-based-other and non-home-based trips from the 2012 CHTS for the SACOG region.

SACOG residents, it did not take into consideration a visitor who traveled from outside the region for these recreational or business purposes (which are reflected in the Streetlight Data results).

- **Thus, it is expected that the Streetlight Data average Visitor VMT estimate in Table 1 would be somewhat greater than the CHTS estimate for the above reasons.**

Readers interested in knowing more about Streetlight Data and Fehr & Peers' local validation of it can follow the link below to find a white paper on the topic: <https://learn.streetlightdata.com/sb-743-metric-methodology-validation>.

Table 2 illustrates the range CBG-specific metrics for each traveler type. Appendix A shows specific VMT values for all CBGs in Citrus Heights.

Table 2: Summarized VMT Results for Citrus Heights			
Metric	Resident VMT	Worker VMT	Visitor VMT
Minimum Value	12.3 ¹	6.9 ³	5.9 ⁴
Maximum Value	23.3 ²	24.8 ³	28.2 ⁴
Median Value	17.0	11.4	10.5
Average Value	17.4	12.8	16.0
SACOG Region Average	20.1	18.1	20.0
Percentage of CBGs less than SACOG Average	87%	92%	97%
Percentage of CBGs at 85% or less of SACOG Average	52%	74%	87%
<p>Note:</p> <p>¹ For this CBG (1372), 39% of households make \$35,000 or less per year, average household size is 2.11, and average auto ownership is 1.71.</p> <p>² For this CBG (1322), 17% of households make \$35,000 or less per year, average household size is 3.28, and average auto ownership is 2.17.</p> <p>³ Both CBGs consists of less than 20 employees according to LEHD. Results appear to be outliers based on small sample size.</p> <p>⁴ The minimum value CBG is located in the southwest corner of Greenback Lane/San Juan Avenue and features a number of local-serving stores. In contrast, the maximum value CBG is comprised of a cemetery, which draws visitors from considerable distances.</p>			

III. PROPOSED VMT THRESHOLDS OF SIGNIFICANCE FOR LAND USE PROJECTS

This chapter presents the thresholds of significance pertaining to VMT that Citrus Heights will apply when analyzing the transportation impacts of land use projects that are subject to CEQA. Projects that are exempt from CEQA or require only ministerial actions by staff are not subject to the evaluations described below. SB 743 does not preclude analyses of a land use project's potential impacts to bicycle/pedestrian facilities, transit, construction, emergency access, nonstandard design features, etc. within CEQA.

Efficiency Threshold

Page 10 of the *Technical Advisory* states that OPR recommends that a per capita or per employee VMT that is 15 percent below that of existing development may be a reasonable threshold. Lacking any other information that would suggest a different threshold value should be applied, the City of Citrus Heights has concluded that this threshold should be applied for land use projects in the City.

Project Screening

The *Technical Advisory* offers guidance regarding land use projects that are presumed to be less-than-significant. Five such project types are presented below.

1. Small Projects – The *Technical Advisory* concludes that, absent any information to the contrary, projects that generate 110 trips per day or less may be assumed to cause a less-than-significant transportation impact.

Evaluation: According to Table 2, the average VMT per resident in Citrus Heights is 17.4 miles per day. According to the US Census⁸, the City has an average household size of 2.54 persons, which corresponds to 44.2 VMT per household or dwelling unit. Data from the Streetlight database shows that the average home-based trip length in Citrus Heights is 8.8 miles. By dividing VMT by average trip length, an average trip generation rate of 5.0 daily trips per dwelling unit is calculated.

A set of mathematical calculations were performed based on 67 percent of all units in the City being single-family, and the data from ITE's *Trip Generation Manual* indicating that multi-family units, on average, generate 42 percent fewer trips than single-family units. These calculations result in average trip rates of 5.80 daily trips for single-family units and 3.37 daily trips per multi-

⁸ Accessed (on December 30, 2020) at:
<https://www.census.gov/quickfacts/fact/table/citrusheightscitycalifornia/PST045219>

family units. Thus, the *Technical Advisory* small project threshold of 110 daily trips would correspond to 19 single-family dwelling units or 32 multi-family dwelling units in Citrus Heights⁹.

The *Technical Advisory* references 10,000 square feet of office space as generating no more than 110 daily trips, and thus concludes that VMT impacts for office buildings up to this size would generally be presumed to be less than significant. Table 2 indicates that the average VMT per worker in Citrus Heights is 29.3 percent less than the SACOG region average. Based on the reasonable premise that the OPR guidance was developed for a regional/statewide scale, the unique, lower travel characteristics of workers in Citrus Heights suggests a more appropriate threshold for office space in Citrus Heights would be 14,000 square feet¹⁰.

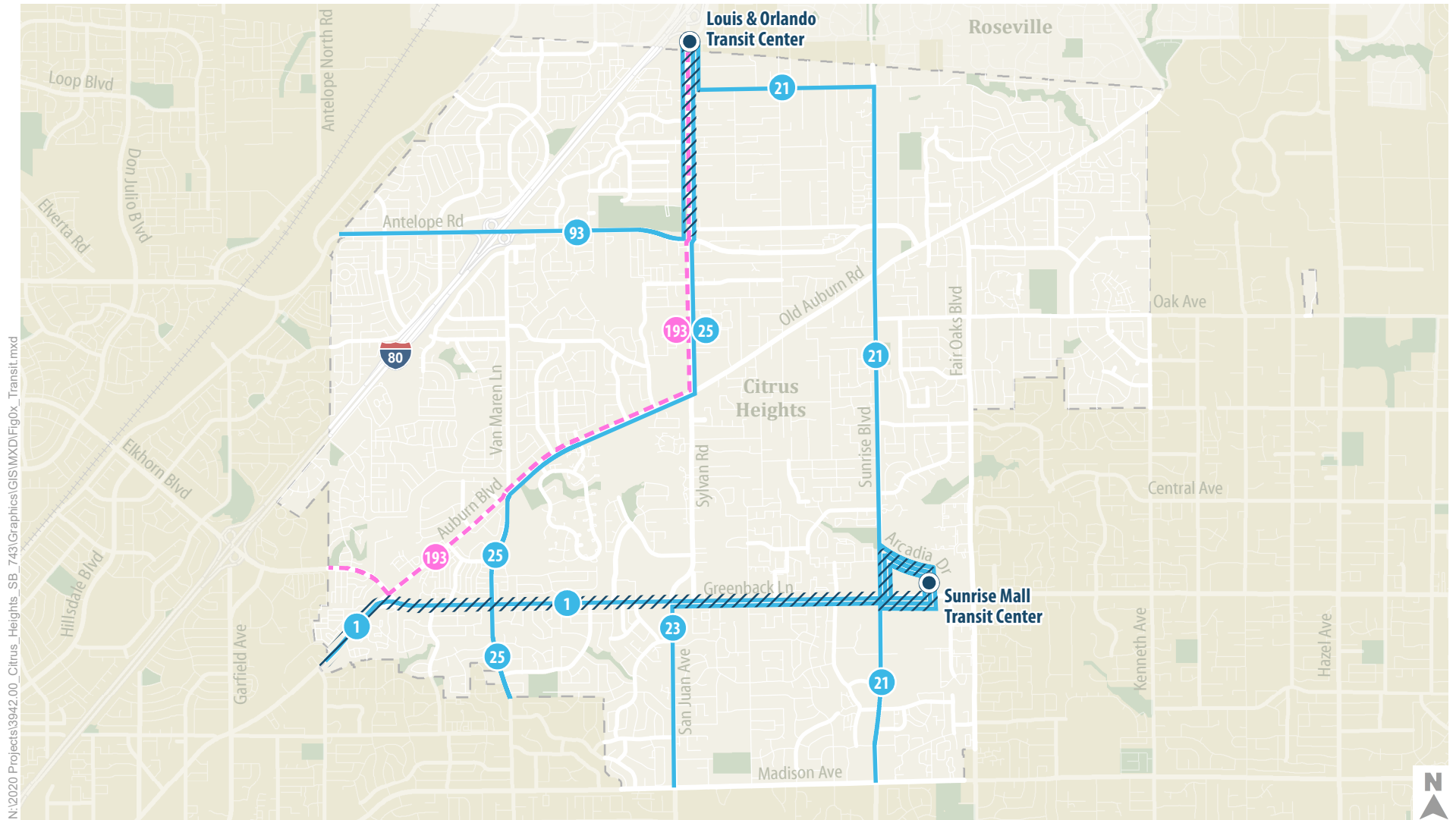
2. *Projects near Transit Stations* – projects located within ½ mile of an “existing major transit stop” or an “existing stop along a high quality transit corridor”¹¹ would have a less-than-significant impact on VMT.

Evaluation: **Figure 2** shows existing transit service and specific corridors that qualify as high-quality transit corridors. In later figures, a one-mile bandwidth along each high-quality transit corridor is shown so as to illustrate the geographic extent of the ½-mile maximum distance from each street centerline. Proposed land uses within these areas would be presumed to cause less-than-significant VMT impacts according to the *Technical Advisory* provided certain project design conditions are met. Citrus Heights staff has concluded that some, but not all of the project or site-specific guidance on Page 14 of the *Technical Advisory* is applicable to Citrus Heights given the infill nature of most new projects in the city and the small size of most remainder parcels (i.e., achieving a 0.75 FAR is unrealistic on most parcels and specific assumptions for those parcels may not have been made or known as part of the MTP/SCS). Thus, the City will require proposed projects being considered for an exemption of VMT impacts under this category to consist of transit supportive land uses, provide no more than 20 percent above the city parking code requirement, and to not replace affordable housing.

⁹ Calculated as: 110 daily trips / 5.80 Single-Family trips per unit = 18.96 (or 19 units).

¹⁰ Calculated as follows: 10,000 sq. ft. office x (1 / (1 - 29.3%)) = 14,144 sq. ft. of office

¹¹ According to Public Resources Code 21155, a high-quality transit corridor means fixed route bus service with service intervals of 15 minutes or less during peak commute hours (regardless of whether service is provided by a single route or multiple routes).



- Local Bus Service
- High Quality Transit Corridor
- Peak Hour Bus Service

Note: Based on current operating schedules of Sac RT, which were restored in Fall 2020 to pre-COVID levels.



Figure 2

3. *Affordable Residential Development* – projects consisting of a high percentage of affordable housing may be assumed to cause a less-than-significant transportation impact on VMT because they may improve jobs-housing balance and/or otherwise generate less VMT than market-based units.
Evaluation: According to the SACOG 2018 household survey¹², person trip rates were 18 percent higher among individuals in households making over \$50,000 per year versus less than \$50,000 per year. Additionally, as noted in the *Technical Advisory*, affordable housing is typically situated in close proximity to jobs, thereby reducing commute distances.
4. *Redevelopment Projects* – If a proposed redevelopment project leads to a net overall decrease in VMT (when compared against the VMT of the existing land uses), the project would lead to a less-than-significant transportation impact.
Evaluation: This is a generally reasonable conclusion. However, in most instances, redevelopment occurs on sites that may not be operating at optimal levels (e.g., an underperforming mall or strip retail center). Hence, a question that must be answered pertains to whether the comparison should be based on the existing VMT of the site or VMT of the site if operating at full capacity. When the City receives an application to redevelop a large existing property, a detailed evaluation of this topic should occur.^{13,14}
5. *Local Serving Retail* – Trip lengths may be shortened and VMT reduced by adding “local-serving” retail opportunities that improve retail destination proximity. Page 17 of the *Technical Advisory* generally describes retail development including stores less than 50,000 square feet as locally-serving. In May 2020, OPR staff indicated during online webinars that any retail building that is 50,000 square feet or less may be considered locally-serving.
Evaluation: Many agencies have followed this guidance, though it is noted that a 50,000 square foot retail center would typically generate about 5,000 VMT, which is more than five times that allowed under the Small Projects definition above.

¹² Found at: <https://www.sacog.org/post/2018-sacog-regional-household-travel-survey>

¹³ In the case of the proposed redevelopment of Sunrise Mall (i.e., Sunrise Tomorrow Specific Plan), its location in a VMT-efficient CBG, proximity to a high-quality transit corridor, and density led to a conclusion of a less-than-significant VMT impact without needing to broach the topic of being a redevelopment project.

¹⁴ For sites that have existing uses that would be redeveloped, the VMT comparison would consist of two steps. First, the daily trip generation of existing and proposed uses would be calculated based on ITE or other trip rates. Second, the VMT of the existing and proposed uses would be calculated by multiplying daily trips (by trip purpose type) by average trip length (using sources such as Streetlight data, CHTS data, or SACOG household survey).

Proposed VMT Thresholds of Significance – Residential Land Uses

The City of Citrus Heights will apply the following thresholds of significance when analyzing the VMT transportation impacts of residential land use projects under CEQA. The project would cause a significant impact if it would:

- Not qualify under one of the applicable exemption categories (small projects, projects near high-quality transit corridors, affordable housing, or redevelopment projects), and its VMT exceeded 85 percent of the regional per capita average. If the above conditions are met, the project's VMT impact could still be found to be less-than-significant if it did not cause the total VMT generated by the City of Citrus Heights to increase¹⁵.

Due to its built-out nature, proposed projects in Citrus Heights are located on infill sites that will always be much smaller when compared to the CBG to which it belongs. The City, like many other jurisdictions, may use a "blending/adjacency evaluation" that considers the relative placement of a project within a given CBG and the travel characteristics of adjacent CBGs containing similar land uses. Refer to Chapter V for a case study example of how this process works.

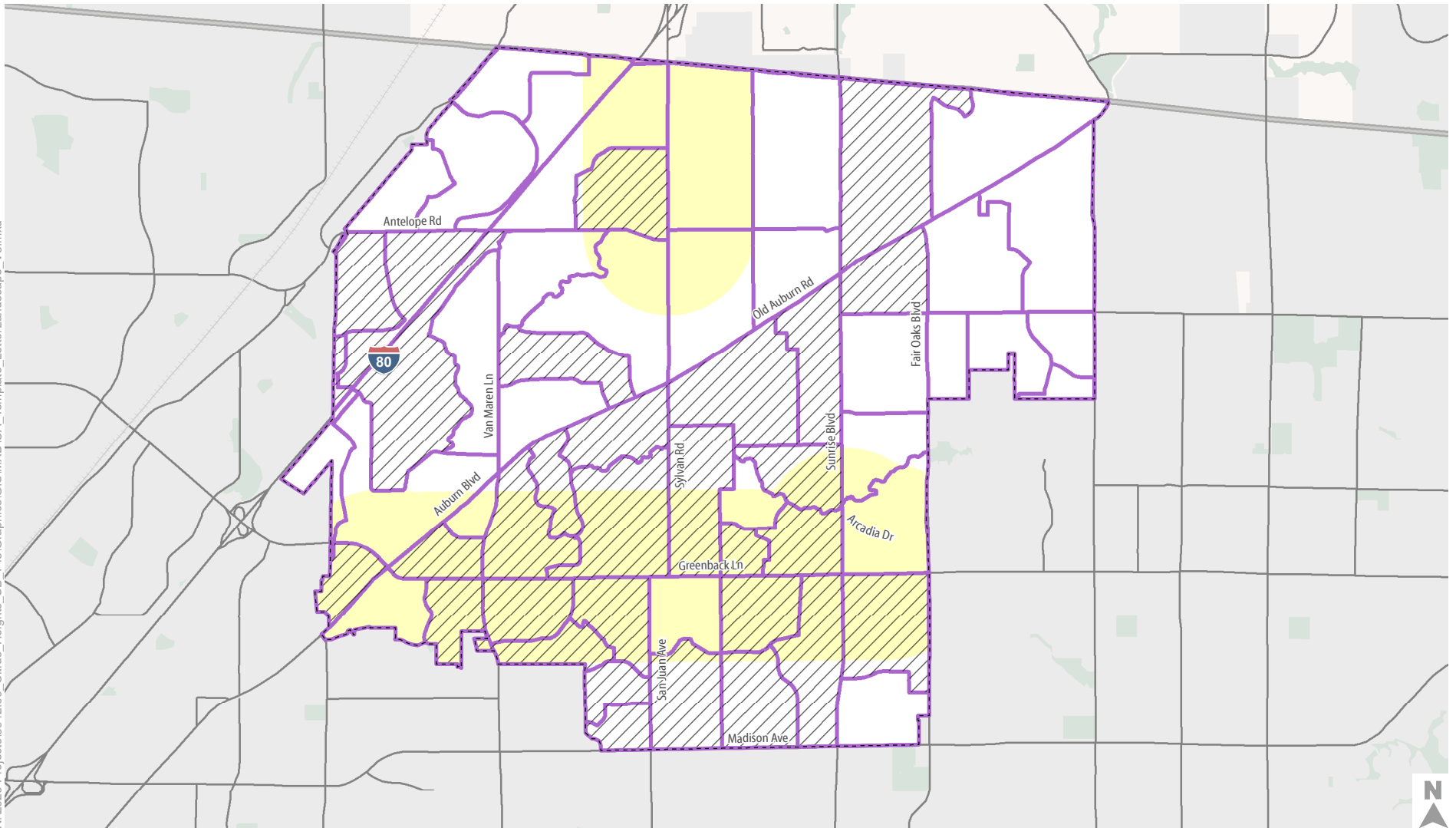
Figure 3 is a screening map of those CBGs that have average home-based VMT per resident values that are 85 percent or less of the SACOG regional average. Also overlaid on this figure are the City's existing high-quality transit corridors. Based on this figure, it can be inferred that 36 percent of CBGs in Citrus Heights have portions or all its block group areas that would not meet either the proximity to high-quality transit or screening map exemptions. However, 64 percent of all CBGs would be exempted.





The end of this chapter includes a checklist showing the potential exemptions for different land use projects.

Proposed VMT Thresholds of Significance – Employment Uses

The City of Citrus Heights will apply the following thresholds of significance when analyzing the VMT transportation impacts of employment-based land use projects under CEQA. This includes office buildings, medical-office, research-and-development, and similar uses whose purpose is primarily employment-related (versus a service business).

¹⁵ The determination of whether the proposed project would cause a net increase/decrease in citywide VMT may be based on output from either SACOG's SACSIM travel demand model or its prior SACMET model. However, such analyses are time-consuming and complicated to perform. Thus, such investigations should only be conducted when there is reason to believe a net reduction in VMT may occur.



-  Census Block Groups
-  Residential HBx VMT per device per day is 85% or less of SACOG average 20.1
-  High Quality Transit Corridor (1/2 mile buffer)
-  Citrus Heights City Limits

1. This figure serves as a screening map, which should be used with appropriate professional engineering judgment. Refer to report text for details.
2. Data (provided by Streetlight Data, Inc.) represents averaged travel conditions for all Tuesdays, Wednesdays, and Thursdays for the entirety of 2019.



Figure 3

Screening Map - Average Daily Home Based VMT per Resident

Agenda Packet Page 503

The project would cause a significant impact if it would:

- Not qualify under one of the applicable exemption categories (small projects, projects near high-quality transit corridors, or redevelopment projects), and its VMT exceeded 85 percent of the regional per employee average. If the above conditions are met, the project's VMT impact could still be found to be less-than-significant if it did not cause the total VMT generated by the City of Citrus Heights to increase.

Figure 4 is a screening map of those CBGs that have average home-to-work (and return) VMT per employee values that are 85 percent or less of the SACOG regional average. Also overlaid on this figure are the City's existing high-quality transit corridors. Based on this figure, it can be inferred that 13 percent of the CBGs in Citrus Heights have portions or all its block group area that would not meet either the proximity to high-quality transit or screening map exemptions. However, 87 percent of all CBGs would be exempted.

Proposed VMT Thresholds of Significance – Retail/Entertainment Uses

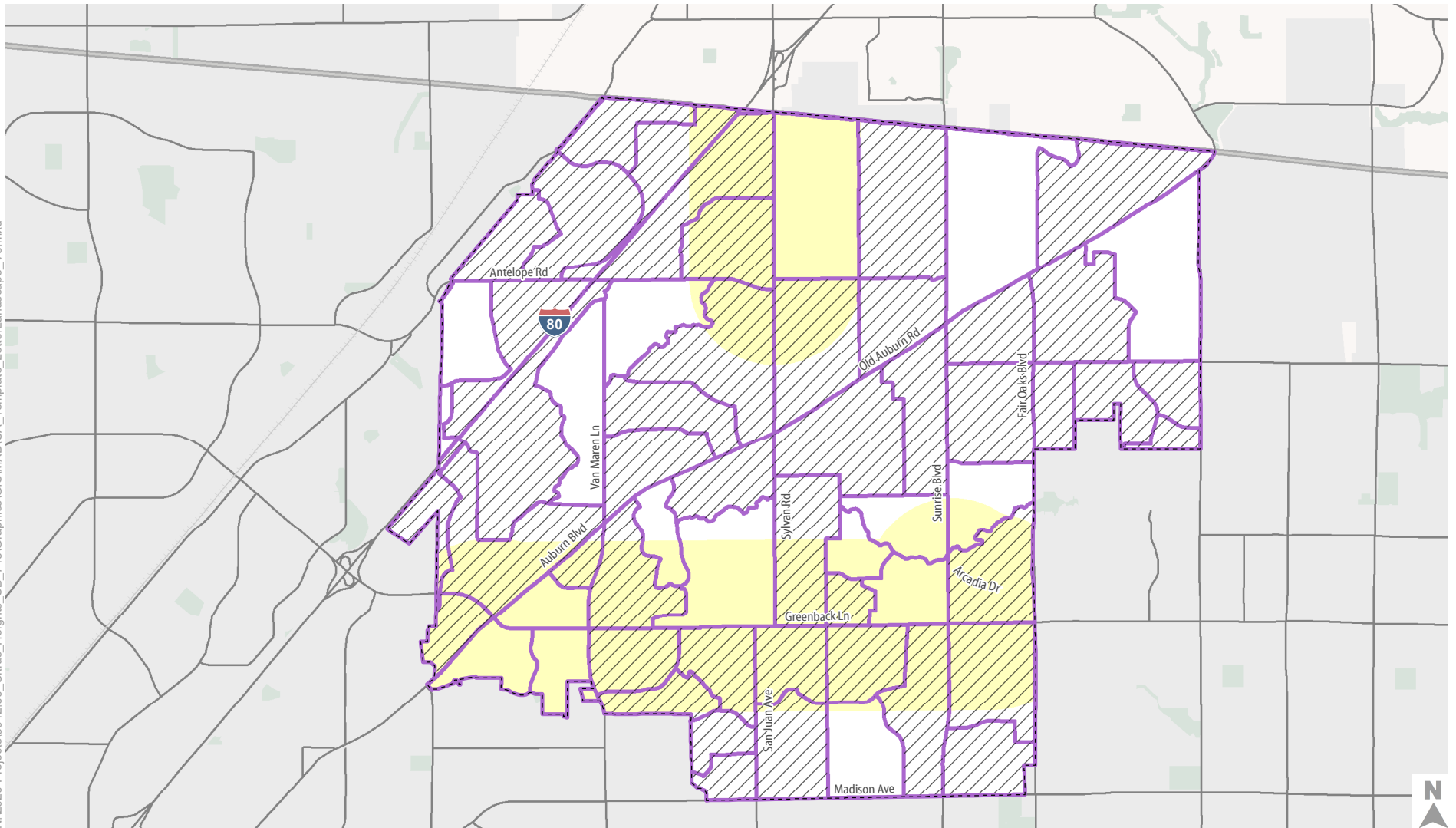
The City of Citrus Heights will apply the following thresholds of significance when analyzing the VMT transportation impacts of retail and entertainment land use projects under CEQA. The project would cause a significant impact if it would:





- Not qualify under one of the applicable exemption categories (small projects, projects near high-quality transit corridors, redevelopment projects, or local serving retail), and its VMT exceeded 85 percent of the regional per visitor average. If the above conditions are met, the project's VMT impact could still be found to be less-than-significant if it did not cause the total VMT generated by the City of Citrus Heights to increase.

Figure 5 is a screening map of those CBGs that have average VMT per visitor values that are 85 percent or less of the SACOG regional average. Also overlaid on this figure are the City's existing high-quality transit corridors. Based on this figure, it can be inferred that 21 percent of the CBGs in Citrus Heights have portions or all its block group area that would not meet either the proximity to high-quality transit or screening map exemptions. However, 79 percent of all CBGs would be exempted.

Proposed VMT Thresholds of Significance – Atypical and Mixed-Use Projects

Special consideration will be necessary to analyze VMT impacts for land uses that do not fit into any of the above categories. Common examples are hotels, medical centers, wineries, churches, schools/colleges, etc. These uses should be analyzed on a case-by-case basis using available information and applying the general intent of the *Technical Advisory*.



-  Census Block Groups
-  High Quality Transit Corridor (1/2 mile buffer)
-  Citrus Heights City Limits
-  Average Daily Employee HBW VMT is 85% or less of SACOG region average 18.1

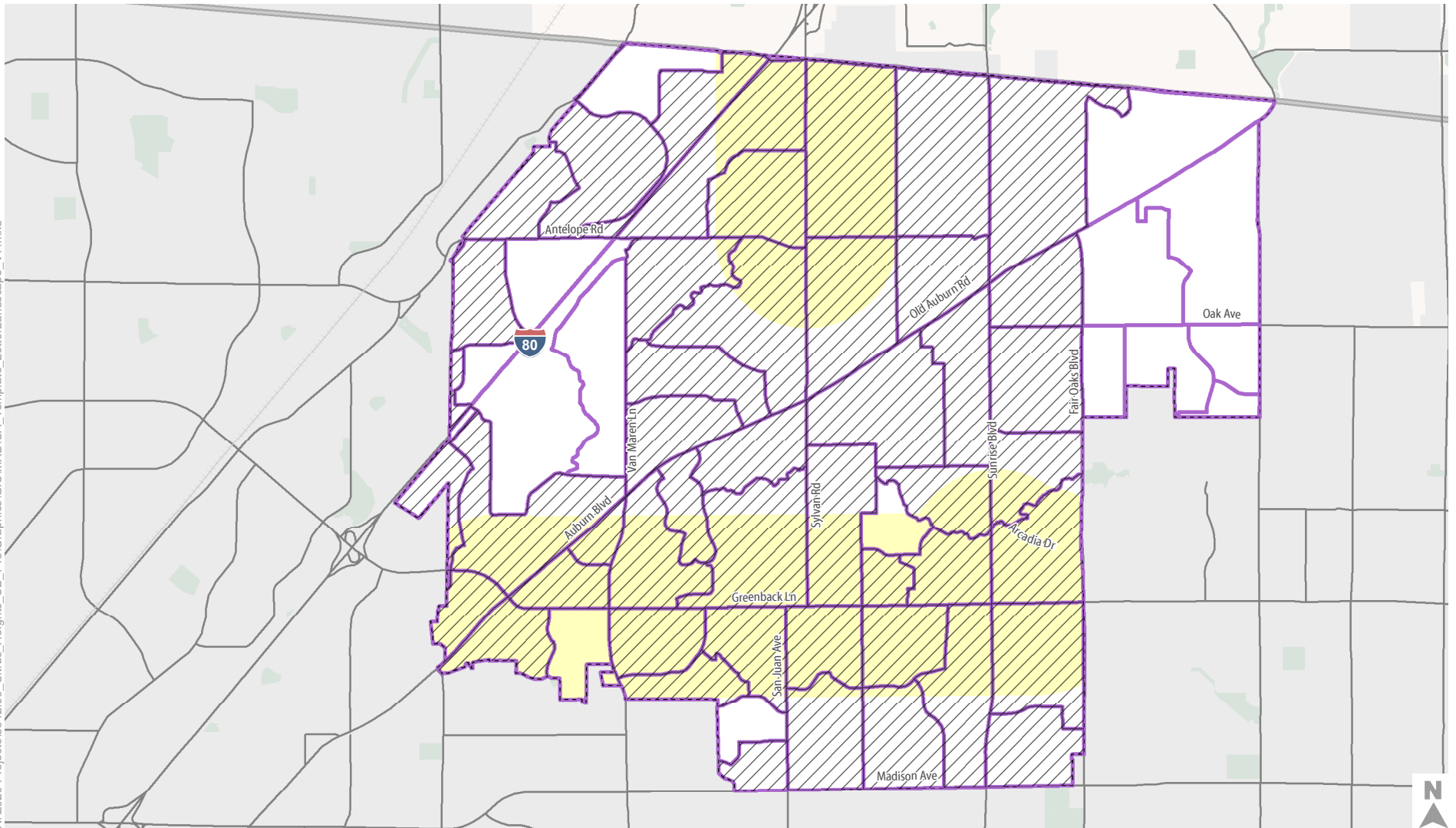
HBW = Home-Based Work trip purpose





1. This figure serves as a screening map, which should be used with appropriate professional engineering judgment. Refer to report text for details.
2. Data (provided by Streetlight Data, Inc.) represents averaged travel conditions for all Tuesdays, Wednesdays, and Thursdays for the entirety of 2019.



Screening Map - Average Daily Home-to-Work VMT per Employee

Figure 4



-  Census Block Groups
-  High Quality Transit Corridor (1/2 mile buffer)
-  Citrus Heights City Limits
-  Average Daily Visitor VMT is 85% or less of SACOG region average

1. This figure shows the average daily VMT on days in which a visitor travels to a given block group. It considers the number of times per day that the visitor travels to/from the site and the average length of the trip.
2. Data (provided by Streetlight Data, Inc.) represents averaged travel conditions on all days (including weekdays and weekends) of 2019.



Figure 5

Average Daily VMT per Visitor

Additionally, projects that feature a mix of complementary land uses on-site should be analyzed using a technical approach geared toward the specifics of the project. The *Technical Advisory* describes two possible approaches: (1) analyze (considering internal trips) and determine significant impacts of each project component separately, or (2) consider significant impacts based on the project's dominant land use.

Checklist for VMT Thresholds of Significance for Land Use Projects ¹

Exemptions (i.e., VMT impacts presumed less-than-significant)

- *Small Projects*: up to 19 single-family units, 32 multi-family units, or 14,000 square feet of office
- *Projects near High-Quality Transit*²: Situated within cross-hatched areas of Figures 3-5.
- *Affordable Housing*
- *Redevelopment Projects*: project results in a net decrease in VMT.
- *Local-Serving Retail*: Projects that consist of 50,000 square feet of retail space or less.

Project Screening

- Projects that are situated in "low VMT generating" census block groups (see Figures 3-5) are presumed less-than-significant.

Other Considerations

- Use of "Blending/Adjacency" evaluations for certain projects (see Chapter V for details).
- Projects that result in a net decrease in overall VMT.

1: Applies only to projects that are subject to CEQA.

2: Subject to being transit supportive, providing parking that is no more than 20% over the City code requirement, and not removing affordable housing.

IV. PROPOSED VMT THRESHOLDS OF SIGNIFICANCE FOR TRANSPORTATION PROJECTS

This chapter provides an introductory discussion of how transportation projects should be evaluated under CEQA. Since this is a complex and evolving topic, only a high-level overview is provided at this point. The majority of planned roadway improvement projects in Citrus Heights take the form of Complete Streets and bicycle/pedestrian/transit facility upgrades. Few, if any, of the projects are capacity-inducing. Therefore, much of the material below may not be relevant to City transportation projects.

Technical Advisory Guidance on VMT Impacts from Transportation Projects

Pages 19- 28 of the Technical Advisory discuss a number of aspects of this topic. Following are some of the key recommendations from it:

1. The "induced vehicle travel" caused by certain transportation projects must be quantified. Projects that would likely lead to a "measurable and substantial" increase in vehicle travel (i.e., VMT) generally include: addition of through lanes on existing or new highways, including general purposes lanes, carpool lanes, auxiliary lanes, or lanes through grade-separated interchanges.
2. A variety of transportation projects would not be expected to induce more vehicle travel. The following page lists these project types, though it is noted that evidence is not provided to support that conclusion of no net VMT.
3. A generally accepted interpretation of the Technical Advisory is that a transportation project that causes a net increase in VMT would be considered to have a significant impact. Although a specific significance threshold is not provided in the *Technical Advisory*, it states on multiple occasions that transportation projects that do not generate additional VMT are presumed to have less-than-significant impacts. Part 2b of Section 15064.3 of the CEQA Guidelines (Determining the Significance of Transportation Impacts) states that "Transportation projects that reduce, or have no impact on VMT should be presumed to cause a less than significant transportation impact."
4. VMT attributable to a project should represent the difference in VMT with and without the project across the full area in which driving patterns are expected to change. VMT should be not truncated at model or jurisdictional boundaries.

Proposed VMT Threshold of Significance – Transportation Projects

The City of Citrus Heights will apply the following threshold of significance when analyzing the VMT transportation impacts of transportation projects under CEQA.

- A transportation project would cause a significant transportation impact if it would lead to induced travel and increased VMT.

Below is the list of projects on Page 21 of the *Technical Advisory* that are presumed to not cause a significant transportation impact.

- Roadway shoulder enhancements to provide “breakdown space,” dedicated space for use only by transit vehicles, to provide bicycle access, or to otherwise improve safety, but which will not be used as automobile vehicle travel lanes
- Addition of an auxiliary lane of less than one mile in length designed to improve roadway safety
- Installation, removal, or reconfiguration of traffic lanes that are not for through traffic, such as left, right, and U-turn pockets, two-way left turn lanes, or emergency breakdown lanes that are not utilized as through lanes
- Addition of roadway capacity on local or collector streets provided the project also substantially improves conditions for pedestrians, cyclists, and, if applicable, transit
- Conversion of existing general purpose lanes (including ramps) to managed lanes or transit lanes, or changing lane management in a manner that would not substantially increase vehicle travel
- Addition of a new lane that is permanently restricted to use only by transit vehicles
- Reduction in number of through lanes
- Grade separation to separate vehicles from rail, transit, pedestrians or bicycles, or to replace a lane in order to separate preferential vehicles (e.g., HOV, HOT, or trucks) from general vehicles
- Installation, removal, or reconfiguration of traffic control devices, including Transit Signal Priority (TSP) features
- Installation of traffic metering systems, detection systems, cameras, changeable message signs and other electronics designed to optimize vehicle, bicycle, or pedestrian flow
- Timing of signals to optimize vehicle, bicycle, or pedestrian flow
- Installation of roundabouts or traffic circles
- Installation or reconfiguration of traffic calming devices
- Adoption of or increase in tolls
- Addition of tolled lanes, where tolls are sufficient to mitigate VMT increase
- Initiation of new transit service
- Conversion of streets from one-way to two-way operation with no net increase in number of traffic lanes
- Removal or relocation of off-street or on-street parking spaces
- Adoption or modification of on-street parking or loading restrictions (including meters, time limits, accessible spaces, and preferential/reserved parking permit programs)
- Addition of traffic wayfinding signage
- Rehabilitation and maintenance projects that do not add motor vehicle capacity
- Addition of new or enhanced bike or pedestrian facilities on existing streets/highways or within existing public rights-of-way
- Addition of Class I bike paths, trails, multi-use paths, or other off-road facilities that serve non-motorized travel
- Installation of publicly available alternative fuel/charging infrastructure
- Addition of passing lanes, truck climbing lanes, or truck brake-check lanes in rural areas that do not increase overall vehicle capacity along the corridor

V. LAND USE PROJECT CASE STUDIES

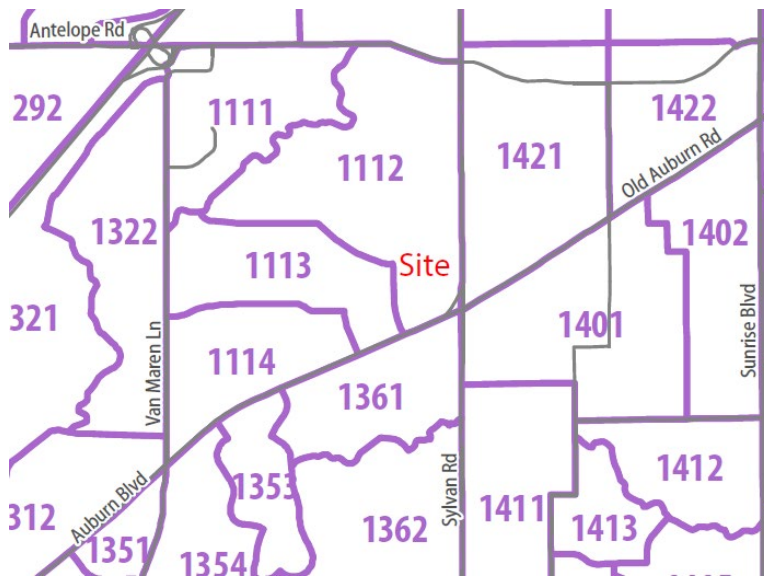
This chapter presents a typical case study that the City could expect. This example purposefully is not in a CBG that is exempted based on efficiency or high-quality transit proximity.

Case Study: Residential at Sylvan Corners

The area near the Auburn Boulevard/Old Auburn Road/Sylvan Road intersection is known as Sylvan Corners. This case study assumes residential development in the undeveloped northwest quadrant of the intersection. Existing land uses at Sylvan Corners include various retail/restaurant uses, and limited office, industrial, churches, and other supporting uses. The Stock Ranch Retail Center (featuring Costco and Sam's Club are located ½ mile to the west on Auburn Boulevard).

The case study is situated in the most southerly portion of CBG 1112 (see "Site" below in red), which is neither a VMT efficient CBG or within ½ mile of high-quality transit. The CBG is large, comprising 280 acres that extends to Antelope Road and nearly to Van Maren Lane. Given the CBG size and variety of land uses within it, residential travel characteristics within this area are not homogenous. This CBG generates an average of 20.9 VMT per resident, which is above both the Citywide average (17.4 VMT per resident) and SACOG regional average (20.1 VMT per resident). However, the immediately adjacent CBGs to Sylvan Corners exhibit much more efficient VMT per resident characteristics:

- CBG 1113 generates 16.9 VMT per resident
- CBG 1361 generates 15.0 VMT per resident
- CBG 1401 generates 15.7 VMT per resident



Based on the above information, an analyst could perform a limited amount of supplemental analysis to demonstrate the following:

1. The project would have travel characteristics more similar to CBGs 1113, 1361, and 1401 (versus 1112) all of which have VMT per resident averages that are less than 85 percent of the SACOG regional average. This could be accomplished by discussing the following (to the extent relevant):
 - Project's proximity to Sylvan Corners and other CBGs that are better indicators of travel efficiency.
 - Project's similarity in land use type, socioeconomic characteristics to adjacent CBGs.

In all likelihood, the analysis would conclude that VMT impacts associated with such a project would be less than significant.

The above example highlights one of several ways in which VMT analysis may be approached, under certain circumstances, without relying entirely on the screening maps. In this particular instance, the project's physical location was the critical factor that influenced the ability to select an alternative approach. This is the "blending/adjacency" type of evaluation discussed in Chapter III.

Other approaches could examine the characteristics of the project against the predominate socioeconomic and commuting characteristics of that CBG (see Appendix A). For instance, if a multi-family project, which is expected to have an above average transit commute mode share (i.e., due to providing free transit passes or reducing parking) is proposed in a CBG that consists primarily of single-family dwelling units with high auto ownership levels, and high levels of commute to work by vehicle, it would be justified to consider adjustments to the project's VMT to reflect the project's unique characteristics relative to the averages of the CBG.¹⁶

Refer to Appendix A for two other case studies.

¹⁶ To illustrate, if the CBG has an average VMT per resident of 18.5 and the unique characteristics of the project were demonstrated to reduce that by 10 percent, then the overall result would be 16.7 which is below the 17.1 VMT per resident threshold. However, substantial evidence would need to be provided to support the project's 10 percent reduction over the CBG average.

VI. MITIGATION MEASURES AND PLAN UPDATES

This chapter provides an overview of potential mitigation measures to address significant VMT impacts. Additionally, it describes the extent to which this plan should be updated.

Overview of Mitigation Measure Strategies

Feasible mitigation measures will be recommended for land use projects that exceed the applicable VMT threshold and cause a significant impact. While an abundance of potential VMT reduction strategies exist, not all of these are applicable to suburban settings, and many have not undergone sufficient academic/technical review to demonstrate their effectiveness. In CEQA, it is important to demonstrate that any recommended mitigation measures are both feasible and effective.

Mitigation measures for VMT impacts will principally focus on modifying the project to generate less VMT, often through the implementation of transportation demand management (TDM) strategies. This is in contrast to pre-SB 743 environmental review efforts, in which significant transportation impacts were based on traffic operations and would be mitigated by typically adding roadway capacity at the impacted facilities. Since the latter solution would not reduce a project's VMT, off-site capacity-increasing improvements to address significant transportation improvements will no longer be recommended in CEQA documents (but may still be included as conditions of approval).

Alternative approaches available to the City could include a VMT mitigation bank or impact fee program, in which a project pays a proportionate fee into the program that will fund certain improvements (e.g., bikeway projects, transit enhancements, etc.) within the City that would reduce VMT. However, these programs can be very complex, and time-intensive to establish and operate.

Fehr & Peers, working in conjunction with researchers at UC Berkeley (for the ARB Zero Carbon Buildings Study) has identified a set of TDM strategies whose reduction percentages are highly defensible and suitable for use in environmental analysis documents because they have been derived from academically prepared, peer-reviewed studies that would represent substantial evidence regarding the effectiveness of the given strategy. The following list of mitigation measures are considered feasible within Citrus Heights:

- Charge for Cost of Off-Street Parking
- Limit Parking Supply (requires a 'closed system' and available non-auto modes of travel)
- Employee Parking Cashout Program
- Transit Pass Subsidies
- Transit Service Frequency or Coverage Area Improved
- Point to point or "last mile" shuttles

- Commute Marketing Program (i.e., mode choice options at worksite, TDM coordinator, etc.)
- Carpool/Vanpool Incentives
- Pedestrian-Oriented Design

The effectiveness of a given TDM strategy can be highly dependent on the project's geographic setting and specific tenants. Thus, the effectiveness is often expressed as a range. Additionally, multiple TDM strategies implemented at the same time may have overlapping "dampening" effects, which must be considered.

The above list excludes several other well-known TDM strategies that could be applicable to projects in Citrus Heights. Unfortunately, the strategies, which are listed below, have limited academic research supporting them and hence do not currently provide substantial evidence as to the efficacy:

- Pay per use (versus monthly) parking
- Reduced parking supply
- Emergency ride home
- Transportation Network Company (TNC) partnerships
- Secure bike parking
- Showers and lockers for bicyclists
- Public bike repair stations
- Bikeshare systems
- Ridematching programs
- Carshare availability and subsidy programs

It is noted that TDM strategy effectiveness is the focus on several ongoing transportation research studies. It is conceivable that those research efforts may yield more defensible estimates of the effectiveness of the above and other TDM strategies.

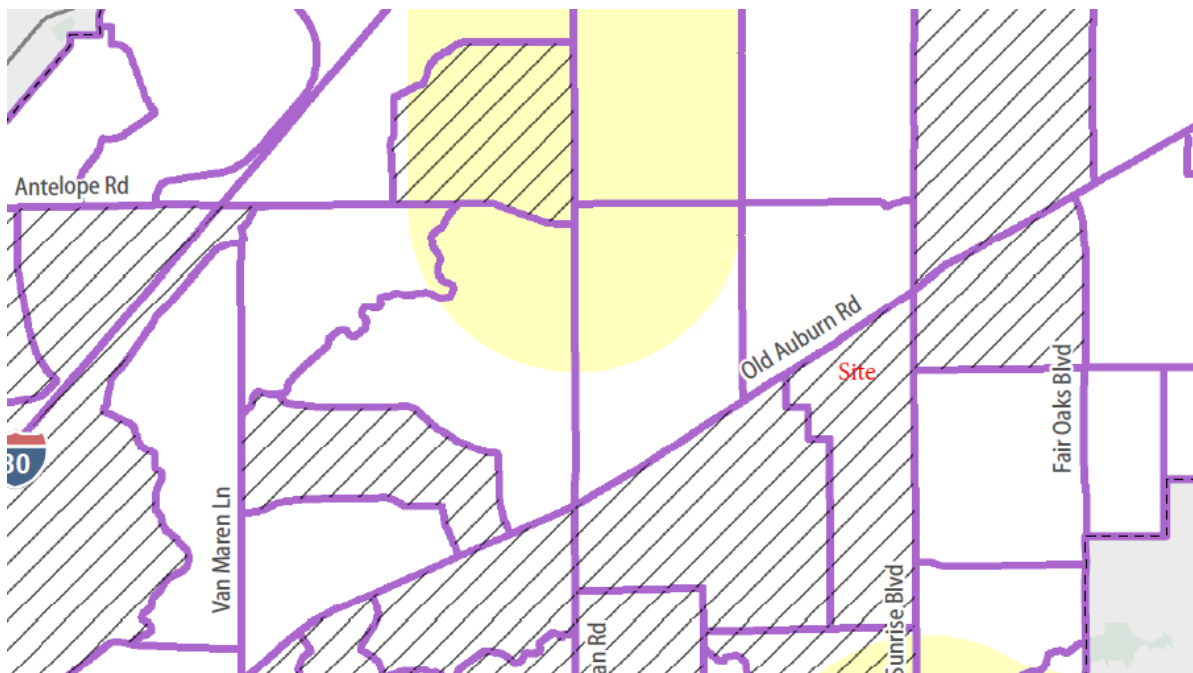
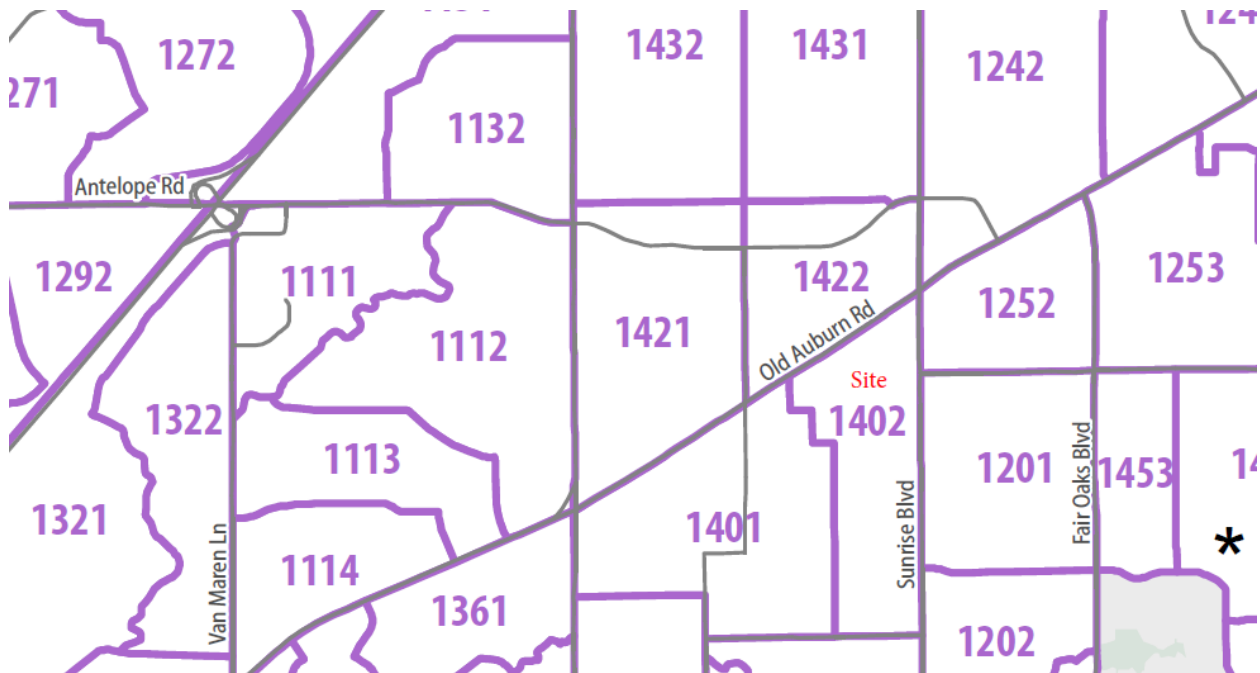
Subsequent Updates to SB 743 Implementation Plan

This report should be periodically updated as necessary to reflect any of the following:

- Changes in planned/proposed land uses (both within and outside of Citrus Heights) that could have a substantial effect on VMT thresholds.
 - Changes in the planned roadway system (both within and outside of Citrus Heights) that could have a substantial effect on VMT thresholds.
 - Changes in state-of-the-practice or technical guidance from agencies with respect to how VMT should be calculated and/or VMT thresholds should be set.
- Changes in mode choice options that could have a substantial effect on travel and VMT calculations.

Appendix A – Case Studies and Supporting Technical Information

Case Study 1: Old Auburn/Sunrise Residential Project – The specific area under evaluation is an approximate five-acre undeveloped parcel located south of Old Auburn Road and west of Sunrise Boulevard. It could yield 40 dwelling units if developed at 8 units/acre, which is greater than the small project exemption for single-family units. It is in CBG 1402 which according to Figure 3, would generate VMT per du that is less than 85% of the regional average. Hence, conclusion would be less than significant VMT impact. No analysis required.

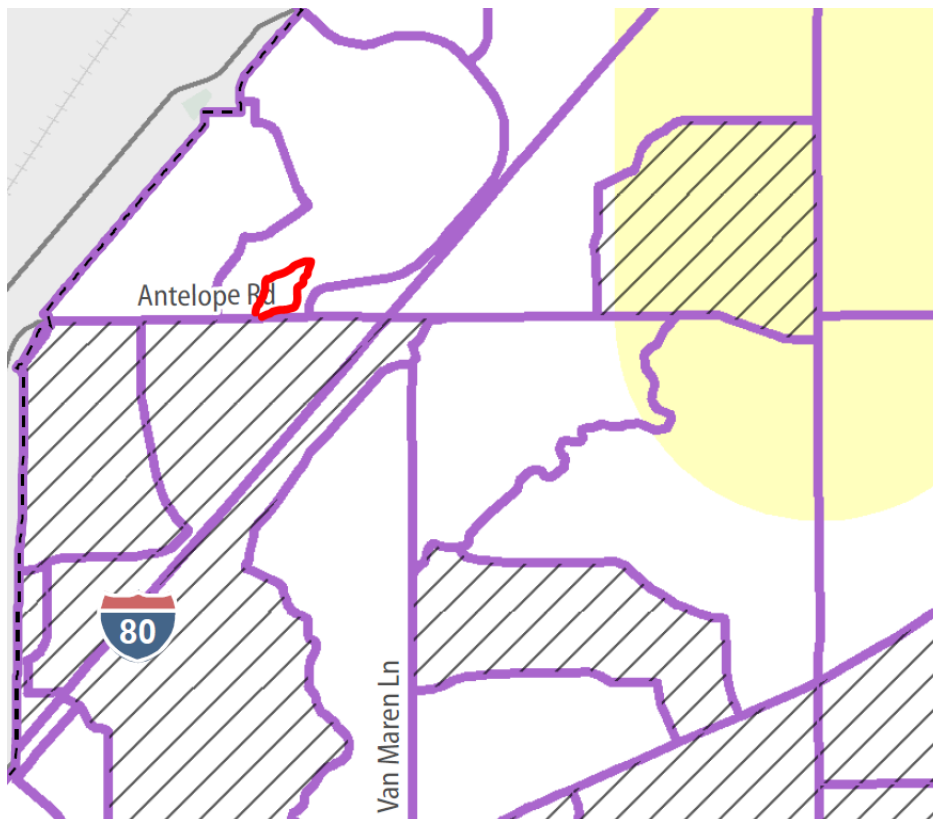
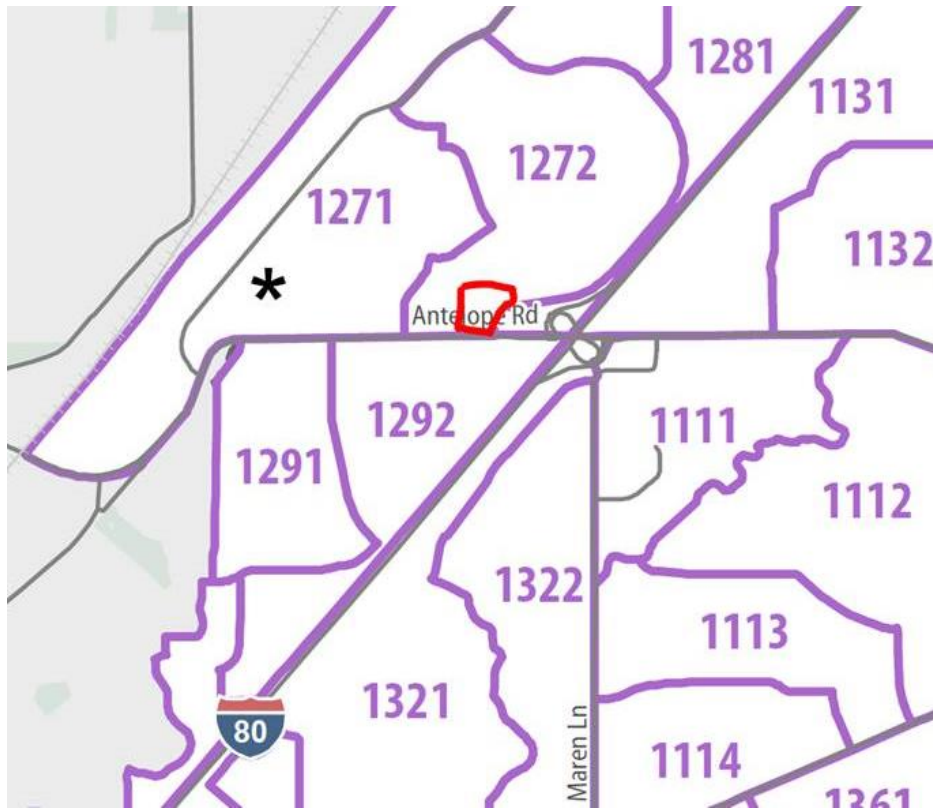


Case Study 2: Antelope/I-80 (north side) – This area is represented by CBG 1272, which is vast (215 acres), extending northwesterly to Roseville Road. Proposed development would likely occur on a portion of the underdeveloped Summerhill Plaza Retail Center, which is located along Antelope Road and Lichen Drive. That site is situated adjacent to CBG 1292 (note that low-generating CBG 1291 is also adjacent, but excluded from consideration because it consists of a large number of mobile home units). Socioeconomic and travel characteristics of CBGs 1272 and 1292 are shown below:

CBG	Avg VMT per Resident	% of Single-Family Dwelling Units	Average Vehicles per Household	Percent of Residents that are Under 18	Percent who Commute to Work by Private Vehicle
1272 (project zone)	20.8	93%	2.09	29%	99%
1292 (adjacent zone)	14.4	85%	1.87	19%	94%

Conclusion: The specific type of residential development would dictate the conclusion with respect to VMT impacts. Here are two diverging examples:

- Low Density Residential: entire retail center replaced by estate-type, single-family detached dwelling units. If yield exceeded 19 units, small project exemption would not apply. This land use would likely have characteristics similar to its CBG (1272) and have VMT per resident that is above the threshold, hence creating a significant VMT impact. A less than significant finding *could* potentially be achieved if the parking supply was limited and/or a study demonstrated that the project's proximity to retail and the SacRT Bus Line #93 would contribute to fewer external vehicle trips.
- High Density Residential: part of retail center is redeveloped with 15 to 20 multi-family units per acre, exceeding the small project exemption. Assuming parking supply is limited to 1.75 spaces per unit or less, the site's close proximity to CBG 1292 which is more similar in nature to the project's land uses, would suggest it would generate VMT per resident that is 85% of the regional average. Hence, VMT impacts would be less than significant.



Blockgroup	Res HBx VMT/device/day	Emp HBW VMT/device/day	Avg Daily Visitor VMT per Device
60670081111	20.0	16.6	0.592
60670081112	20.9	9.3	0.724
60670081113	16.9	10.4	0.714
60670081114	18.1	11.6	0.482
60670081131	18.6	11.1	0.656
60670081132	14.7	8.9	0.635
60670081191	15.8	11.1	0.419
60670081192	16.4	10.2	0.516
60670081193	16.6	10.4	0.631
60670081194	15.5	9.5	0.502
60670081195	16.2	10.8	0.889
60670081201	18.7	10.0	0.587
60670081202	18.5	18.0	0.538
60670081203	18.8	11.4	0.501
60670081241	19.2	12.6	0.887
60670081242	16.5	16.3	0.596
60670081251	18.3	20.8	0.928
60670081252	16.7	9.7	0.496
60670081253	21.2	9.7	0.775
60670081271	18.4	7.2	0.651
60670081272	20.8	9.6	0.654
60670081281	18.4	10.6	0.586
60670081282	18.3	7.3	0.954
60670081291	13.8	17.4	0.670
60670081292	14.4	14.7	0.825
60670081303	15.4	24.8	0.436
60670081311	21.4	14.8	0.512
60670081312	17.5	11.0	0.586
60670081314	14.3	13.0	0.568
60670081321	17.0	15.3	0.997
60670081322	23.3	15.4	0.813
60670081341	13.5	18.3	1.072
60670081342	17.1	20.0	0.609
60670081351	18.0	6.9	0.564
60670081352	16.2	17.8	0.479
60670081353	13.6	21.0	0.534
60670081354	13.9	9.1	0.399
60670081361	15.0	15.3	0.614
60670081362	15.3	18.1	0.608
60670081371	19.5	14.6	0.473
60670081372	12.3	11.1	0.499
60670081381	12.9	11.4	0.587
60670081382	15.7	9.4	0.454
60670081383	15.0	16.5	0.579
60670081391	13.5	12.9	0.560
60670081392	18.9	12.5	0.545
60670081401	15.7	10.0	0.574
60670081402	15.9	12.1	0.593
60670081411	13.5	12.7	0.632
60670081412	14.0	15.7	0.680
60670081413	17.7	16.8	0.923
60670081414	14.1	10.8	0.566
60670081415	16.6	16.0	0.554
60670081421	19.8	9.4	0.558
60670081422	17.8	14.4	0.509
60670081431	17.1	10.6	0.623
60670081432	20.8	16.2	0.597
60670081451	22.0	10.0	0.772
60670081452	21.7	10.1	0.815
60670081453	18.2	8.2	0.787
60670081454	19.4	9.6	0.897
SACOG Region Avg.	20.1	18.1	0.895

Geography ID	Acres	Count of Residents (ACS)	AVG HH Size	Percent Owner Occupied Units	Percent Making \$35k or less (60% of AMI)	Percent Retired	Percent of Population Under 18	Percent Population 65 and Over	% SF Units	% Commute to Work by private vehicle	Average Vehicles per HH
60670081111	151.71	1981	2.49	0.72	14.3%	25.1%	22.8%	14.8%	92.8%	97.2%	2.06
60670081112	280.04	1601	2.68	0.88	38.3%	50.3%	18.6%	18.1%	65.5%	91.9%	1.93
60670081113	139.10	1886	3.07	0.70	19.9%	32.7%	19.1%	17.2%	100.0%	97.4%	2.11
60670081114	127.71	922	2.55	0.84	20.2%	28.8%	20.2%	9.2%	100.0%	100.0%	1.73
60670081131	238.92	2536	2.40	0.51	23.6%	30.0%	17.9%	8.4%	49.0%	97.4%	1.99
60670081132	158.28	1090	2.69	0.70	11.1%	34.9%	26.1%	16.4%	100.0%	56.3%	1.88
60670081191	120.99	1953	2.69	0.32	26.9%	36.3%	20.1%	6.5%	51.8%	94.9%	1.89
60670081192	117.31	1539	3.10	0.76	28.4%	43.4%	26.3%	18.8%	97.3%	100.0%	2.06
60670081193	100.19	881	2.81	0.85	19.2%	26.0%	19.1%	8.4%	100.0%	85.2%	1.91
60670081194	77.50	462	2.51	0.82	24.3%	41.9%	22.4%	22.4%	95.7%	100.0%	2.11
60670081195	50.00	537	2.89	0.81	11.6%	31.8%	22.6%	14.4%	100.0%	91.7%	1.95
60670081201	186.27	1921	2.50	0.57	30.1%	41.5%	18.3%	19.5%	67.5%	97.5%	1.73
60670081202	123.50	1620	3.00	0.64	23.5%	33.6%	28.6%	15.0%	89.4%	100.0%	1.96
60670081203	177.56	1232	1.82	0.04	27.3%	16.8%	12.5%	5.9%	9.2%	100.0%	1.45
60670081241	263.61	3119	3.25	0.87	11.6%	37.6%	23.9%	19.7%	100.0%	99.4%	2.45
60670081242	337.84	1677	2.34	0.81	16.0%	33.6%	13.6%	22.6%	100.0%	93.3%	2.19
60670081251	297.24	2940	2.66	0.84	15.1%	33.9%	16.2%	18.9%	83.2%	97.3%	2.00
60670081252	127.55	980	2.81	0.59	15.8%	46.8%	12.0%	28.3%	68.5%	96.1%	1.81
60670081253	207.35	1503	2.55	0.72	10.2%	35.7%	19.4%	17.8%	100.0%	100.0%	1.90
60670081271	320.00	1338	3.09	0.70	17.4%	31.2%	24.9%	7.2%	100.0%	95.6%	2.09
60670081272	215.60	3105	2.93	0.59	15.9%	34.5%	29.0%	13.2%	93.4%	99.3%	2.09
60670081281	135.73	1210	2.57	0.86	35.5%	47.3%	16.2%	21.8%	57.3%	100.0%	1.74
60670081282	112.62	1261	2.85	0.75	26.7%	34.8%	21.2%	16.3%	98.9%	97.4%	2.05
60670081291	120.61	1559	2.66	0.87	26.6%	42.4%	18.7%	26.2%	66.5%	90.3%	1.79
60670081292	160.84	1584	2.54	0.69	19.6%	28.1%	19.3%	15.1%	85.2%	93.7%	1.87
60670081303	25.00	276	2.34	0.92	16.3%	42.6%	19.8%	21.9%	100.0%	100.0%	1.91
60670081311	28.00	449	2.35	0.92	16.0%	20.1%	14.6%	7.3%	100.0%	100.0%	1.95
60670081312	257.01	1711	2.70	0.82	24.5%	38.5%	29.1%	19.0%	92.4%	95.2%	2.04
60670081314	97.00	1570	2.47	0.50	48.6%	37.3%	23.4%	20.0%	0.0%	94.1%	1.53
60670081321	292.34	1582	2.03	0.88	30.5%	53.8%	10.1%	44.8%	47.9%	100.0%	1.67
60670081322	193.80	2297	3.28	0.81	17.4%	43.3%	23.2%	10.6%	95.5%	93.2%	2.17
60670081341	100.10	821	2.76	0.80	22.1%	39.6%	15.8%	30.0%	100.0%	100.0%	1.94
60670081342	141.89	2054	2.48	0.32	30.1%	35.2%	25.4%	11.2%	38.3%	93.4%	1.35
60670081351	38.88	822	1.70	0.20	37.9%	64.2%	25.3%	38.4%	21.5%	89.0%	1.06
60670081352	75.46	1782	2.53	0.28	56.0%	19.7%	27.9%	5.8%	28.8%	93.0%	1.55
60670081353	75.44	820	1.92	0.56	36.2%	47.3%	13.0%	36.0%	61.0%	100.0%	1.34
60670081354	154.17	1051	1.99	0.51	41.3%	46.4%	17.5%	32.4%	63.6%	100.0%	1.22
60670081361	126.40	714	1.91	0.29	44.9%	43.9%	14.0%	15.4%	52.9%	100.0%	1.49
60670081362	237.35	1530	2.20	0.44	25.5%	40.3%	13.1%	27.3%	49.7%	98.4%	1.66
60670081371	113.76	1526	3.10	0.36	31.1%	28.7%	31.1%	9.3%	42.1%	95.4%	1.88
60670081372	154.95	924	2.11	0.45	38.7%	37.6%	16.5%	21.4%	61.9%	94.6%	1.71
60670081381	178.59	956	2.70	0.38	37.0%	35.8%	15.9%	20.4%	51.7%	87.8%	1.36
60670081382	121.70	1654	2.88	0.50	25.6%	39.5%	30.9%	13.0%	67.1%	95.2%	2.04
60670081383	146.34	1075	2.65	0.80	5.7%	38.5%	17.7%	20.0%	100.0%	98.1%	1.99
60670081391	196.27	1906	2.27	0.10	53.8%	38.2%	21.5%	9.4%	13.5%	94.0%	1.07
60670081392	133.70	1818	2.04	0.29	34.1%	31.5%	10.1%	20.7%	44.3%	91.8%	1.49
60670081401	245.84	1576	3.09	0.83	23.5%	39.3%	24.4%	17.1%	100.0%	100.0%	2.16
60670081402	162.68	1397	2.40	0.32	36.0%	38.8%	13.8%	20.3%	54.3%	95.3%	1.62
60670081411	188.07	1072	2.71	0.89	15.2%	48.5%	12.0%	26.2%	100.0%	97.7%	1.98
60670081412	111.65	1258	2.65	0.55	23.6%	29.3%	22.3%	14.0%	79.9%	100.0%	1.84
60670081413	63.78	782	3.55	0.72	0.0%	35.1%	30.6%	17.6%	100.0%	100.0%	1.63
60670081414	47.74	1597	2.34	0.03	50.7%	27.2%	31.5%	1.6%	0.0%	72.6%	1.30
60670081415	107.26	2037	2.40	0.03	50.8%	27.3%	23.8%	5.0%	25.1%	91.0%	1.59
60670081421	234.26	2591	2.89	0.30	26.2%	30.8%	28.3%	5.6%	51.9%	99.7%	1.99
60670081422	137.23	1188	2.27	0.38	25.4%	41.3%	18.6%	10.9%	43.5%	100.0%	2.02
60670081431	293.80	2358	2.35	0.33	29.5%	32.3%	19.6%	12.8%	45.3%	93.1%	1.74
60670081432	300.72	1781	2.80	0.67	22.5%	28.3%	17.1%	15.2%	95.4%	96.2%	2.06
60670081451	65.00	619	3.03	0.71	12.1%	41.0%	34.1%	10.7%	100.0%	100.0%	2.01
60670081452	108.00	861	3.04	0.64	13.7%	36.4%	14.9%	7.5%	100.0%	90.5%	2.80
60670081453	88.88	842	2.95	0.75	18.9%	44.5%	13.5%	25.1%	100.0%	100.0%	2.42
60670081454	25.00	313	2.77	0.78	16.4%	44.2%	25.5%	18.8%	100.0%	100.0%	2.08

Transportation Impact Study Guidelines for City of Citrus Heights

March 2021

Prepared By:

FEHR  PEERS

Prepared for:



TABLE OF CONTENTS

1.	INTRODUCTION.....	1
	Background	1
	Preparers of Studies	2 <u>12</u>
	Project Considerations	3 <u>42</u>
	Intent of Study Guidelines	3 <u>43</u>
	Relationship to Recent Caltrans Policy Documents.....	4 <u>13</u>
	General Plan Context.....	4 <u>14</u>
2.	TRIGGERS REQUIRING AN IMPACT STUDY.....	6<u>15</u>
	Project Definition	7 <u>46</u>
	Trip Generation	8 <u>46</u>
3.	SCOPE OF THE STUDY	12<u>49</u>
	Study Area.....	12 <u>49</u>
	Transportation Analysis Scenarios.....	12 <u>49</u>
	Transportation Analysis Time Periods	14 <u>44</u>
	Consultation with Other Jurisdictions.....	14 <u>44</u>
	Major Components of the Study.....	15 <u>42</u>
4.	RELEVANT POLICIES	17<u>44</u>
	Vehicle Level of Service.....	23 <u>20</u>
5.	ANALYSIS METHODOLOGY	24<u>21</u>
	Transportation Data Collection.....	24 <u>21</u>
	Multimodal Analysis.....	25 <u>22</u>
	Traffic Operations Analysis	27 <u>23</u>
	Traffic Forecasts.....	35 <u>26</u>
6.	IMPACT ASSESSMENT.....	39<u>29</u>
	Scenario Evaluation	39 <u>29</u>
	Significance Criteria	39 <u>29</u>
	Cumulative Impacts.....	42 <u>31</u>
7.	MITIGATION MEASURES.....	43<u>32</u>
8.	RECOMMENDED PROCESS AND DOCUMENTATION.....	46<u>35</u>

1. INTRODUCTION

Transportation impact study (TIS) guidelines are routinely established by jurisdictions to ~~assist~~provide guidance on how to properly analyze the ~~applicants with assessing~~ potential transportation~~traffic~~ impacts of proposed projects. The following guidelines have been developed to provide a clear and consistent technical approach to transportation impact analysis for projects within the City of Citrus Heights'~~jurisdiction~~.

BACKGROUND

The First Citrus Heights TIS Guidelines were adopted in 2011 as part of the General Plan Update. The Guidelines were update in 2021~~This update to the City's 2011 Guidelines is warranted~~ not only because of changes in analysis techniques and methods in the 10 years since they were last prepared, but moreover because of Senate Bill (SB) 743. This landmark law requires that environmental analyses performed under the California Environmental Quality Act (CEQA) do not use level of service (LOS) as the basis for identifying impacts of a proposed project to the transportation system¹.

SB 743, passed in 2013, required the California Governor's Office of Planning and Research (OPR) to develop new CEQA guidelines that address traffic metrics under CEQA. In December 2018, OPR published the *Technical Advisory on Evaluating Transportation Impacts in CEQA*, which provided guidance for implementing SB 743. Under this guideline, Vehicle Miles Traveled (VMT) is the primary metric used to identify transportation impacts. On July 1, 2020, the provisions of Section 15064.3 became effective statewide.

~~SB 743, passed in 2013, required the California Governor's Office of Planning and Research (OPR) to develop new CEQA guidelines that address traffic metrics under CEQA. As stated in the legislation, upon adoption of the new guidelines, "automobile delay, as described solely by level of service or similar measures of vehicular capacity or traffic congestion shall not be considered a significant impact on the environment pursuant to this division, except in locations specifically identified in the guidelines, if any." In December 2018, OPR published *Technical Advisory on Evaluating Transportation Impacts in CEQA*, which provided guidance for implementing SB 743. On December 28, 2018, the Resources Agency adopted CEQA Guidelines Section 15064.3. Under this guideline, VMT is the primary metric used to identify transportation impacts. On July 1, 2020, the provisions of Section 15064.3 became effective statewide.~~Citrus Heights will evaluate land use proposals and transportation projects in a manner consistent with SB 743 and guidance contained in the *Technical Advisory*. To this end, the City has prepared its *SB 743 Implementation Guidelines for Citrus Heights* (~~February~~ March 2021). Readers should refer to that document for details on VMT analysis methods, significance criteria, and mitigation measures.

¹ CEQA Guidelines Section 15064.3 states that "automobile delay, as described solely by level of service or similar measures of vehicular capacity or traffic congestion shall not be considered a significant impact on the environment pursuant to this division, except in locations specifically identified in the guidelines, if any."

Although no longer permitted within CEQA documents, the LOS analysis (and identification of locations whose operations would be adversely affected) is still prepared to provide helpful information to decisionmakers and the public, to assist staff in understanding what types of improvements should be considered as a Condition of Approval for the project, and to evaluate the project's consistency with the City's General Plan LOS policy.

As a result of SB 743, the format of transportation impact studies prepared in Citrus Heights will be different than in the past, and will likely vary depending on the type of environmental document that is prepared:

- (Mitigated) Negative Declarations² – a single comprehensive TIS document (either report or technical memorandum will be prepared containing all components of the analysis.
- Environmental Impact Reports (EIRs) – will consist of the transportation chapter, which will include the VMT analysis and analysis of other topical areas (e.g., impacts to bicycle, pedestrian, transit facilities and services, hazardous conditions, emergency response, construction impacts, etc.). A separate Local Transportation Analysis (LTA) will be prepared, which presents the LOS analysis.

The above approach enables City Community Development Department staff who are preparing a (Mitigated) Negative Declaration to copy/paste relevant information from the TIS directly into that document. The TIS itself will typically not be attached to the (Mitigated) Negative Declaration, but will be included in the staff report and posted on the City's website for review by the general public.

The LTA prepared in support of an EIR will not be included as an appendix to the Draft or Final EIR. The LTA will be included in the staff report and posted on the City's website for review by the general public. This approach was successfully first undertaken on the Sunrise Tomorrow Specific Plan Draft EIR.

~~This document establishes protocols for transportation impact studies and reports based on the current state-of-the-practice in transportation planning and engineering. It describes how Senate Bill (SB) 743 has fundamentally changed how transportation impacts are covered in the California Environmental Quality Act (CEQA) and ways in which roadway system level of service (LOS) may still be evaluated.~~ The City expects these guidelines to continue resulting in studies that provide comprehensive and accurate analysis of potential transportation impacts to City facilities and services.

PREPARERS OF STUDIES

The City of Citrus Heights has established on-call transportation consulting agreements with consulting firms with demonstrated expertise in preparing high-quality and unbiased TISs. The City will not accept TISs prepared by consultants directly for an applicant. Refer to Section 6 for more details,~~which involve development activity in and around Citrus Heights and affect the City's transportation system.~~

² Also includes Addendums to EIRs and other types of environmental documents that do not require public circulation and review.

~~This information is essential for decision makers and the public when evaluating individual projects.~~

PROJECT CONSIDERATIONS

The following types of projects, ~~which involve development activity in and around Citrus Heights and affect the City's transportation system,~~ may require a TIS [as determined by the City Engineer](#):

- Transportation infrastructure modification or expansion, including capital improvement projects (CIP) on city roads and state ~~highways~~[facilities](#).
- Land use entitlements requiring discretionary approval by the City of Citrus Heights, which includes annexations, general plan amendments, specific plans, zoning changes, conditional use permits, design review permits, and tentative maps.
- Land use activity advanced by agencies other than the City of Citrus Heights that is subject to jurisdictional review under state and federal law.
- ~~Land use activity advanced by agencies other than the City of Citrus Heights that is inconsistent with the City's General Plan.~~

Section 2 identifies specific project parameters or "triggers" that may necessitate a TIS.

INTENT OF STUDY GUIDELINES

These guidelines address key elements required for preparing and reviewing transportation impact studies in Citrus Heights. This document is intended to be a resource applied in concert with professional judgment. The following major issues are addressed in this document:

- Situations and thresholds that commonly trigger the need for a TIS.
- Scope and extent of the required study.
- Transportation impact analysis methods.
- Criteria to determine if the transportation-related impacts of a proposed project are significant under the California Environmental Quality Act (CEQA).
- Mitigation measure requirements.
- Guidelines for documentation of the findings, conclusions, and recommendations.

The City of Citrus Heights will ~~primarily~~ review transportation studies and reports based on the guidelines presented in this document. However, each project is unique, and TIS guidelines are not intended to be prescriptive beyond practical. Not all criteria and analyses described in this document will apply to every project. Early and consistent communication with the ~~Planning~~[Community Development](#) and General Services

Department is encouraged to confirm the [appropriate](#) type and level of analysis required on a case-by-case basis.

RELATIONSHIP TO RECENT CALTRANS POLICY DOCUMENTS

[In May 2020, the California Department of Transportation \(Caltrans\) published the *Vehicle Miles Traveled-Focused Transportation Impact Study Guide* \(TISG\), which replaced its Guide for the Preparation of Traffic Impact Studies \(2002\). The TISG generally endorses the policies, technical approaches, and recommendations from OPR's *Technical Advisory*. It also indicates that Caltrans intends to "transition away from requesting LOS or other vehicle operations analyses of land use projects", instead placing the focus on VMT and safety.](#)

[Caltrans published the *Interim Local Land Development and Intergovernmental Review \(LDIGR\) Safety Review Practitioners Guidance* in December 2020. This document provides guidance for conducting safety reviews of land use projects and plans that may affect the State Highway System. Although the LDIGR Safety Review Practitioners Guidance stops short of including specific thresholds of significance or providing specific recommendations for how safety evaluations should be included in CEQA documents, it does clearly indicate the State's expectation that, when appropriate, CEQA studies of land use projects should include safety investigations of the State Highway System. Furthermore, that document specifies that mitigation measures for identified safety impacts should avoid increasing roadway capacity, which may induce VMT or affect conditions for vulnerable users, such as bicyclists or pedestrians.](#)

[Citrus Heights will follow applicable analysis methods and general guidance provided in these recent Caltrans documents, to the extent they are applicable to the land use proposal or transportation project being evaluated.](#)

GENERAL PLAN CONTEXT

The Community Development Element of the Citrus Heights General Plan specifically identified the development and adoption of transportation impact study guidelines that consider all modes of travel and establish clear guidance for analysis and significance criteria (Action 29.2.B and Policy 29.3.).

The General Plan was updated in 2011 to guide future decision-making in the City. The common vision is to be a highly livable place that is safe, has a strong sense of identity, and offers economic opportunity. As part of this vision, the City values a transportation network that supports mobility for all users, including drivers, transit users, bicyclists, and pedestrians of all ages and abilities.

For projects that are consistent with the General Plan, the impact analysis is generally limited to an evaluation of the project access points [\(including nearby intersections\)](#) and connectivity to the existing adjacent bicycle, pedestrian, vehicle, and transit facilities (see Sections 2 and 3 for details). [Projects consistent with the General Plan that are large and/or unique \(e.g., located on a corridor that is not part of the City's LOS-exempted corridor list\) may trigger an expanded analysis subject to the determination of the City engineer. **If a project**](#)

is inconsistent with the General Plan, these guidelines do not apply, and a consultation with the ~~Planning~~ Community Development and General Services Departments is required.

The General Plan vision is supported by eight planning principles, with the two most relevant listed here:

Mobility: Increasing traffic, much of it from outside the City, will exacerbate congestion on the City's major roadways and also result in cut-through travel through residential neighborhoods, higher vehicle speeds and increased noise levels. Solutions could include street improvements, fixed-route transit (i.e., connecting key commercial districts), and improved bicycle and pedestrian routes. Where appropriate, streets should be completed and connected. In the past, roadways were viewed primarily for automobile travel. This viewpoint has evolved to one where roads are seen within a complete streets context, where the needs of all travel modes, users, and ability levels are equally important.

Sustainability: The City should promote efforts to improve communitywide sustainability for both the existing built environment and new development. Building and site design and construction practices should include energy, water, and other conservation techniques that reduce the consumption of natural resources. In addition, the City should support a transition to cleaner, more renewable energy sources. The City should implement measures to improve air quality and reduce greenhouse gas emissions.

Section 4 contains specific references to relevant transportation and mobility policies of the General Plan.

2. TRIGGERS REQUIRING AN IMPACT STUDY

Unless waived by the City [Engineer](#), a TIS is required when any one of the following conditions is met:

- The project has the potential to create a significant environmental impact under CEQA (check Table ~~67~~ for a list of significance thresholds for all modes).
- The proposed project has the potential to generate 200 new [\(i.e., accounts for pass-by trips attracted to certain retail uses, see Section 3\)](#) passenger vehicle trips per day, ~~or an equivalent number of truck trips according to the table below (see Appendix F for FHWA vehicle classification definitions).~~

Vehicle and Truck Trip Equivalencies		
Vehicle Classification	Description	Trigger for a TIS (New Vehicle Trips Per Day)
Auto	2-axes	200
Small Truck	2-axes/6-tires (includes buses)	50
Medium Truck	3- & 4-axes	20
Large Truck	5-plus-axes	5

- The project requires a permit application, which is subject to discretionary approval.
- The project will substantially alter physical or operational conditions on a City roadway, bikeway, sidewalk, or other transportation facility.
- The project ~~may adversely also~~ affects [transportation](#) safety.

In general, a [prepared TIS report](#) is applicable for two years. After two or more years of inactivity, an updated TIS may be required.

Does my project require a transportation impact study?

Actions That May Be Subject to CEQA

- Infrastructure construction
- Adoption of an ordinance or resolution
- Land use changes
- Funding from public agency contracts, subsidies, and loans
- Issuance of a lease, permit, license, certificate, or other entitlement

If the proposed project includes any of the above actions, an Initial Study (IS) should be prepared to determine the appropriate environmental clearance documents, such as an Environmental Impact Report (EIR).

PROJECT DEFINITION

The applicant shall provide a project description that, at a minimum, includes the following:

- Specific land uses intended for the site [including a detailed project site plan](#).
- Size or intensity of the proposed development [and uses](#) (e.g., square footage, acreage, dwelling units, tonnage, [number of employees or residents](#), etc.).

~~Documentation~~ ~~City staff will then normally determine whether to inform the City whether~~ the project generates 200 or more [new](#) passenger vehicle trips per day. Refer to Table 1 for typical project trip generation estimates [that reach this threshold](#). ~~City staff will also determine whether the project may have adverse affects of~~ [an off-site transportation facilities or services including transit, roadways, bikeways, and sidewalks](#).

~~Documentation to inform the City whether the project will affect off-site transportation facilities or services including transit, roadways, bikeways, and sidewalks (see discussion of multimodal analysis on page 22 and Table 7 on page 32 for more information about potential multimodal impacts).~~

~~An~~ [This detailed and accurate information is critical to](#) ~~accurate project description will help~~ determine if a TIS is required based on potential significant environmental impacts or trip generation.

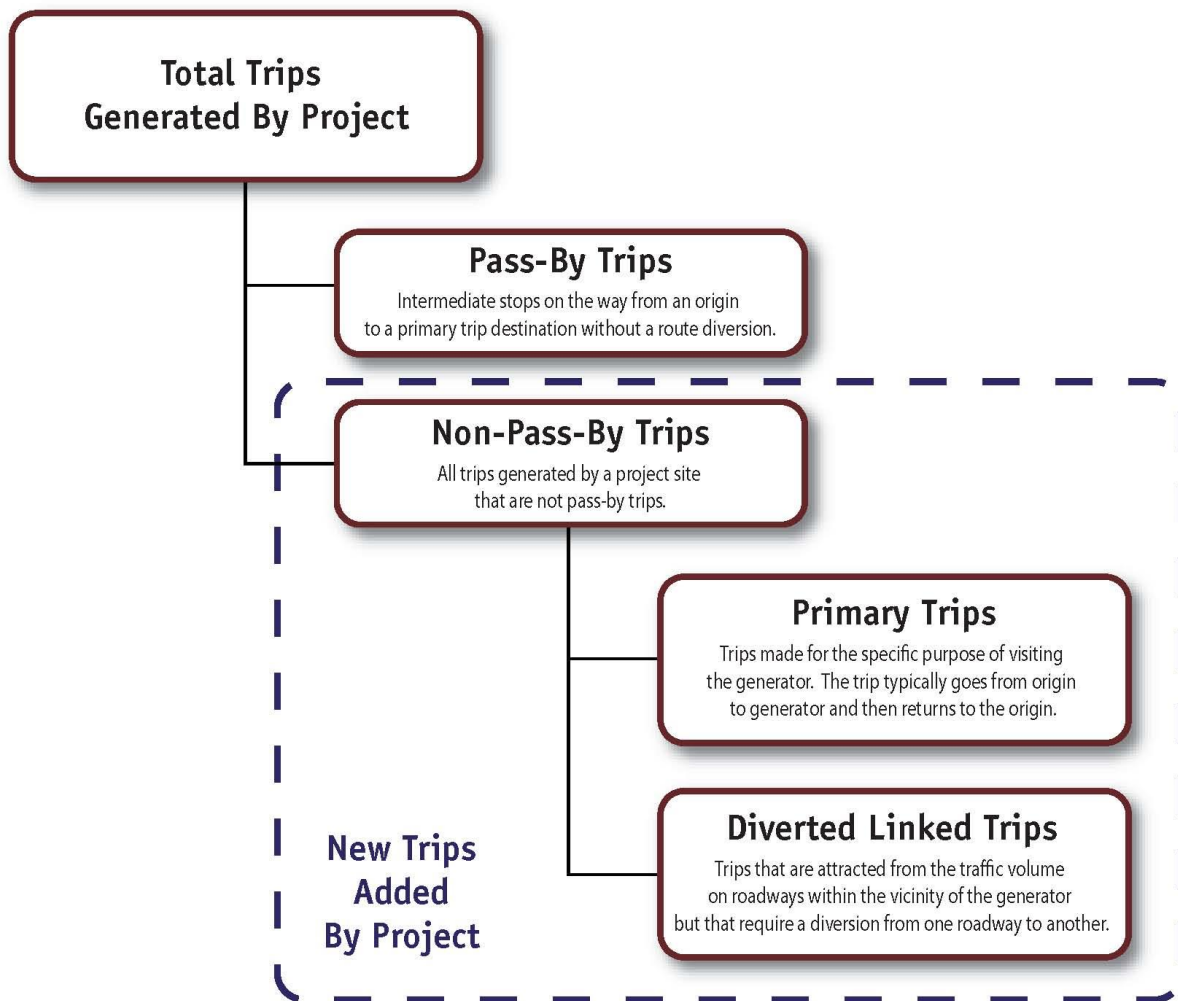
TABLE 1: [QUANTITY OF LAND DEVELOPMENT THAT TRIGGERS 200 DAILY TRIP THRESHOLD](#) ~~TYPICAL DAILY TRIP GENERATION ESTIMATE FOR COMMON PROPOSED DEVELOPMENT TYPES~~

Proposed Development Example (ITE Number Land Use Code)	ITE Daily Trip Generation New Daily Trips Generated ^{1,2}
Single Family Detached Housing (210) – 20 22 dwelling units	208 191
Multifamily Housing Mid-Rise (221) – 37 dwelling units	201
Specialty Shopping Retail Center (820 14) – 7 5 KSF leasable area	219 222
General Office Building (710) – 21 10 KSF gross floor area	420 510
High-Turnover (Sit-Down) Restaurant (932) Fast Food with Drive-Through Window (934) – 2.5 1 KSF gross floor area	218 496
Medical-Dental Office Building (720) – 6 KSF gross floor area Nursing Home (620) – 50 beds	209 119
¹ Value Trip rates based on data published in does not consider by-pass trips Trip Generation Manual, 10th Edition (ITE, 2017). ² KSF = 1,000 square feet Assumes pass-by percentages of 17% for shopping center and 22% for high-turnover sit down restuarant, based on the premise that the daily pass-by rate is half of the PM peak hour pass-by rate contained in the Trip Generation Handbook, 3rd Edition (ITE, 2017). KSF = 1,000 square feet	

|

TRIP GENERATION

~~All applicants are required to submit a~~The trip generation analysis ~~shall identify~~that identifies the number of new daily and peak hour vehicle-trips added by the proposed project. The trip generation estimation for all new or proposed development projects shall include the summation of primary trips, pass-by trips, and diverted linked trips. The following figure describes trip types relevant to trip generation and the difference between the total trips generated by the project versus new trips added by the project.



The estimation of new trips generated by the proposed development project may include credit for trips associated with existing uses on the site. Existing uses are typically those actively present on the project site at the time data is gathered for the traffic impact study.

The final estimate of new daily and peak-hour trips associated with a proposed development project should represent the net contribution of the proposed project. The City will review the trip generation analysis and determine if additional analysis is required.

Trip generation analysis should be primarily based on trip generation rates derived from local empirical data. Recognizing that this is not always possible, ~~applicants may use~~ the most recent version of the Institute of Transportation Engineers (ITE) *Trip Generation Manual*³ and recommendations provided in the *Trip Generation Handbook* should be used.⁴ If multiple trip generation rate sources exist, the study shall provide a comparison and use the rates that best reflect local conditions and applicable regulatory constraints.

The project trip generation rate cannot be based solely on one nearby or similar land use facility. The sample used for non-standard trip generation rates shall include at least three similar facilities in Citrus Heights or neighboring jurisdictions with similar characteristics.

If the study involves comparable sites located in other communities, ~~the applicant must demonstrate to the satisfaction of the City that the~~ chosen sites and uses to be studied should be reasonably equivalent to the site and use proposed within the City.

The final trip generation rates used for the project should be a weighted average of the various trip generation rates available. A tabular summary of the final trip generation rate calculation shall

Establishing Trip Generation for an Unknown Use

Option 1:

In the case of "shell" buildings with unidentified uses or where the ultimate tenant use of the building cannot be restricted, the City Engineer will likely recommend the use of the highest traffic intensity among all permitted uses to establish traffic impacts and to calculate project impact fees.

Option 2:

Traffic impacts may be assessed based on a use with lower traffic intensity if the City and the developer establish a trip budget threshold. The trip budget will be monitored by the Planning Department. Additional evaluation is required if the trip budget will be exceeded by a proposed project.

¹ *Trip Generation Manual*, 10⁹th Edition, Institute of Transportation Engineers, ~~2017~~, Washington, D.C., ~~2012~~

² *Trip Generation Handbook*, ~~3rd~~ ^{2nd} Edition; *An ITE Recommended Practice*, Institute of Transportation Engineers, ~~Washington, D.C., June 2004~~ 2017.

be provided. Appendix A provides sample trip generation calculations.

3. SCOPE OF THE STUDY

The contents and extent of a transportation impact study depend on the location and size of the proposed development, the prevailing conditions in the surrounding area, and the technical questions being asked by decision makers and the public.

STUDY AREA

Defining a study area needs to be ~~done~~ developed through consultation with City staff ~~through a process~~ that results in substantial evidence (facts, analysis, etc.) ~~that supports~~ supportings the study area delineation. The boundary should extend as far as any potential CEQA impact might occur, including across jurisdictional boundaries. The City must approve study locations before traffic data collection and analysis commences. Careful consideration of all modes and facilities (i.e., transit, pedestrian, bicycle, vehicle, rail crossings, etc.) is required when selecting the study area boundary. The study area should be viewed as the "area of influence" of a specific project. The extent of the study area should be determined based on consultation with City staff and the following guidance:

- ~~• If the project is of statewide, area-wide, or regional significance as defined in Section 15206 of the CEQA Guidelines, then the study area shall consider highways and rail facilities within 10 miles of the project site.~~
- For potential impacts to pedestrian facilities, the study area should be a minimum of ½ mile.
- For potential impacts to bicycle facilities, the study area should be a minimum of two miles.

Additional facilities may be studied based on circumstances unique to the site. ~~Applicants should consult with the City early regarding any additional study locations based on local or site-specific issues,~~ especially those related to pedestrians, bicycles, and transit.

TRANSPORTATION ANALYSIS SCENARIOS

The potential transportation analysis scenarios are listed below. ~~Most isolated or small~~ P-projects consistent with the General Plan will be required ~~only~~ only to complete the Baseline Conditions analysis. ~~Larger projects and projects near other potential development projects may be required to analyze both Baseline and Near Term Conditions.~~ Future Conditions would typically be required for projects that are proposing general plan changes or at the discretion of the City engineer depending on the project type and location.

BASELINE CONDITIONS

- **Baseline Conditions** represents ~~ed by~~ ed by transportation conditions for all travel modes in the study area based on recent field observations. The baseline condition is often the existing condition unless there is an impending planned transportation or land use projects that will be in place prior to the

proposed project is constructed. Traffic volumes for roadway analysis should be based on recent count data⁵. For CEQA compliance, the transportation impact analysis should include a description of the physical environmental condition in the vicinity of the project, as they exist at the time of the notice of preparation is published, or if no notice of preparation is published, at the time environmental analysis is commenced, from both a local and regional perspective (CEQA Guidelines Section 15125(a)).

- **Baseline Plus Project Conditions** represented by project changes to baseline transportation conditions for all travel modes in the study area. ~~Traffic volume forecasts for roadway analysis should reflect baseline conditions plus traffic generated by the proposed project.~~ For re-use or conversion projects, this will involve accounting for any existing use of the site that remains or will be discontinued. Per SB 743, any analysis of roadways or intersections performed as part of the study would not be included in the environmental document, but may be included in a staff report so that the public and decision-makers are aware of the project's effects.

~~NEAR-TERM CONDITIONS~~

- ~~**Baseline Plus Approved Projects (BPAP) Conditions** represented by changes to baseline transportation conditions for all travel modes in the study area resulting from approved projects. Traffic volume forecasts for roadway analysis should reflect existing conditions plus growth due to approved development (this scenario may be skipped if the study area has limited or no approved developments).~~
- ~~**BPAP Plus Project Conditions** represented by changes to the BPAP conditions caused by the proposed project (this scenario may be skipped if the study area has limited or no approved developments).~~

FUTURE CONDITIONS

- **Cumulative No Project Conditions** represented by transportation conditions for all travel modes in the study area reflecting all approved projects plus pending projects or expected development of other areas of the City designated for growth. In most cases, the project site will likely be vacant under this scenario. In some cases though, this scenario may need to account for any existing uses on the site that could continue and potential increases in development allowed by ministerial approvals only.
- **Cumulative Plus Project Conditions** represented by Cumulative Conditions plus changes to these conditions caused by the proposed project. This scenario needs to account for whether the project is changing any existing or planned land uses on the site.

⁵ At the time this set of guidelines was being prepared, the COVID pandemic was still causing substantial changes in travel behaviors. Consequently, analyses are continuing to rely on "Pre-COVID" traffic volumes, which are available from the City's online count database, historic counts from vendors, and "big data" from Streetlight, Data, Inc.

Additional analysis scenarios may be required in the traffic impact analysis dependent on project conditions and setting. For example, other scenarios may be needed to test phasing or other interim conditions, at the discretion of the City.

TRANSPORTATION ANALYSIS TIME PERIODS

The determination of analysis time periods will depend on the travel modes being evaluated. For non-auto travel modes, the analysis may include daily, peak period, or peak hour conditions. Final determination shall be made in consultation with City staff. For roadway analysis, General Plan Policy 29.2 states that peak hour traffic volumes will be used in determining compliance with the vehicle LOS standard. These may include weekday AM, Mid-Day, and PM hours as well as Saturday Mid-Day or PM peak hours.

Based on the land use of the proposed project and upon consultation with City, the study should analyze traffic operations during the peak hour of the following time periods:

- Weekday morning peak (7:00 – 9:00 AM)
- Weekday evening peak (4:00 – 6:30 PM)

For some projects, the City may substitute or require additional peak hour analysis for the following time periods:

- Weekday afternoon [or school](#) peak (2:00 – 4:00 PM)
- Friday evening peak (5:00 – 7:00 PM)
- Weekend midday peak (11:00 AM – 1:00 PM)
- Weekend evening peak (4:00 – 7:30 PM)

The determination of study time periods should be made separately for each proposed project based upon the peaking characteristics of project-generated traffic and peaking characteristics of the adjacent street system and land uses. The time period(s) that should be analyzed are those that exhibit the maximum combined level of project-generated traffic and adjacent street traffic.

CONSULTATION WITH OTHER JURISDICTIONS

If the study area overlaps with other jurisdictions, the other jurisdictions must be consulted to verify study locations and to specify the impact significance criteria that should be used in the TIS for these locations. Section 15086 of the CEQA Guidelines shall be followed as the basis for satisfying consultation requirements. [Although no longer required by CEQA, a limited/focus amount of intersection operations analysis in another jurisdiction may be warranted under certain conditions subject to the final determination of the City](#)

~~engineer. In some cases, overlap will occur for roadway system analysis. Section 4 and Appendix C contain information on LOS policy thresholds for Caltrans, unincorporated Sacramento County, unincorporated Placer County, and the City of Roseville.~~

MAJOR COMPONENTS OF THE STUDY

The extent and complexity of a transportation impact study can vary greatly. Table 2 provides basic transportation and circulation elements that shall be acknowledged in every project requiring a TIS. Table 3 in Section 4 identifies relevant policies by each element. Specific analysis methodologies and significance criteria for each of the listed elements are described in further detail in Sections 5 and 6. Communicating the transportation impact study results is as important as the analysis itself. Effective graphics, charts, and simulations are often necessary to successfully communicate analysis results to decision makers and the public.

TABLE 2: TRANSPORTATION AND MOBILITY ELEMENTS ADDRESSED IN AN IMPACT STUDY

Elements	Evaluation
On-site Circulation	Review and evaluate site access locations, driveway throat depths, size of major circulation features with respect to operations and safety, turning movement volumes at site access points, queuing at site access driveways, adequacy of passenger drop-off and pick-up areas , dimensions of truck loading areas, truck delivery routes , and emergency access. Address and accommodate pedestrian and bicycle access. See Appendix B-D for a sample.
Off-Site Traffic Operations	Study all roadway facilities using methods and procedures contained in the latest version of the <i>Highway Capacity Manual</i> (HCM).
Bicycle Facilities	Identify any existing or planned bicycle facilities that may be affected by the project. Focus on maintaining or enhancing connectivity and completing network gaps.
Pedestrian Facilities and Americans with Disabilities Act (ADA) compliance	Identify any existing or planned pedestrian facilities that may be affected by the project. Focus on maintaining or enhancing connectivity, completing network gaps, and removing barriers. Disclose evaluation and documentation of project features (e.g., road widening) with likely disparate impact on pedestrians (e.g., longer crossing time).
Parking	Compare the project parking plan with City standards.
Trucks (or other heavy vehicles)	For truck traffic generating projects, identify the number of truck trips that will be generated, and design facilities necessary to accommodate truck traffic. This will generally require evaluation of the Traffic Index for existing roadways serving the project and an assessment of whether roadways meet current City design standards.
Transit	Identify any existing or planned transit facilities that may be affected by the project. Focus on maintaining or enhancing connectivity, and completing network gaps, and avoiding increases in transit vehicle travel times . For system planning, use crush load as capacity, not seated capacity.

Elements	Evaluation
Intersection Traffic Control	The City allows roundabouts instead of traffic signals or all-way stop control. Evaluate unsignalized intersections located within the study area to determine appropriate traffic control with or without the project.
General and Related Plan Consistency	Evaluate the project against transportation-related goals, policies, and actions set forth in the General Plan, Pedestrian Master Plan , and Bikeway Master Plan . Depending on project type and location, evaluation may be needed for the Old Auburn Road Complete Streets (OARCS) Plan, Carriage/Lauppe Safe Schools Corridor Plan, Multi-Modal Transportation Safety Program (MMTSP) and City Intelligent Transportation System (ITS) Master Plan.
Other Subject Areas	Consider other subject areas on a case-by-case basis.
Other Jurisdictional Requirements	In situations where several agencies must approve a development or are responsible for affected roadways, the applicant must coordination with multiple contact lead and responsible agencies may be necessary to to determine issues to be addressed, finalize scope of work, analysis methods, and assumptions, study, etc. In general, the applicant will be responsible for analyzing project impacts against appropriate jurisdictional thresholds; however, the analysis methodology will be determined by the City in compliance with CEQA and the impacts will be mitigated consistent with City direction.
Safety	Projects subject to CEQA review should be analyzed for potential safety impacts to the state highway system in accordance with the Interim Local Land Development and Intergovernmental Review (LDIGR) Safety Review Practitioners Guidance (Caltrans, December 2020). Safety evaluations may also be performed for City-maintained roadways using the following available documents: City of Citrus Heights Bikeway Master Plan (2015) and City of Citrus Heights Pedestrian Master Plan (2016). By the end of 2021, a Local Road Safety Plan (LRSP) will be completed for the City's transportation system, which may also be utilized in safety evaluations.
VMT	Projects subject to CEQA review should be analyzed for VMT impacts in accordance with the SB 743 Implementation Guidelines for City of Citrus Heights (2021). This document includes an extensive discussion that defines VMT, describes how per capita and per employee VMT is estimated, presents recommended significance criteria for VMT impacts, and identifies feasible mitigation measures for significant VMT impacts.

4. RELEVANT POLICIES

An important aspect of a TIS is to provide sufficient information for the City to determine that a project is consistent with the General Plan and other applicable City plans. As such, individual projects must be reviewed against relevant policies contained in the General Plan. Applicants with a project within the [Sunrise Tomorrow Specific Plan](#), Stock Ranch Guide for Development, or Boulevard Plan areas should also confirm applicable policies from these plans with the City.

Table 3 lists the most common policies associated with each element of a TIS in an abbreviated fashion. Applicants should review the full policy statement in the General Plan.

TABLE 3: PRIMARY TRANSPORTATION AND MOBILITY GENERAL PLAN POLICIES

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
On-site Circulation	<ul style="list-style-type: none">➤ Policy 29.4: Support safe, complete and well-connected neighborhood street, bicycle, and pedestrian access and connections that balance circulation needs with the neighborhood context.➤ Policy 29.8: Minimize the number of access points along arterial roadways, including by consolidating or relocating driveways to provide for more efficient traffic movement.➤ Policy 30.3: Discourage the construction of private streets to ensure full public access to the City circulation system.➤ Policy 6.5: Consider the neighborhood context when developing traffic calming devices for established rural neighborhoods➤ Policy 13.1: Improve mobility in the Sunrise MarketPlace area to provide adequate access for vehicles, transit, bicycles and pedestrians.➤ Policy 13.2: Create convenient connections across Sunrise Boulevard for vehicles, bicycles, pedestrians and transit.	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Off-Site Traffic Operations	<ul style="list-style-type: none"> ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 3.5 and 7.6: Plan, design, and construct neighborhood and rural residential streets to encourage walking and bicycling while discouraging high vehicle speeds and volumes consistent with Policy 29.1. ➤ Policy 6.1: Provide public improvements that are appropriate and compatible with the unique qualities of the City's rural neighborhoods. ➤ Policy 6.5: Consider the neighborhood context when developing traffic calming devices for established rural neighborhoods. ➤ Policy 29.2: Measure customer satisfaction related to vehicle travel using level of service (LOS) according to procedures in the latest version of the Highway Capacity Manual published by the Transportation Research Board. The City will strive to achieve LOS E or better conditions for City roadways and intersections during peak hours, with exception of the exempt locations identified in this policy. ➤ Policy 53.2: Minimize the impacts of vehicle emissions on air quality. ➤ Policy 29.4: Support safe, complete and well-connected neighborhood street, bicycle, and pedestrian access and connections that balance circulation needs with the neighborhood context. ➤ Policy 30.2: Require public street right-of-way dedications and improvements as development occurs. 	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Bicycle Facilities	<ul style="list-style-type: none"> ➤ Policy 30.1: Improve aesthetic features along the City's roadways and maintain landscaping in an efficient and timely manner especially when it enhances the walking and biking environment. ➤ Policy 19.1: Promote improvements to major corridors to make them more distinctive and inviting. Encourage installation and maintenance of landscaping in median and street frontages along arterial roadways. ➤ Policy 19.2: Establish a street tree planting program for major corridors. ➤ Policy 19.3: Require landscaping on commercial, residential, and institutional uses adjacent to all public street frontages. ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 29.1 – Action B: Evaluate projects to ensure that the safety, comfort, and convenience of pedestrians and bicyclists are given equal level of consideration to drivers. ➤ Policy 29.1 – Action C: Consider ways to increase and improve travel choices when reviewing development or transportation infrastructure projects. ➤ Policy 29.1 – Action E: Improve the existing street network to minimize travel times and improve mobility for transit, bicycle, and walking trips between new projects and surrounding land uses to reduce vehicle trips. ➤ Policy 3.5 and 7.6: Plan, design, and construct neighborhood and rural residential streets to encourage walking and bicycling while discouraging high vehicle speeds and volumes consistent with Policy 29.1. ➤ Policy 13.1: Improve mobility in the Sunrise MarketPlace area to provide adequate access for vehicles, transit, bicycles and pedestrians. ➤ Policy 13.2: Create convenient connections across Sunrise Boulevard for vehicles, bicycles, pedestrians and transit. ➤ Policy 54.1: Encourage alternative modes of transportation and trip-reducing strategies. <p><u>Policies and planned improvements contained within the City of Citrus Heights Bikeway Master Plan (2015) should also be reviewed.</u></p>	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Pedestrian Facilities and Americans with Disabilities Act (ADA) compliance	<ul style="list-style-type: none"> ➤ Policy 30.1: Improve aesthetic features along the City's roadways and maintain landscaping in an efficient and timely manner especially when it enhances the walking and biking environment. ➤ Policy 19.1: Promote improvements to major corridors to make them more distinctive and inviting. Encourage installation and maintenance of landscaping in median and street frontages along arterial roadways. ➤ Policy 19.2: Establish a street tree planting program for major corridors. ➤ Policy 19.3: Require landscaping on commercial, residential, and institutional uses adjacent to all public street frontages. ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 29.1 – Action B: Evaluate projects to ensure that the safety, comfort, and convenience of pedestrians and bicyclists are given equal level of consideration to drivers. ➤ Policy 29.1 – Action C: Consider ways to increase and improve travel choices when reviewing development or transportation infrastructure projects. ➤ Policy 29.1 – Action D: Require sidewalks on all arterial and collector streets. Where feasible, separate sidewalks from streets on arterials and collectors with landscaping including a tree canopy to create shade ➤ Policy 29.1 – Action E: Improve the existing street network to minimize travel times and improve mobility for transit, bicycle, and walking trips between new projects and surrounding land uses to reduce vehicle trips. ➤ Policy 3.5 and 7.6: Plan, design, and construct neighborhood and rural residential streets to encourage walking and bicycling while discouraging high vehicle speeds and volumes consistent with Policy 29.1. ➤ Policy 6.6: Support development of "safe routes" to school for children residing in rural neighborhoods. ➤ Policy 13.1: Improve mobility in the Sunrise MarketPlace area to provide adequate access for vehicles, transit, bicycles and pedestrians. ➤ Policy 13.2: Create convenient connections across Sunrise Boulevard for vehicles, bicycles, pedestrians and transit. ➤ Policy 13.4: Facilitate the development of new buildings in areas currently devoted to parking to shorten distances between buildings and foster better pedestrian connections between shopping centers. ➤ Policy 30.4: Maintain street and sidewalks in rural residential areas that balance circulation needs and compatibility within the surrounding neighborhoods. ➤ Policy 54.1: Encourage alternative modes of transportation and trip-reducing strategies <p>Policies and planned improvements contained within the City of Citrus Heights Pedestrian Master Plan (2016) should also be reviewed.</p>	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Parking	<ul style="list-style-type: none"> ➤ Policy 13.4 – Action A: Consider establishment of a maximum parking ratio for the MarketPlace area that recognizes the value of land for additional development that can increase shopping and employment opportunities while also improving the convenience of walking, bicycling, and using transit. 	
Trucks	<ul style="list-style-type: none"> ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 29.5: Encourage movement of goods by truck on freeways and other appropriate designated routes. 	
Transit	<ul style="list-style-type: none"> ➤ Policy 31.1: Strive to increase fixed-route and demand responsive (i.e., paratransit) transit service coverage and frequency to Citrus Heights residents and employees. ➤ Policy 31.2: Strive to provide public transit that is an attractive, convenient, dependable and safe alternative to the automobile. ➤ Policy 31.4: Require new development to provide transit enhancements, (including, but not limited to bus pull-outs and bus shelters) where appropriate, that decrease transit travel times, improve access to transit stops, or improve the amenities, security, or travel information at transit stops. ➤ Policy 13.3: Promote installation of additional, distinctive transit stops at key activity areas and encourage covered shelters at existing and new stops. ➤ Policy 8.3: Support the creation of transit centers near Greenback Lane/Sunrise Boulevard and Greenback Lane/Auburn Boulevard. ➤ Policy 29.1: Strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use. ➤ Policy 29.1 – Action C: Consider ways to increase and improve travel choices when reviewing development or transportation infrastructure projects. ➤ Policy 29.1 – Action E: Improve the existing street network to minimize travel times and improve mobility for transit, bicycle, and walking trips between new projects and surrounding land uses to reduce vehicle trips. ➤ Policy 13.1: Improve mobility in the Sunrise MarketPlace area to provide adequate access for vehicles, transit, bicycles and pedestrians. ➤ Policy 13.2: Create convenient connections across Sunrise Boulevard for vehicles, bicycles, pedestrians and transit. ➤ Policy 13.5: Promote transit-oriented development through reuse and redevelopment of opportunity sites near the Greenback Lane/Sunrise Boulevard intersection. ➤ Policy 54.1: Encourage alternative modes of transportation and trip-reducing strategies 	

Element	General Plan Transportation and Mobility Policy	Project Applicability (Check here)
Intersection Traffic Control	<ul style="list-style-type: none"> ➤ Policy 32.1: Evaluate and utilize technologies that can improve the performance, reliability, and safety of the transportation system (such as signal coordination, centralized traffic control, red-light cameras, and real-time travel information). ➤ Policy 53.1 – Action D: Synchronize traffic signals on roads susceptible to high emission levels from idling vehicles. ➤ Policies and planned improvements contained within the City of Citrus Heights ITS Master Plan should also be reviewed. 	
General Plan Consistency	<ul style="list-style-type: none"> ➤ Review other elements of the General Plan for applicable policies, especially the Community Development and Community Health chapters. 	
Other Subject Areas	<ul style="list-style-type: none"> ➤ Policy 10.8: Discourage concentration of auto intensive facilities (such as drive through and gas station uses) and ensure that drive-through businesses are allowed only where compatible with the surrounding areas. ➤ Policy 3.5 – Action A: Regulate development to limit traffic on new local residential streets to 3,000 vehicles per day. 	
Other Jurisdictional Requirements	<ul style="list-style-type: none"> ➤ Policy 29.6: Collaborate with neighboring jurisdictions when updating the General Plan and preparing the Capital Improvement Program to work toward providing a regional Complete Streets transportation network for all modes. ➤ Policy 33.2: Establish formal and informal processes with regional agencies, the City of Roseville, Sacramento County, and Placer County to review and provide input on proposed development within one-half mile of the City limits. ➤ Policy 33.1: Maintain open communication and cooperation with all public agencies that serve residents and businesses in Citrus Heights. 	

VEHICLE LEVEL OF SERVICE

Historically, vehicle ~~level of service (LOS)~~ thresholds have been the prevailing criteria applied to transportation projects. The City of Citrus Heights recognizes that vehicle LOS is one performance measure that needs to be carefully weighed against other City objectives to balance the preservation of community neighborhood values with a safe and efficient circulation system. The City's vehicle LOS standard is described in detail in the Community Development element of the General Plan (Policy 29.2) and is summarized to the right. Objectives or thresholds for other transportation modes have also been established and are addressed in these guidelines in Table ~~67 on page 32~~.

If the TIS study area extends into an adjacent jurisdiction, their LOS threshold shall also be used to determine deficient operations ~~for the impact significance criteria for analysis locations in that jurisdiction. Appendix C contains the LOS policies and thresholds for Caltrans, incorporated cities, and unincorporated counties in the vicinity of Citrus Heights. Applicants are required to verify LOS thresholds for study area intersections and roadways.~~ The General Plan also states that LOS exceptions may be allowed on a case-by-case basis, where ~~mitigation improvements are~~ is infeasible or would conflict with other community values.

City of Citrus Heights Vehicle LOS Policy

Measure customer satisfaction related to vehicle travel using LOS according to procedures in the latest version of the Highway Capacity Manual published by the Transportation Research Board. The City will strive to achieve LOS E or better conditions for City roadways and intersections during peak hours (these may include weekday AM, Mid-Day, and PM hours as well as Saturday Mid-Day or PM peak hours). The intent of the policy is to effectively utilize the roadway network capacity while balancing the desire to minimize potential adverse effects of vehicle travel on the environment and other modes.

Exceptions to LOS E are allowed at certain locations as specified in Policy 29.2 (see pages 35 and 36 in these guidelines for more details).

Source:

*Citrus Heights General Plan
Policy 29.2*

5. ANALYSIS METHODOLOGY

This section provides data collection and analysis procedures for conducting transportation impact studies in Citrus Heights. The City is committed to a balanced analysis for all modes of travel. The methodology presented in this section includes robust data collection and analysis techniques for pedestrian, bicycle and transit networks, in addition to vehicle circulation.

TRANSPORTATION DATA COLLECTION

Accurate data is essential to achieve a high level of confidence in transportation analysis results. Existing traffic conditions data should be collected using the guidelines set forth in Table 4.

TABLE 4: BASELINE CONDITIONS DATA COLLECTION PROTOCOL

Data Set	Procedure
Peak period turning movement counts	<p>Collect data for all study intersections on a Tuesday, Wednesday, or Thursday during weeks without holidays, large special events, or heavy construction in the study area that results in temporary travel pattern shifts. Fall or Spring days without rain and when school is in session are preferred.</p> <ul style="list-style-type: none"> ➤ Care should be taken to collect data on days when schools are in session. ➤ Consult with the City to determine if adjustments are necessary to account for seasonal variation in traffic volumes. ➤ <u>Due to the effects of the COVID pandemic on travel, existing traffic counts should be representative of pre-COVID conditions so long as travel continues to be affected. This is often accomplished by purchasing historic counts from vendors or by purchasing anonymous "big data" from Streetlight Data, Inc. or other vendors. Once a new post-COVID condition is reached, new traffic counts may be used to represent then current conditions. Careful coordination with City staff is required to determine the appropriate count timeframe. Traffic counts shall not be used if more than two years old at study initiation. If available, City counts may be used but the traffic counts must be adjusted to reflect current year traffic volumes and patterns.</u> ➤ Bicycles and pedestrians should be included in all counts. ➤ Some projects may require vehicle classification or occupancy counts. Consult with the City on a case-by-case basis.
Daily traffic counts	Collect data for all study roadway segments using the parameters described above for peak period turning movement counts with the exception of collecting bicycle and pedestrian data.
Roadway geometrics	Establish existing geometrics from a combination of aerial photography, as-built plans, and site visits.
Travel time and speed	Only Collect as necessary <u>(e.g., for microsimulation validation or to document how travel speeds may change)</u> . Collect data using a floating car survey.
Signal timing	Request timing from the City and other operating agencies such as Caltrans or Sacramento County. Verify timing in the field.

Data Set	Procedure
Collision data	Obtain City collision data from City staff and other adjacent agencies as applicable (i.e. City of Roseville). If collision analysis is required on state highway system, obtain from and Statewide Integrated Traffic Records System (SWITRS) or similar database (depending on study purpose), through the local California Highway Patrol or through the following Web site: www.chp.ca.gov/switrs .
Mode split	Summarize daily and peak hour mode split from study area or communities adjacent to study area. Data sources could include the Census journey-to-work survey, the SACOG household travel survey, or other available surveys.
Transit routes and use	Map existing transit routes and stops serving the study area and identify service hours and levels of use . Document amenities (benches, shelters, bicycle parking, etc.) available at transit stops and centers within ¼-mile of non-residential projects and a ½-mile of residential projects.
Bicycle and pedestrian facilities	Map existing bicycle and pedestrian facilities within the study area (include sidewalks, crosswalks, signal heads, push buttons, related signing and striping). Document barriers, deficiencies and high-pedestrian demand land uses including schools, parking, senior housing facilities, and transit stops or centers. Consider using evaluation tools such as www.walkscore.org or similar tools to quantify walkability.

MULTIMODAL ANALYSIS

Evaluate the project's potential adverse effects on transportation facilities and services related to transit, bicycles, and pedestrians. The evaluation could include identification of any disruption to existing facilities and services or interference with the implementation of planned facilities and services. This effort will require identifying and mapping existing facilities. Particular attention should be made to roadway or intersection widening ~~mitigation~~ [improvements and the addition of new driveway locations](#), that would increase pedestrian/bicycle crossing times or increase the potential for vehicle and pedestrian/bicycle conflicts. Consideration should also be given to how a project affects accessibility between each travel mode and the surrounding land uses.

For some projects, more detailed multi-modal analysis may be required. In these cases, the methodology shall be selected in consultation with City staff and should consider new tools, procedures, and performance measures such as those listed below.

- **Multimodal LOS** – The ~~2010~~-HCM contains multi-modal LOS analysis methods ~~and there are approximately 15 other methods in practice around the U.S.~~ Simulation models can also be used to measure performance (i.e., person-delay) for all modes using the transportation network.
- **Activity Connectedness** – Travel time for each mode (vehicles, transit, bicycles, and walking) between the project and surrounding land uses can be used to gauge the degree of accessibility for a project. The City desires to minimize travel time to necessary destinations while minimizing unnecessary vehicle travel.

- ~~Tools such as Index and Walkscore can be used to gauge this measure specifically for walking.~~
The main idea is to evaluate activity centers and destinations around projects to ensure that walk times to necessary destinations are minimized and the walking experience is comfortable.
- **Speed Management** – Desired travel speeds for each mode should be considered in project evaluation where new transportation facilities are being constructed. The City desires roadways to be designed for 40 miles per hour or less on arterial streets and 30 miles per hour or less on collector streets to reduce the severity of collisions and minimize air pollution and greenhouse gas emissions.

TRAFFIC OPERATIONS ANALYSIS

Traffic ~~impacts-conditions~~ shall be analyzed using standard or state-of-the-practice professional procedures for trip generation, trip distribution, and traffic assignment, which can generally be found through organizations such as Institute of Transportation Engineers (ITE), Caltrans, Federal Highway Administration (FHWA), and American Planning Association (APA).

General Plan Policy 29.2 states that vehicle LOS definitions and calculation methods must be consistent with the latest edition of the *Highway Capacity Manual* (HCM). The HCM is published by the Transportation Research Board. The ~~2010-most recent version is the 6th Edition~~ ~~version was~~ published in 201~~6~~⁴.

Analysis Parameters

~~Table 5 provides guidance for use of specific a~~ Analysis parameters (e.g., signal phasing, conflicting pedestrian volumes, etc.). ~~for Baseline and Baseline Plus Project conditions shall be based on field measurements taken during traffic count collection or field observation. This typically applies to Baseline Plus Approved Projects and Baseline Plus Approved Projects Plus Project analysis.~~

~~For new study intersections and under Cumulative conditions, Table 5 provides guidance on state-of-the-practice procedures. Consult with the City regarding other analysis parameters not listed in Table 5.~~

TABLE 5: ANALYSIS PARAMETER RECOMMENDATIONS

Parameter	Recommendation
Peak hour factor (PHF)	<p>A PHF of 1.0 should be used for all analyses that involve City-owned and operated facilities. This approach follows has also been adopted by the Cities of Sacramento and Roseville, and County of Sacramento. By applying a 1.0 PHF, the reported LOS is based on conditions for the entire peak hour, versus the busiest 15-minutes of the peak hour. In selecting a 1.0 PHF, the City concluded that evaluating and reporting conditions (and sizing infrastructure) for the busiest 15-minutes of the day is overly conservative.</p> <p>If analyses are being performed for a Caltrans facility, coordination with District 3 traffic operations staff is recommended to confirm PHF and other assumptions. Use measured approach PHF obtained through traffic data collection. For cumulative scenarios and baseline conditions where peak hour factors are not available, refer to the most recent HCM and maintain consistency through analysis scenarios and peak hours.</p> <p>If a simulation model is used for analysis, the PHF should be applied for the peak 15-minute period. For the remaining analysis periods, if used, the volume should be adjusted to match the total hourly volume.</p>

Parameter	Recommendation
Saturation flow rate	Prior analyses within Citrus Heights have demonstrated that use of the default ideal A field measurement of the saturation flow rate (i.e., 1,900 vehicles per hour per lane) contained in the latest version of the HCM allows for microsimulation models to be validated to prescribed specifications. Under certain circumstances, however, field measurement of saturation flow rates may be warranted. is recommended in accordance with the procedure in the most recent HCM. For cumulative conditions, use the value recommended in the most recent HCM unless physical conditions and traffic controls warrant a change. The 2010 HCM recommends 1,900 vehicles per hour per lane.
Yellow phase	If a traffic signal is present under baseline conditions, use existing yellow phase <u>from city provided timing sheets</u> . For future conditions, consult with the City and consider the most recent version of the California Manual on Uniform Traffic Control Devices (MUTCD).
All red phase	If traffic signal is present under baseline conditions, use existing all red phase <u>from city provided timing sheets</u> . For future conditions, consult with the City and consider the most recent version of the California MUTCD. All red phase may be greater on high-speed roadways.
Conflicting pedestrians for signalized intersections and roundabouts	Should be based on existing pedestrian counts or observations. Otherwise, refer to the most current version of the HCM to determine the amount of pedestrian activations per cycle into appropriate categories.
Traffic signal cycle lengths and phasing	Replicate existing cycle length and phasing (e.g., leading left turns) when possible <u>from city provided timing sheets</u> . For new signalized locations, <u>coordinate with City staff regarding assumed signal phasing, cycle lengths, and coordination with other intersections along the study corridor.</u> segment the cycle lengths into the following three categories unless other cycle lengths can be justified through the traffic operations analysis. Near freeway interchanges/regional commercial — limit signal cycle lengths to less than 120 seconds Other areas — limit signal cycle lengths to less than 90 seconds Ensure that minimum pedestrian and bicyclist crossing times are satisfied per the most recent version of the California MUTCD.
Heavy truck percentages	Based on the existing heavy-truck percentage and adjusted to account for future planned development. In general, heavy-truck percentages should be greater on truck routes and main thoroughfares than on local streets. Minimum recommended value is 3%.
Lane utilization factor	If applicable, adjust lane utilization factors based on field observations. Otherwise, refer to the most current version of the HCM.

Analysis Tools and Methods

Traffic operations analysis ~~for freeways and local roadways~~ should be conducted using tools and methods approved by the City of Citrus Heights.

Intersection Analysis – Traffic Operations

Microsimulation models, such as SimTraffic or VISSIM, should be used at the majority of intersections in the City. These models better consider the effects of signal coordination, upstream/downstream bottlenecks, turn lane overflow effects, imbalanced lane utilization, congested conditions, and other factors than deterministic

models such as Synchro. They also produce 95th percentile queue lengths that are more accurate than deterministic models. In instances, where the intersection is isolated (i.e., not coordinated), under-capacity, and does not have a critical turn movement whose storage is to be evaluated, deterministic models are suitable to use.

Microsimulation analysis requires at least 10 runs to be performed, statistical outliers to be removed, and reported results to be the average of 10 runs. For oversaturated conditions, it may be necessary to perform a 15-minute or 30-minute, pre-peak hour seeding runs so that congested conditions are present at the beginning of the peak hour of analysis. Microsimulation models should be validated to existing conditions using a variety of performance standards such as average travel time, queue lengths, and vehicles served. The level of variance between the ten runs should be checked to ensure that it is not excessive (e.g., the average delay is 90 seconds and variance is 60 seconds would be considered excessive). High variance results are typically indicative of including a run where "a vehicle gets stuck" or randomness inherent to this type of analysis has a material affect on the outcome.

Intersection Analysis – Signal Phasing / Swept Path Analysis

When changes are being proposed at a signalized intersection that would result in changes in signal phasing, the AutoTurn software program should be used to determine whether simultaneous left-turn movements are possible in the event protected left-turn phasing is being considered. Exhibits should be prepared as part of the TIS to illustrate these swept paths (e.g., for a bus or WB-50 type truck). Coordination with City staff is required to determine the appropriate design vehicle(s).

Roundabout Analysis – Traffic Operations

Roundabouts may be analyzed using the Sidra software program or microsimulation analysis. The volume-to-capacity (V/C) ratio should be checked on each approach to the roundabout to verify that they are less than 0.85. Values over 0.85 suggest the potential for lengthy queuing and delays and require supplemental review to determine its appropriateness. Microsimulation would be particularly appropriate if a roundabout is proposed at a busy (with many vehicles, bicyclists, and pedestrians) intersection on an existing four-lane arterial.

Roundabout Analysis – Geometric Review

The geometric configuration of a proposed roundabout should be analyzed to determine its ability to accommodate buses or a truck using the AutoTurn software program. Coordination with City staff is required to determine the appropriate design vehicle(s). Other considerations in the geometric review are the placement of crosswalks, and the required amount of lateral transition to maintain slow speeds through the roundabout.

Table 6 identifies recommended analysis tools. Other tools or methods may be used upon receiving approval from the City Engineer. Special conditions related to congested conditions, state highway facilities, and roundabouts are discussed in more detail below.

TABLE 6: INTERSECTION OPERATIONS ANALYSIS RECOMMENDED ANALYSIS TOOL

Software/ Method	Traffic Studies ¹		Roundabouts		Arterial/ Interchange Operations	Simulation Analysis ⁴		
	Operations ²	Signal Coordination ³				Unique Geometries	Heavily Congested Conditions	Multi- Modal
Synchro/ SimTraffic	X	X	X		X	X		
TRAFFIX	X		X					
HCS	X				X			

Notes: The most current version of analysis software (with updated software patches) should be used.

- 1. Refer to thresholds for identifying if a traffic study is required.
- 2. Appropriate for isolated intersection operations or for signal systems that are not coordinated.
- 3. Mandatory for coordinated signal systems to maximize vehicle progression.
- 4. Should be applied to analyzing operations of congested conditions or non-standard conditions where traditional analytical approaches may not be appropriate.
- 5. Specific software program selection should be conducted in consultation with the City and consider the types of technical questions being asked in the study and the modes to be included.

Congested Conditions

~~The City may require the use of simulation modeling for congested conditions as recommended by the HCM. Since simulation tools can simultaneously evaluate vehicle interactions across a complete network (including the interaction of multiple modes), they can provide a more complete understanding of traffic operating conditions during peak congested periods and what may happen when a specific bottleneck is modified or eliminated.~~

State Highway Analysis

~~In Citrus Heights, the analysis of state highways could include nearby freeways. Freeway analysis will typically include basic freeway segments, ramp junctions, weaving sections, and ramp terminal intersections. HCM methods shall be used for basic freeway segments, ramp junctions, and ramp terminal intersections, but Caltrans has alternative analysis methods for weaving sections as defined in the Caltrans *Highway Design Manual* (HDM Section 504.7). The Caltrans District 3 traffic operations branch should be consulted before beginning any weaving analysis. Analyzing ramp terminal intersections should consider that these intersections are closely spaced in most cases and operate as an integrated set versus as isolated locations.~~

Roundabout Analysis

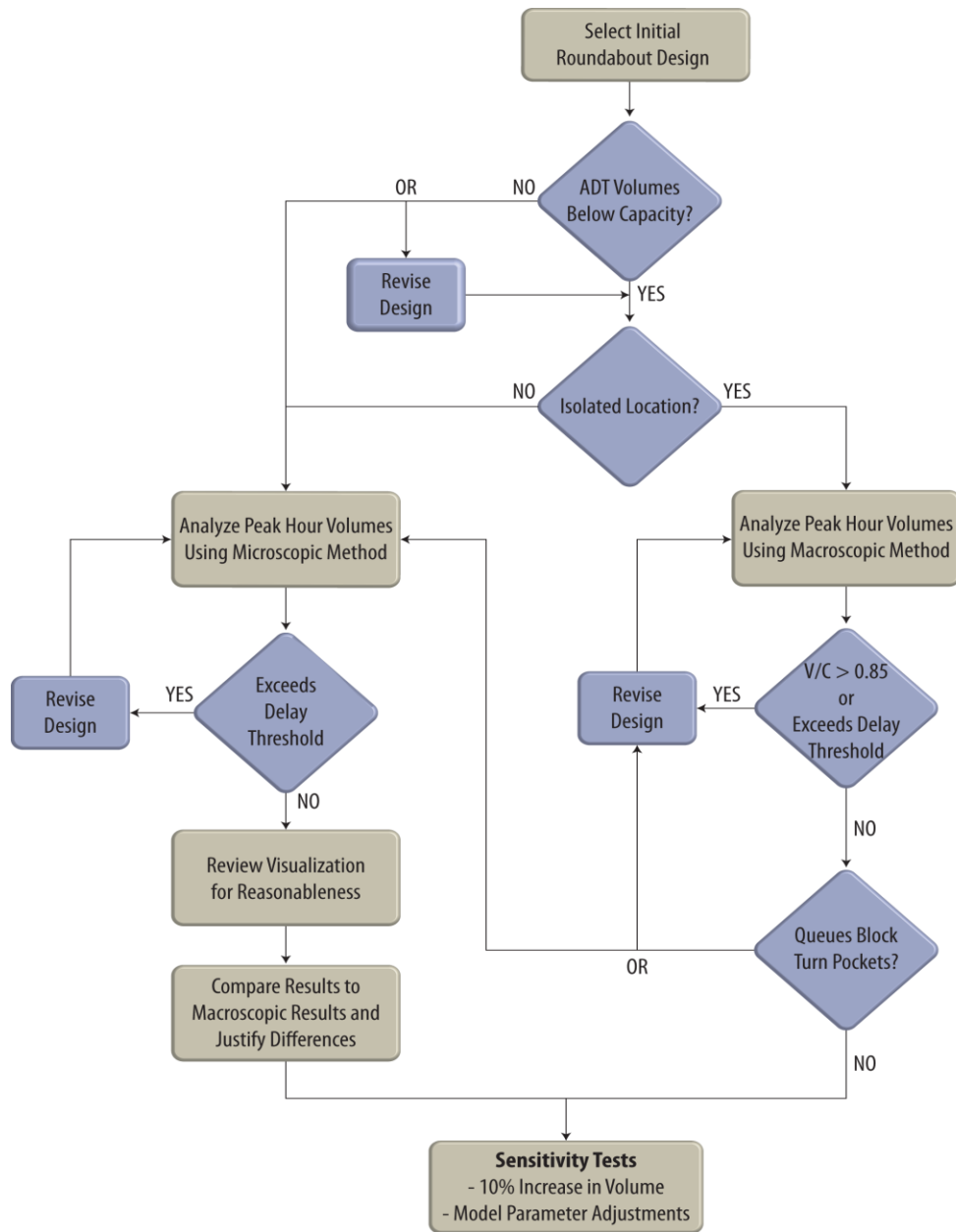
~~*Roundabouts: An Informational Guide (Second Edition)* provides guidance on how to evaluate roundabouts. Roundabout analysis is critical when the intersection is projected to operate at or near capacity. The flowchart on the next page provides a framework for roundabout analysis.~~

~~The framework starts with comparing the forecast daily volumes against certain thresholds to identify if a roundabout is a feasible intersection treatment. If the initial check indicates that the roundabout would be near or at capacity, simulation should be used, or the roundabout should be redesigned to provide additional capacity. If the daily volumes are below the threshold and the intersection is isolated, then a macroscopic analysis (HCM 2010 or SIDRA) may be sufficient.~~

~~Queue lengths should be reviewed to ensure that they do not spill beyond available storage and interfere with overall operations. If vehicles have sufficient storage, then volume to capacity (V/C) ratios should be reviewed to verify that they are less than 0.85. If the V/C ratios are less than 0.85, proceed with sensitivity testing to provide a level of confidence if traffic volumes increase or decrease beyond what was forecasted.~~

~~When comparing roundabout versus signal control at a given location, long-term maintenance costs should be calculated and considered in the evaluation.~~

Roundabout Analysis Flowchart



ON-SITE TRANSPORTATION REVIEW

A detailed site [plan](#) review is required for every project. ~~Consideration~~ [The following site plan elements should be reviewed in detail. Other elements may also be need to be studied depending on the type of project.](#) ~~should be given to the following qualitative and quantitative reviews and summarized in the TIS:~~

- [Driveway spacing, width, permitted turning movements, curb return radii, and sight distance.](#)
- [Minimum required throat depth \(for inbound and outbound travel\) at project driveways based on minimum requirements as described in Section 106 of the City's Municipal Code. For special circumstances, throat depth may be calculated using either microsimulation or the method described in *Estimation of Maximum Queue Lengths at Unsignalized Intersections* \(ITE Journal, 2001\). Use of 95th percentile queue lengths from Synchro is discouraged due to known limitations in accuracy.](#)
- [Need for deceleration lanes at driveways.](#)
- ~~Truck turning movement adequacy including refuse, delivery trucks, etc.~~ ~~Existence of any current traffic problems in the local area such as a high accident location, non-standard intersection or roadway, or an intersection in need of a traffic signal.~~
- [Presence/adequacy of](#) ~~Applicability of context-sensitive design practices compatible with adjacent neighborhoods or other areas that may be impacted by the project traffic.~~ [pedestrian paths from the project site to adjacent sidewalks.](#)
- [Need for median treatments along adjacent roadways to accommodate project access.](#)
- ~~Close proximity of proposed site driveway(s) to other driveways or intersections.~~
- ~~Adequacy of vehicle parking relative to both the anticipated project demand and zoning code requirements.~~
- ~~Adequacy of the project site design to fully satisfy truck loading demand on-site, when the anticipated number of deliveries and service calls may exceed 10 per day.~~
- [Review of on-site circulation including width of drive aisles, parking lot layouts, drive-through lane storage, pedestrian pathways, and other circulation-related considerations.](#)

[Appendix B](#) includes several sample illustrations of site plan review recommendations.

- ~~Adequacy of the project site design to provide at least the minimum required throat depth at project driveways.~~
- ~~Adequacy of the project site design to convey all vehicle types.~~
- ~~Adequacy of on-site vehicle, bicycle, and pedestrian circulation and provision of safe pedestrian paths from residential areas to school sites, public streets to commercial and residential areas, and the project site to nearby transit facilities.~~
- ~~Project site design resulting in inadequate emergency access or response times.~~

Parking

Coordination with City staff is required to determine if analysis of the project's parking supply is required. In many instances, City staff will verify that the project's parking supply is consistent with requirements from the City's Municipal Code. However, in special cases, a focused parking supply and demand analysis may be warranted as part of the TIS. This could include projects that propose less parking than the code requires or projects that propose a mix of complementary mix of land uses that provide opportunities for shared parking. In the latter case, the latest version of the Shared Parking spreadsheet (jointly published by the Urban Land Institute and National Parking Association in 2020) should be used.

~~Appendix D includes a sample illustration of site review recommendations.~~

TRAFFIC FORECASTS ~~AND VMT ANALYSIS~~

Most TISs require one or both of the following types of traffic forecasts of the roadway system:

1. "Plus Project" Traffic Forecasts
2. Cumulative Traffic Forecasts

These forecasts may be used for peak hour intersection operations or for daily roadway segments to be used as inputs into air or noise analyses. The need for cumulative forecasts and analysis is subject to the discretion of the City engineer based on the project type and location.

1. "Plus Project" Traffic Forecasts

Most "Plus Project" forecasts rely on the standard four-step method in transportation planning: trip generation, mode split, trip distribution, and traffic assignment. Each of these steps is discussed below:

Trip Generation

Chapter 2 of these guidelines provided a detailed recommended approach for estimating a project's trip generation.

Mode Split

Mode split refers to the method of travel a resident, employee, customer, etc. selects to travel to/from a proposed project. Mode split can be estimated in a number of ways, such as the following:

- Mode of travel observed at nearby, similar facilities.
- Estimates derived from the US Census, American Community Survey, or SACOG 2018 Household Survey.
- Use of a mixed-use trip generation model that has been demonstrated to be validated to local conditions.

Note that if ITE trip generation estimates are used for the trip generation estimate, those rates already have a certain (but not discernable) amount of non-auto travel built into them. This is because those rates were collected primarily in suburban settings with limited transit options, and free parking. Despite this, some (modest) levels of external travel by walking, biking, and transit likely occurred.

Trip Distribution

Trip distribution refers to the directionality of external trips made by a resident, employee, customer, etc. to/from a proposed project. Trip distribution can be estimated in a number of ways, such as the following:

- Review of existing turning movements and complementary land uses.

- Use of "big data" (using anonymous cell phone records) to measure the trip distribution of similar land uses located near the proposed project.
- Use of a travel demand model to track trips from a specific Traffic Analysis Zone (TAZ) that represents the project site.
- Other trip distribution specific data, such as school district boundaries, ZIP codes of students/employees, general population distribution, and presence of similar/competing facilities (e.g., coffee shop) nearby.

Trip distribution patterns often differ by land use type. They may also differ by time-of-day or for the inbound/outbound travel directions. These and other factors should be considered when developing trip distribution estimates.

Trip Assignment

Trip assignment refers to the specific path of travel expected to be used by project vehicle trips. Trip assignment is typically determined in one of the following ways:

- Review of existing turning movements (e.g., relative amount of left-turn traffic from two parallel routes).
- Travel time comparisons among potential routes (collected via in-person travel time runs or based on Google Maps travel time comparisons).
- Permitted driveway and adjacent intersection turning movements. Trip assignment will likely change if left-turns into or out of the project driveway are permitted versus prohibited. The availability of u-turns at a nearby intersection may also influence trip assignment.

In some instances, a project may result in changes in background travel. This could occur if new roadways are being constructed that provide new travel options. The introduction of certain land uses to an area that currently have none may background travel patterns.⁶

Consultants should submit a Project Travel Characteristics memo to City staff for review/approval prior to initiating next steps in the analysis. Doing so avoids having to redo work should there need to be updates to parts of the trip generation and distribution elements.

2. Cumulative Traffic Forecasts

The City of Citrus Heights does not have its own travel demand model that has been calibrated to local conditions. This is a fairly common situation for cities that are nearly built out and not expecting large amounts of new development. The City's roadway network and land uses are contained in both the SACMET and SACSIM versions of the Sacramento Area Council of Governments (SACOG) travel demand models. The SACSIM model is SACOG's officially adopted model as it was utilized for its now adopted 2020 MTP/SCS. The SACMET model has been used for several project-level applications in the City over the past decade. The SACMET model is trip-

⁶ As an example, the City of Citrus Heights does not current have any hotels within its boundaries. A proposed hotel could be evaluated by using "big data" to reveal travel patterns from nearby hotels outside the City limits to understand how much and where travel from these facilities is occurring within the City.

based while the SACSIM model is activity-based. The SACSIM model is very complicated and time-consuming to use, requiring many hours to make land uses changes and to run.

Consultants working on TISs in the City should coordinate with City staff regarding the most appropriate model to use. This will depend on the type of project, location of project, and any prior studies within the study area that may have been performed.

The difference method forecasting process should be used to develop cumulative traffic forecasts. This approach generally adds the growth in traffic between base and future year models to existing counts. This approach is used because it effectively accounts for base year model errors (e.g., underprediction of traffic on a certain roadway) that could otherwise translate to the future year forecasts if not accounted for by this method.

Consultants are recommended to coordinate with City staff regarding the most appropriate means for developing Cumulative Plus Project traffic forecasts. Several options are available as described below:

- Manually add project trips on top of Cumulative No Project forecasts.
- Add project to the travel demand model and develop Cumulative Plus Project forecasts. Manually subtract project trips to obtain Cumulative No Project forecasts.
- Allow the travel demand model to develop both Cumulative No Project forecasts and Cumulative Plus Project forecasts.

As there are distinct advantages and disadvantages to each method, collaboration with City staff is required to determine the most appropriate method.

~~A fundamental requirement for establishing transportation analysis is to follow state-of-the-practice or best practice methodology. This ensures that the analysis meets environmental regulatory conditions and provides a high level of confidence in the results. For traffic volume forecasts, this means that the forecasting models being used should meet the following five criteria:~~

- ~~• **The scale of the model should match that of the project.** Most studies will cover local projects, meaning that they involve specific intersections, roadways, interchanges, or corridors. Therefore, locally valid travel demand models should be used to develop traffic volume forecasts. Using regional travel demand models without modification to address the scale of the project is not appropriate.~~
- ~~• **The model should be calibrated and validated within the study area.** The model's validation in the study area should be verified for each time period being forecast (i.e., daily, AM peak hour, PM peak hour, etc.) and for each mode being analyzed.~~
- ~~• **The model validation should include static and dynamic tests.** Static validation tests should include those specified in *California 2010 Regional Transportation Plan Guidelines* (California Transportation Commission, 2010) and *Model Validation and Reasonableness Checking Manual*, (FHWA, 1997). Dynamic tests verify that the model contains an appropriate level of sensitivity related to the types of~~

transportation network or land use changes associated with the project. Appendix E contains sample tests:

- ~~**The model forecasts should be adjusted to account for base year model error.**~~ Raw model volume forecasts need to be adjusted to account for differences between base year model volume estimates and base year traffic counts. The specific methodology should be based on *National Cooperative Highway Research Project 255, Highway Traffic Data for Urbanized Area Project Planning and Design*, Transportation Research Board, December 1982.
- ~~**The model's land use or socioeconomic forecasts should be tested for reasonableness.**~~ Models are used to forecast travel demand for a specific horizon year (i.e., 20 years). The land use and socioeconomic forecasts need to match the horizon year and be based on reasonable market conditions that reflect past and future development trends for the specific study area.

For the Citrus Heights General Plan Update, a sub-area model for the City was developed using the regional SACMET model. This sub-area model is intended for City-wide analysis and did not include the 4Ds or a complete validation according to the *California 2010 Regional Transportation Plan Guidelines*. Use of this model would likely require refinement, calibration, and validation for any local studies unless resource or schedule limitations prevent the development of a better model. In these cases, the consultant shall meet with the City to develop optional forecasting methods that are compatible with the available resources and City-approved schedule for the project analysis.

6. IMPACT ASSESSMENT

The main intent of the TIS is to determine potential transportation impacts of proposed projects. This information is essential for decision makers and the public when evaluating individual projects. This section explains what operating conditions shall be used when determining an impact. These guidelines also establish criteria for when a project impact is considered significant.

SCENARIO EVALUATION

Transportation impact determination for a proposed development project shall be based upon the comparison of the following scenarios using the significance criteria cited below:

- Baseline Conditions vs. Baseline Plus Project Conditions
- ~~Baseline Plus Approved Projects (BPAP) Conditions vs. BPAP Plus Project Conditions~~
- Cumulative No Project Conditions vs. Cumulative Plus Project Conditions

SIGNIFICANCE CRITERIA

A project impact is considered significant when it meets the criteria listed in Table 6. [Several of these criteria warrant further discussion, as provided below:](#)

- [1. Pursuant to SB 743, significance criteria are not provided for potential roadway network LOS impacts. Instead, a new VMT impact significance criterion is provided. Refer back to the SB 743 Implementation Guidelines for Citrus Heights \(2021\) for more information about the significance criteria.](#)
- [2. The third bullet under On-Site Circulation pertains to hazardous conditions including insufficient storage in turn lanes. The City has determined that on arterial roadways, a left- or right-turn lane whose 95th percentile vehicle queue exceeds the available turn lane storage as a result of a proposed project would be deemed to substantially increase hazards. This would be considered a significant impact. The above interpretation is more typically applied at intersections with collector streets or driveways, but not necessarily at the intersection of two arterials. This is because greater delays and queuing are typical at these arterial-arterial intersections and drivers expect more frequent stop-and-go traffic. The City Engineer will make the final determination with regard to the significance of turn pocket queuing exceedances.](#)
- [3. Disruptions to transit service could include measurements of project-related increases in bus running time along high-frequency bus routes, along with the reported on-time performance of that route from SacRT. Studies show that late running buses can contribute to declining ridership.](#)

1. _____

TABLE 6: SIGNIFICANCE CRITERIA

Elements	<u>The project would cause a significant impact if it would:</u> Significant Impact Determination
On-Site Circulation	<ul style="list-style-type: none"> ➤ IncludeProject designs for on-site circulation, access, and parking areas <u>that</u> fail to meet City or industry standard design guidelines. ➤ A project failsFail to provide adequate accessibility for service and delivery trucks on-site, including access to truck loading areas. ➤ <u>Substantially increase hazards due to geometric design features (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).</u>
Off-Site Traffic Operations	<p>➤ Pursuant to SB 743, off-site traffic impacts are no longer considered significant under CEQA. Therefore, no significance criteria are provided. A roadway segment or intersection operates acceptably according to Policy 29.2 under a no project scenario and the addition of project trips causes overall traffic operations on the facility to operate unacceptably.</p> <p><u>A roadway segment or intersection operates unacceptably according to Policy 29.2 under a no project scenario and the project contributes more than five percent of the total peak hour traffic volume on the roadway segment or entering the intersection.</u></p>
Bicycle Facilities	<ul style="list-style-type: none"> ➤ A projectD disrupts existing or planned bicycle facilities or conflicts with adopted City bicycle plans, guidelines, <u>adopted complete streets plans</u>, policies, or standards. ➤ The projectA adds trips to an existing <u>bicycle transportation</u> facility -which adversely affects the operation or safety of the transportation system or service (e.g., bike path) that does not meet current design standards.
Pedestrian Facilities and Americans with Disabilities Act (ADA) compliance	<ul style="list-style-type: none"> ➤ A projectF fails to provide accessible and safe pedestrian connections between buildings and to adjacent streets and transit facilities. ➤ A projectD disrupts existing or planned pedestrian facilities or conflicts with adopted City pedestrian plans, <u>adopted complete streets plans</u>, guidelines, policies, or standards. ➤ The projectA adds trips to an existing <u>transportation-pedestrian</u> facility which adversely affects the operation or safety of the transportation system or service (e.g., sidewalk) that does not meet current design standards.
Parking	<p><u>The direct effect of a given supply of parking is not considered a potential area of significance. However, the extent to which a given supply of parking results in searching for parking may be considered an indirect significant impact. A project increases off-site parking demand above that which is desired according to the City in the immediate project area.</u></p>
Trucks (or other heavy vehicles)	<ul style="list-style-type: none"> ➤ A projectF fails to provide safe accommodation of forecast truck traffic or temporary construction-related truck traffic. ➤ The project adds 200 daily passenger vehicle trips (or equivalent—see Section 2 Vehicle and Truck Trip Equivalencies) to an existing roadway that does not meet current City design standards (e.g., structural section, horizontal and vertical curves, lane and shoulder width, etc.).
Transit	<ul style="list-style-type: none"> ➤ A project creates demand for public transit services above the crush load capacity that is provided or planned. ➤ Disrupt A project disrupts existing or planned transit facilities and services or conflicts with adopted City transit plans, guidelines, policies, or standards.
Intersection Traffic Control <u>Emergency Vehicle Response</u>	<ul style="list-style-type: none"> ➤ The Result in inadequate emergency access during construction and/or operation <u>addition of project traffic causes an all-way stop-controlled or side street stop-controlled intersection to meet Caltrans signal warrant criteria.</u>

Elements	<u>The project would cause a significant impact if it would: Significant Impact Determination</u>
General Plan Consistency	➤ A project C-conflicts or create <u>ans</u> inconsistency ies with General Plan policies.
Other Subject Areas <u>Construction-Related Impacts</u>	➤ The construction of a project <u>Create</u> creates a temporary but prolonged impact due to lane closures, need for temporary signals, emergency vehicles access, traffic hazards to bikes/pedestrians, damage to roadbed, <u>or</u> truck traffic on roadways not designated as truck routes, etc.
Other Jurisdictional Requirements	➤ The project <u>E-exceeds</u> established significance criteria thresholds for locations under the jurisdiction of other agencies.
<u>VMT Impacts</u>	➤ <u>Not qualify under one of several potential exemption categories (small projects, affordable housing, transit priority, etc.), and its VMT exceeded 85 percent of the regional per capita, per employee, or per visitor average VMT. ¹</u>
¹ Refer to Senate Bill 743 Implementation Guidelines for Citrus Heights (2021) for details.	

CUMULATIVE IMPACTS

Cumulative impact analysis must comply with the California Environmental Quality Act (CEQA). Land use development and infrastructure projects that are consistent with the General Plan, are expected to rely on the General Plan cumulative traffic analysis and EIR conclusions. Specific Plans will require updated cumulative traffic analysis consistent with the following definitions:

- The cumulative scenario is required per CEQA Guidelines Section 15130.
- The general definition of cumulative as a scenario is that it represents past, present, and reasonably foreseeable actions regarding land use development and the transportation network (see CEQA Guidelines Section 15355).

The General Plan environmental impact report (EIR) was based on a full build out of the City's land use designations and will generally cover the cumulative traffic effects of consistent development projects. However, over time, it is likely that general plan amendments or regional growth will influence background traffic volumes. If this occurs, individual projects may be required to conduct a project-specific cumulative analysis based on the determination of the City Engineer.

7. MITIGATION MEASURES

All significant project impacts should be mitigated consistent with the policies of the Citrus Heights General Plan. ~~Under these circumstances, the applicant should meet with the City Engineer and Project Planner to identify mitigation measures that balance the desired vehicle LOS against other City objectives.~~ Table 78 shows the appropriate CEQA mitigation actions for each analysis scenario.

Each mitigation measure will require detailed review, ~~often including traffic operations,~~ to assess resulting ~~significance after mitigation impacts.~~ Table 7 provides a list of common mitigation measures that may be applicable to ~~the a~~ proposed project. Other mitigation strategies may also be applied if supported by substantial evidence demonstrating their effectiveness.

TABLE 7: EXAMPLE MITIGATION MEASURES

Mitigation measures may include, but are not limited to, the following examples:	
Roadway Capacity Expansion <u>Improvement</u> <u>s</u>	<p><u>The following would be considered improvements to improve upon deficient conditions, but not mitigations for significant impacts under CEQA.</u></p> <ul style="list-style-type: none"> ➤ Optimize location of access driveway(s) ➤ Provide additional through traffic lane(s), right-turn lane(s), and left-turn lane(s) if they don't adversely impact other modes or are prohibited per Policy 29.2 (see following page): ➤ Improve sight distances at intersections and driveways to acceptable standards
Traffic Control Modifications (warrants must be met)	<ul style="list-style-type: none"> ➤ Provide for yield or stop control ➤ Install roundabouts ➤ Provide coordination/synchronization <u>or modified signal timings phasings at</u> traffic signals along a corridor ➤ Provide turn-lane channelization through raised islands ➤ Restrict certain turn movements ➤ Optimize location of access driveway(s) and cross-parcel access
Transit Facilities	<ul style="list-style-type: none"> ➤ Provide bus turn-outs, bus shelters, additional bus stops, and park-and-ride lots, <u>and/or prioritized transit treatments (e.g., bus queue jump lanes)</u> ➤ Fund increases in transit level of service
Pedestrian and Bicycle Facilities	<ul style="list-style-type: none"> ➤ Provide for access to, from, and through the development for pedestrians and bicyclists ➤ Designate Class I bicycle paths, Class II bicycle lanes, Class III routes, or Class IV protected facilities and other facilities ➤
Land Use Changes	<ul style="list-style-type: none"> ➤ Alter density or diversity of uses to achieve vehicle trip reductions
<u>Project Access and On-Site Circulation</u>	<ul style="list-style-type: none"> ➤ <u>Relocate/design driveways and/or restrict certain turn movements</u> ➤ <u>Lengthen turn pockets</u> ➤ <u>Construct right-turn deceleration lanes at driveways</u> ➤ <u>Modify site to provide adequate driveway throat depths</u> ➤ <u>Reconfigure on-site parking, drive-through lanes, or other on-site amenities</u>

Mitigation measures may include, but are not limited to, the following examples:

Travel Transportation Demand Management (TDM) for VMT Impacts	Refer to SB 743 Implementation Guidelines for Citrus Heights (February 2021 for discussion of potential TDM strategies. Institute flexible employee working hours where enforceable Institute preferential parking for carpools Encourage employees to use carpools and public transportation Institute Transportation Management Association (TMA)
Construction-Related	➤ Implement Construction Traffic Management Plans

In accordance with Policy 29.2, no roadway widening to provide additional vehicle capacity to the streets listed below will be permitted:

- Sunrise Boulevard – south City limits to north City limits
- Greenback Lane – west City limits to east City limits.
- Old Auburn Road – Sylvan Road to Fair Oaks Boulevard
- Antelope Road – I-80 to Auburn Boulevard
- Auburn Boulevard – Old Auburn Road to northern City limits

Development projects that impact these locations according to these transportation impact study guidelines would require mitigation, including, but not limited to, the following options:

- Actions that reduce vehicle trips or provide non-auto improvements to the transportation network or services
- Signal timing [and/or phasing](#) modifications
- Lengthening of turn pockets

For non-auto mitigation improvements, the mitigation measures should consider the planned bikeway, pedestrian, and transit needs as identified in the Citrus Heights General Plan, [the City of Citrus Heights Bikeway Master Plan \(2015\)](#), and [the City of Citrus Heights Pedestrian Master Plan \(2016\)](#).

~~(Maps 8, 9, and 10—See Appendix G). Similarly, signal timing improvements should consider the planned mobility improvements as identified in the Citrus Heights General Plan (Map 7—see Appendix G).~~

~~For mitigation measures that rely on vehicle trip reduction strategies, the study should quantify the reduction associated with the proposed action based on previous studies backed by substantial evidence, such as the California Air Pollution Control Officers Association (CAPCOA) [Quantifying Greenhouse Gas Mitigation Measures](#). See Appendix H for a sample list of vehicle trip reduction measures as identified in CAPCOA's [Quantifying Greenhouse Gas Mitigation Measures](#).~~

In addition, Policy 29.2 allows the City Council to provide additional exceptions to the LOS E policy where physical mitigation is infeasible or would conflict with other community values, such as:

- Impacts on general safety, particularly pedestrian, bicycle, and transit safety.
- The right-of-way needs and the physical impacts on surrounding private or public properties.
- The visual aesthetics of the required improvement and its impact on community identity and character.
- Environmental impacts including air quality and noise impacts.
- Impacts on quality of life as perceived by residents.

Based on Policy 29.2 and these guidelines, identifying ~~mitigation measures~~improvements for locations where roadway widening is prohibited includes the following key analysis steps:

- Identify locations where the project will ~~cause an impact to traffic operations~~not comply with the General Plan LOS policy~~per Table 7 on page 32.~~
- Consult with City staff to determine which mitigation option(s) should be applied.
- If actions to reduce project vehicle trips is one of the desired mitigation options, determine the number of project vehicle trips that are feasible to remove from the impacted location to reduce or mitigate the project impact. This step should be conducted in consultation with City staff.
- Identify vehicle trip reducing measures and quantify the trip reduction by referring to a study or source that confirms the vehicle trip reduction by citing or relying on substantial evidence.~~To understand how these guidelines may apply to a project that impacts locations where roadway widening is prohibited, an example is provided to the right.~~

8. RECOMMENDED PROCESS AND DOCUMENTATION

~~A professional traffic engineer shall conduct the~~The transportation impact analysis shall be performed under the supervision of either a professional traffic or civil engineer, or a certified professional planner specializing in transportation. It is recommended that the ~~engineer conduct the work in the follow~~occur in a ~~ing~~-phased manner and seek City acceptance before initiating the next task. ~~In some cases, review by other affected jurisdictions will be required.~~

- **Transportation Study Scope of Work** detailing project description, site location, analysis method, area-wide assumptions, study intersections and/or roadways, peak hours for analysis, and traffic data collection.
- **Project Trip Generation and Trip Distribution** documenting all key technical assumptions, data sources, and references.
- **Administrative Draft Transportation Study Report** prepared according to the Scope of Work, Project Trip Generation, and Trip Distribution approved by the City.
 - As discussed in Section 1 of these guidelines, the type of project and environmental review will dictate the format of this report. Refer to Section 1 for details.~~The format of this report may need to be discussed with the EIR preparer to determine if an independent transportation study report is required or if the consultant should prepare a transportation and circulation section for incorporation into the EIR or Mitigated Negative Declaration.~~
- **Draft Transportation Study Report** addressing the City's comments on the Administrative Draft Report (see Appendix C for recommended outline).
- **Final Transportation Study Report / Response to Public Comments** addressing comments from the City, Caltrans, neighboring jurisdictions, etc.

For small projects that require a limited scope of study, it may be suitable to prepare a Technical Memorandum in lieu of a full report. ~~The format of this report may need to be discussed with the EIR consultant. It may be a final report incorporating the comments or written responses to public comment.~~

~~Appendix CB contains a recommended outline for the TIS document.~~

Appendix A: Sample Trip Generation Calculations

The following table exemplifies how trip generation information and assumptions should be prepared and documented for submittal to the City of Citrus Heights.

TABLE A-1: SAMPLE ESTIMATED PROJECT TRIP GENERATION

Land Use	Size	Unit	Daily		Trip Rates						Trips					
			Rate	Trips	AM Peak Hour			PM Peak Hour			AM Peak Hour			PM Peak Hour		
					In	Out	Total	In	Out	Total	In	Out	Total	In	Out	Total
Single-Family ¹	200	du	9.484	1,967.888	0.189	0.567	0.746	0.62	0.37	0.99	378	1114	14852	1254	73	1987
Apartments ²	100	du	7.30	730	0.11	0.42	0.53	0.47	0.26	0.73	11	42	53	47	26	73
Commercial ³	100	ksf	60.12794	6,012.794	1.25094	2.0215	2.0215	2.6034	2.8332	5.4363	12594	7760	202154	260342	283324	543636

Notes: du = dwelling units; ksf = 1,000 square-feet

1. Trip generation based on *Trip Generation, 10th Edition* (Institute of Transportation Engineers ITE, 2017~~08~~) ~~weighted average trip rates-fitted curve equations~~ for Single-Family Detached Housing (Land Use Code 210) ~~Daily: $\ln(T) = 0.92 \ln(X) + 2.71$ (50% Inbound, 50% Outbound)~~
~~AM Peak Hour: $T = 0.70(X) + 9.74$ (25% Inbound, 75% Outbound)~~
~~PM Peak Hour: $\ln(T) = 0.90 \ln(X) + 0.51$ (63% Inbound, 37% Outbound)~~
~~Where: T = trips generated, X = dwelling units, Ln = natural log~~
2. ~~Trip generation based on *Trip Generation, 8th Edition* (Institute of Transportation Engineers ITE, 2008) fitted curve equations for Apartment (Land Use Code 220):~~
~~Daily: $T = 6.06(X) + 123.56$ (50% Inbound, 50% Outbound)~~
~~AM Peak Hour: $T = 0.49(X) + 3.73$ (20% Inbound, 80% Outbound)~~
~~PM Peak Hour: $T = 0.55(X) + 17.65$ (65% Inbound, 35% Outbound)~~
~~Where: T = trips generated, X = dwelling units~~
3. ~~2.~~ Trip generation based on *Trip Generation, 10th Edition* (Institute of Transportation Engineers ITE, 2017~~08~~) fitted curve equations for Shopping Center (Land Use Code 820):
Daily: $\ln(T) = 0.685 \ln(X) + 5.5783$ (50% Inbound, 50% Outbound)
AM Peak Hour: $\ln(T) = 0.59 \ln(X) + 151.78232$ (64.62% Inbound, 39.38% Outbound)
PM Peak Hour: $\ln(T) = 0.674 \ln(X) + 2.89337$ (48.9% Inbound, 52.4% Outbound)
Where: T = trips generated, X = 1,000 square-feet, Ln = natural log

~~Source: *Trip Generation, 8th Edition* (Institute of Transportation Engineers, 2008)~~

Additional Notes:

- Survey data or the most recent version of ITE should be used to calculate trip generation.
- Pass-by reductions should also be considered for commercial uses where applicable.
- For mixed use developments, an internalization estimate should be included based on proven methods or models such as the US EPA MXD model, MXD+ model developed by Fehr & Peers, or other tools, that are supported by substantial evidence.

Appendix B: Sample Site Plan Review

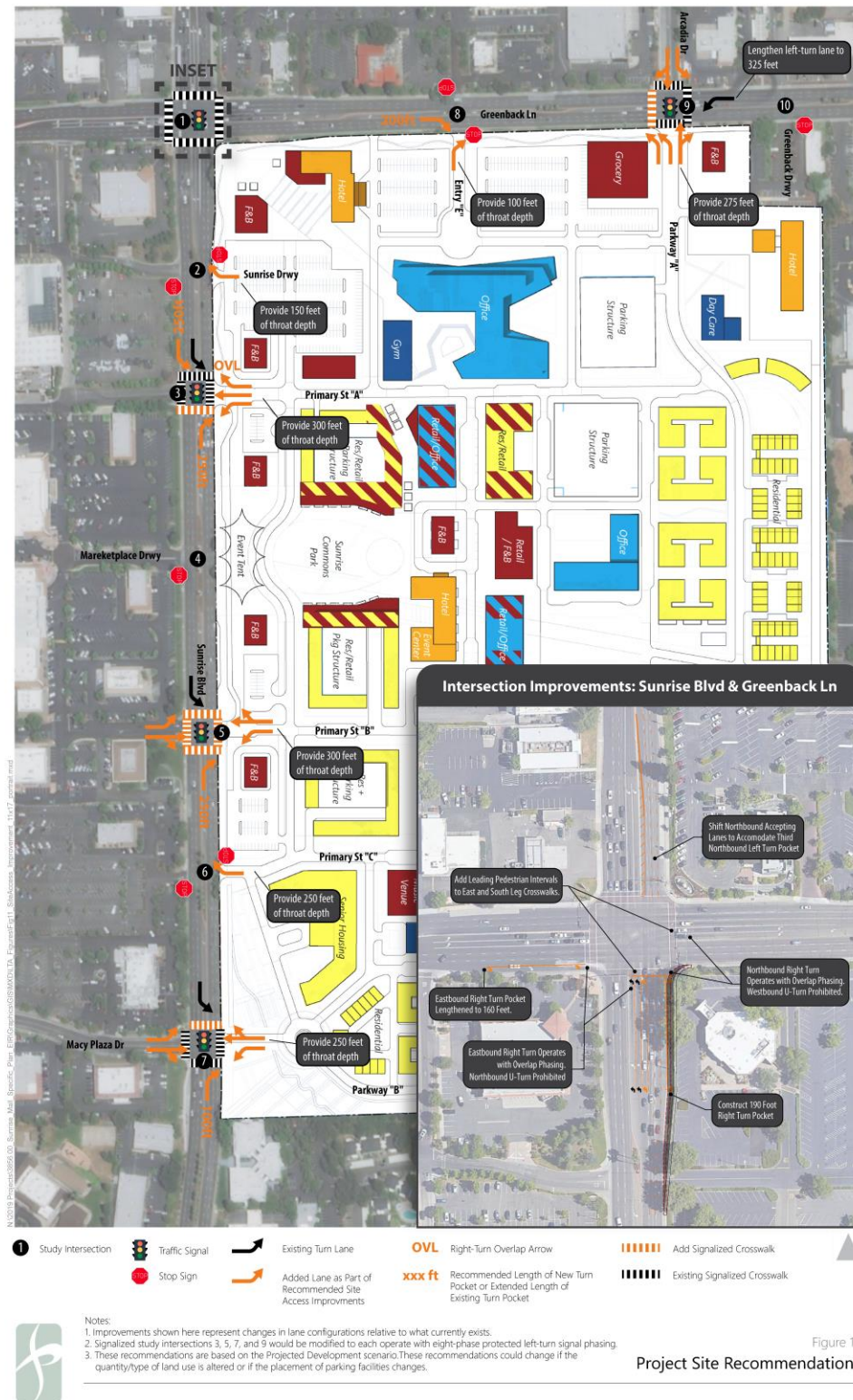
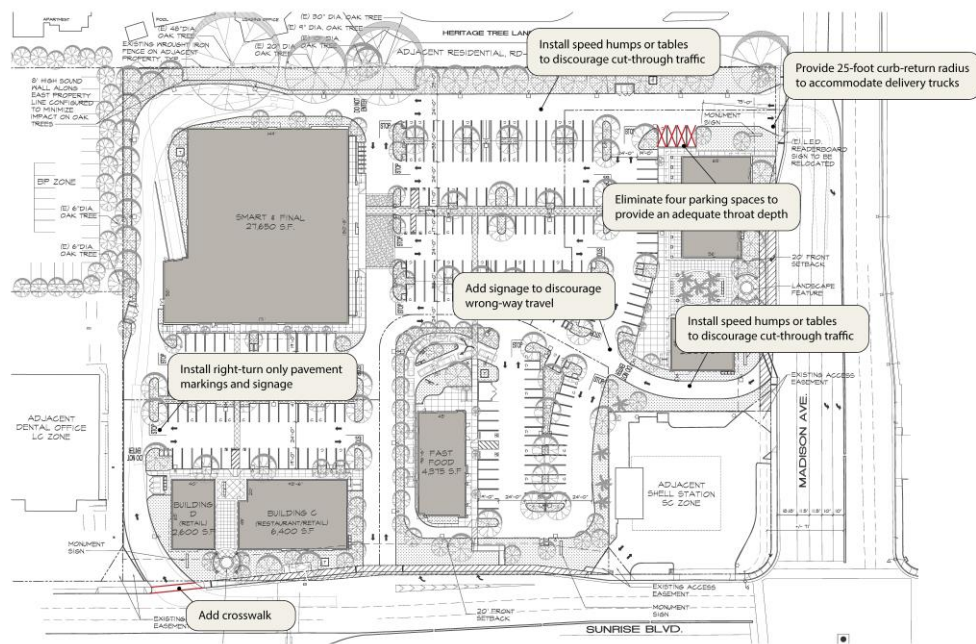


Figure 11

Project Site Recommendations

Sunrise Tomorrow Specific Plan



Retail Center in Northeast Quadrant of Sunrise/Madison Intersection

Appendix C: TIS Report Format Outline

1. Introductory Items

- Front Cover/Title Page ~~—signed and sealed by a registered California Civil or Traffic Engineer~~
- Table of Contents, List of Figures, and List of Tables
- Executive Summary

2. Introduction/Background

- Project description
- ~~Project sponsor/contact info~~
- Type and size of development
- Site plan (include proposed driveways, roadways, traffic control, parking facilities, emergency vehicle access, and internal circulation for vehicles, bicyclists, and pedestrians)
- Location map (include major streets, study intersections, and neighboring zoning and land uses)

3. Baseline Conditions

- Roadway system within project site and surrounding area
- Location and routes of nearest public transit system serving the project
- Location and routes of nearest pedestrian and bicycle facilities serving the project
- Figure of study intersections with peak hour turning movement counts, lane geometries, and traffic control
- Map of study area showing ADT of study roadways
- Table of baseline peak hour average vehicle delay and LOS

4. Baseline Plus Project Conditions

- Table of trip generation for project
- Figure/map of trip distribution (in percent)
- Maps of study area with applicable peak hour turning movements (Project Only and Baseline Plus Project)
- Table of Baseline and Baseline Plus Project intersection peak hour average vehicle delay and LOS
- Traffic signal and other warrants
- Findings of project impacts
- Mitigation measures for project impacts (include a map showing physical mitigation)
- Scheduling and implementation responsibility of mitigation measures
- Impacts of mitigation measures

~~5. Baseline Plus Approved Projects (BPAP) Conditions~~

- ~~• Table of trip generation for approved project(s)~~
- ~~• Figure and/or table of approved projects trip distribution (in percent)~~
- ~~• Map of study area with applicable peak hour turning movements (Approved Projects Only and Baseline Plus Approved)~~
- ~~• Table of intersection peak hour average vehicle delay and LOS~~
- ~~• Traffic signal and other warrants~~

~~6. BPAP Plus Project Conditions~~

- ~~• Similar content to Baseline Plus Project Conditions~~

~~7.5. Cumulative and Cumulative Plus Project Conditions~~

- Map of study area with Cumulative No Project peak hour turning movements
- Map of study area with Cumulative Plus Project peak hour turning movements
- Table of Cumulative and Cumulative Plus Project intersection peak hour average vehicle delay and LOS
- Traffic signal and other warrants
- Findings of project impacts
- Mitigation measures for project impacts (include a map showing physical mitigation)
- Scheduling and implementation responsibility of mitigation measures
- Impacts of mitigation measures

~~8.6. Construction Impacts~~

~~9.7. Phasing Impacts (for large projects only)~~

~~10.8. Appendices~~

- List of references
- ~~• List of authors~~
- Traffic counts
- Technical calculations for all analyses ~~—signed and sealed by a registered California Civil or Traffic Engineer~~

Appendix C: LOS Policies of Adjacent Jurisdictions

~~The policies below reflect the adopted policies of adjacent jurisdictions as of February 2013. These policies should be verified as part of the traffic impact study preparation.~~

~~County of Sacramento General Plan (Adopted November 2011)~~

~~Plan and design the roadway system in a manner that meets Level of Service (LOS) D on rural roadways and LOS E on urban roadways, unless it is infeasible to implement project alternatives or mitigation measures that would achieve LOS D on rural roadways or LOS E on urban roadways. The urban areas are those areas within the Urban Service Boundary as shown in the Land Use Element of the Sacramento County General Plan. The areas outside the Urban Service Boundary are considered rural.~~

~~County of Placer General Plan (Adopted August 1994)~~

~~The County shall develop and manage its roadway system to maintain the following minimum levels of service (LOS):~~

~~LOS "C" on rural roadways, except within one-half mile of state highways where the standard shall be LOS "D".~~

~~LOS "C" on urban/suburban roadways except within one-half mile of state highways where the standard shall be LOS "D".~~

~~The County may allow exceptions to these level of service standards where it finds that the improvements or other measures required to achieve the LOS standards are unacceptable based on established criteria. In allowing any exception to the standards, the County shall consider the following factors:~~

~~The number of hours per day that the intersection or roadway segment would operate at conditions worse than the standard.~~

~~The ability of the required improvement to significantly reduce peak hour delay and improve traffic operations.~~

~~The right-of-way needs and the physical impacts on surrounding properties.~~

~~The visual aesthetics of the required improvement and its impact on community identity and character.~~

~~Environmental impacts including air quality and noise impacts.~~

~~Construction and right-of-way acquisition costs.~~

~~The impacts on general safety.~~

~~The impacts of the required construction phasing and traffic maintenance.~~

~~The impacts on quality of life as perceived by residents.~~

~~Consideration of other environmental, social, or economic factors on which the County may base findings to allow an exceedance of the standards.~~

~~City of Roseville General Plan (Adopted May 2010)~~

~~Maintain a level of service (LOS) "C" standard at a minimum of 70 percent of all signalized intersections and roadway segments in the City during the p.m. peak hours. Exceptions to the LOS "C" standard may be considered for intersections where the City finds that the required improvements are unacceptable based on established criteria identified in the implementation measures. In addition, Pedestrian Districts may be exempted from the LOS standard.~~

~~Caltrans~~

~~All projects that may affect a state facility should consult with Caltrans Department of Intergovernmental Review (IGR) to determine the appropriate LOS standard. Caltrans has a few documents that identify LOS standards for Interstate 80 in Citrus Heights and the vicinity, including: the *Interstate 80 Transportation Corridor Concept Report*, *Interstate 80 and Capital City Freeway Corridor System Management Plan*, and the *Caltrans' Guide for the Preparation of Traffic Impact Studies*.~~

~~The Interstate 80 Transportation Corridor Concept Report (TCCR) identifies a concept LOS F for Interstate 80 in and around Citrus Heights (Caltrans, 2010). A concept LOS, or "target" LOS, is typically used to identify a deficiency or need for improvement when the actual LOS falls below the concept LOS.~~

~~Similarly, the Interstate 80 and Capital City Freeway Corridor System Management Plan (CSMP) identifies I-80 operating at LOS F in and around Citrus Heights.~~

~~Caltrans' *Guide for the Preparation of Traffic Impact Studies* (Caltrans, 2002) provides general guidance regarding the preparation of traffic impacts studies for projects that may have an impact on the State Highway System. The *Guide for the Preparation of Traffic Impact Studies* states that "Caltrans endeavors to maintain a target LOS at the transition between LOS "C" and LOS "D" on state highway facilities; however, Caltrans acknowledges that this may not always be feasible and recommends that the lead agency consult with Caltrans to determine the appropriate target LOS." When the LOS is worse than C, Caltrans will typically request that the current LOS be maintained.~~

~~Appendix H:~~
~~Sample Trip Reduction Measures from~~
~~California Air Pollution Control Officers~~
~~Association (CAPCOA) Quantifying~~
~~Greenhouse Gas Mitigation Measures~~



Walking and bicycling are expected to grow in importance as efforts are made in the City to create a development form that will support multiple travel choices. The City has already completed a bikeway master plan (BMP) and the policies below identify the need to develop a pedestrian master plan, an Americans with Disability Act (ADA) Transition Plan, and a Safe Route to School Plan. *Map 8: Planned Bikeway System* shows the location of existing and proposed bicycle facilities while *Map 9: Potential Pedestrian System Needs* identifies gaps in the existing sidewalk network.

Public transit is also a vital part of the circulation system to serve people unable or desiring not to use automobiles. Public transit can also serve as a means of reducing congestion if the service is well utilized. *Map 10: Transit Emphasis Areas* shows current (2010) bus routes in the City and the areas within ¼ miles of these routes that will be targeted for enhancements to improve transit access, transit facilities, and transit service.

The policies in this section seek to provide a complete transportation system to meet the needs of all residents and to serve businesses within the City.

Goal 29: Plan, design, construct, and manage a Complete Streets transportation network that accommodates the needs of all mobility types, users and ability levels

Policy 29.1: When constructing or modifying transportation facilities, strive to provide for the movement of vehicles, commercial trucks, alternative and low energy vehicles, transit, bicyclists and pedestrians appropriate for the road classification and adjacent land use.

- Actions**
- A. *Update the Capital Improvement Program annually to incorporate necessary circulation system improvements.*
 - B. *Evaluate projects to ensure that the safety, comfort, and convenience of pedestrians and bicyclists are given equal level of consideration to drivers.*
 - C. *Consider ways to increase and improve travel choices when reviewing development or transportation infrastructure projects.*
 - D. *Require sidewalks on all arterial and collector streets. Where feasible, separate sidewalks from streets on arterials and collectors with landscaping including a tree canopy to create shade.*
 - E. *Improve the existing street network to minimize travel times and improve mobility for transit, bicycle, and walking trips between new projects and surrounding land uses to reduce vehicle trips.*



LEGEND

Existing ITS Elements

- Traffic Signal
- Traffic Signal (Maintained by Sacramento County)
- Traffic Signal (Mid-Block 2 Automobile Movements)
- Fire Station Signal
- CCTV
- Changeable Message Sign
- Traffic Control Center
- Flasher
- Pedestrian Signal

Copper Cable Interconnect

Fiber Optics Interconnect

Fiber Optics (for Cameras Only)

City of Citrus Heights

County Boundary

Potential Mobility Improvements

Extend Fiber Optics Interconnect

Convert Copper SIC to Fiber Optics Interconnect

Connect Signals Via Radio Link, and Link to City Hall

Add Left-Turn Lanes

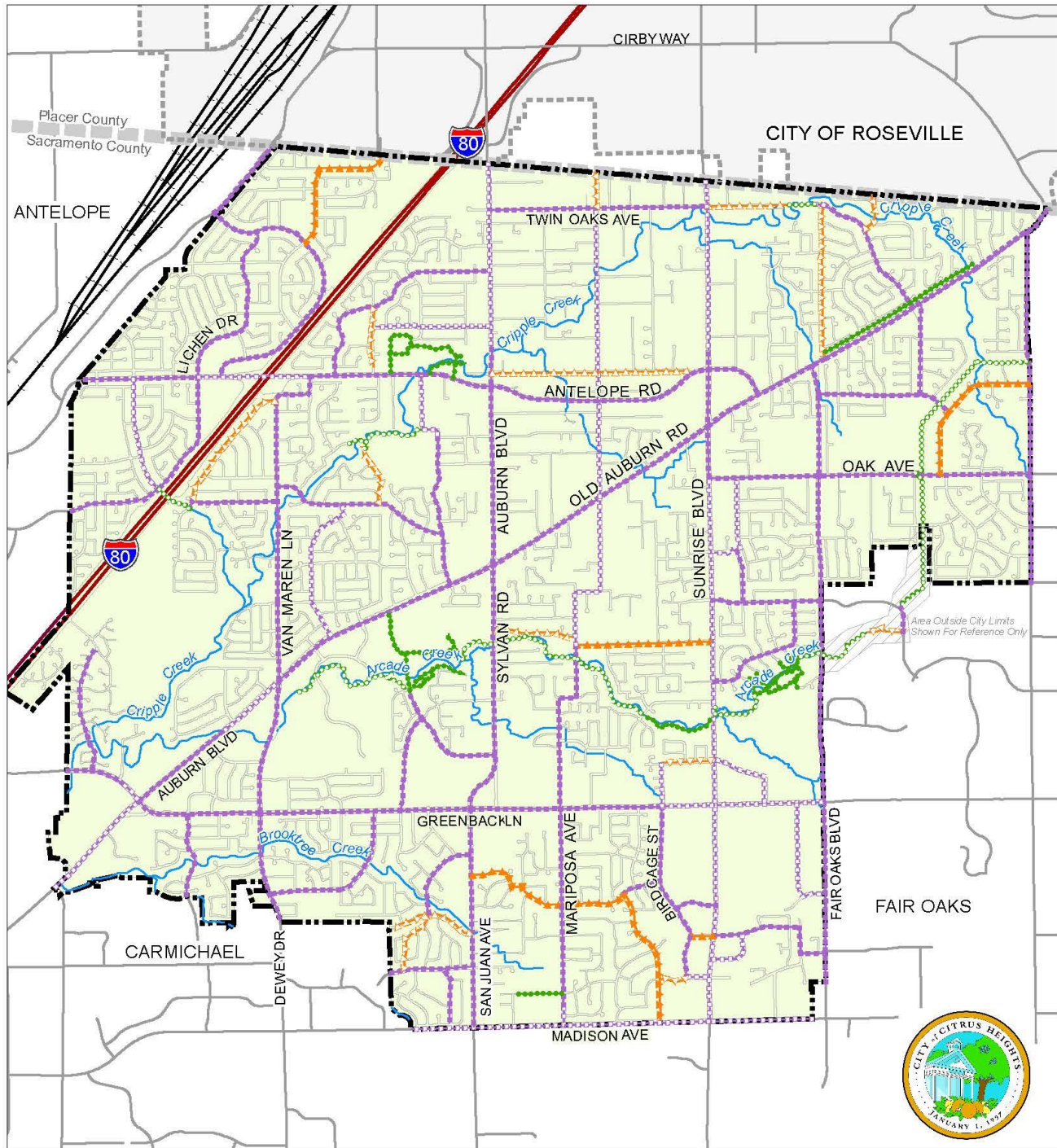


0 0.25 0.5 1
MILES

Map 6
Potential Mobility Improvements



Map 7
2035 Daily Roadway Segment LOS



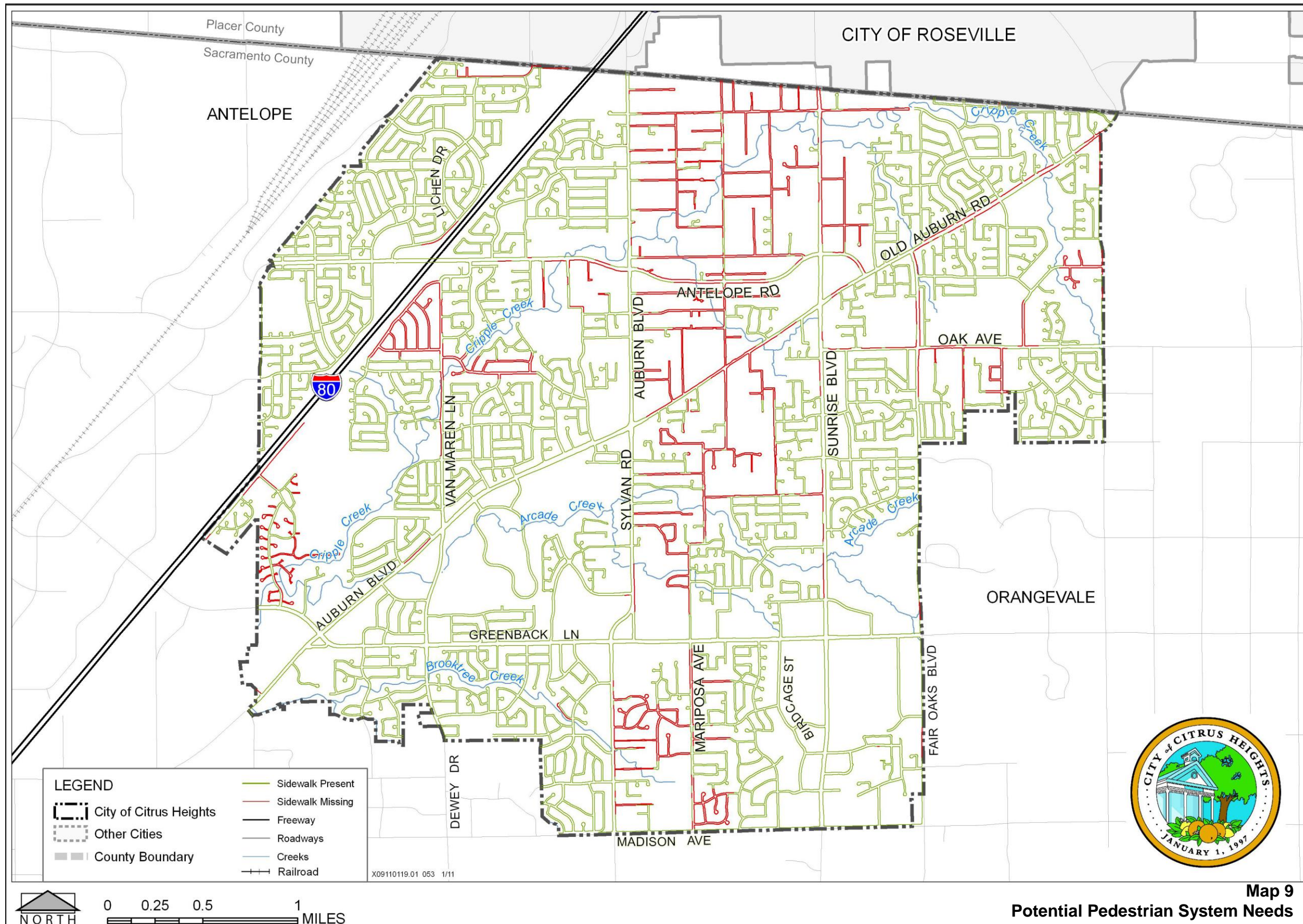
LEGEND

- City of Citrus Heights
- Other Cities
- County Boundary
- Railroad
- Creeks
- Class I Bike Lane (Existing)
- - - Class I Bike Lane (Proposed)
- Class II Bike Lane (Existing)
- - - Class II Bike Lane (Proposed)
- Class III Bike Route (Existing)
- - - Class III Bike Route (Proposed)

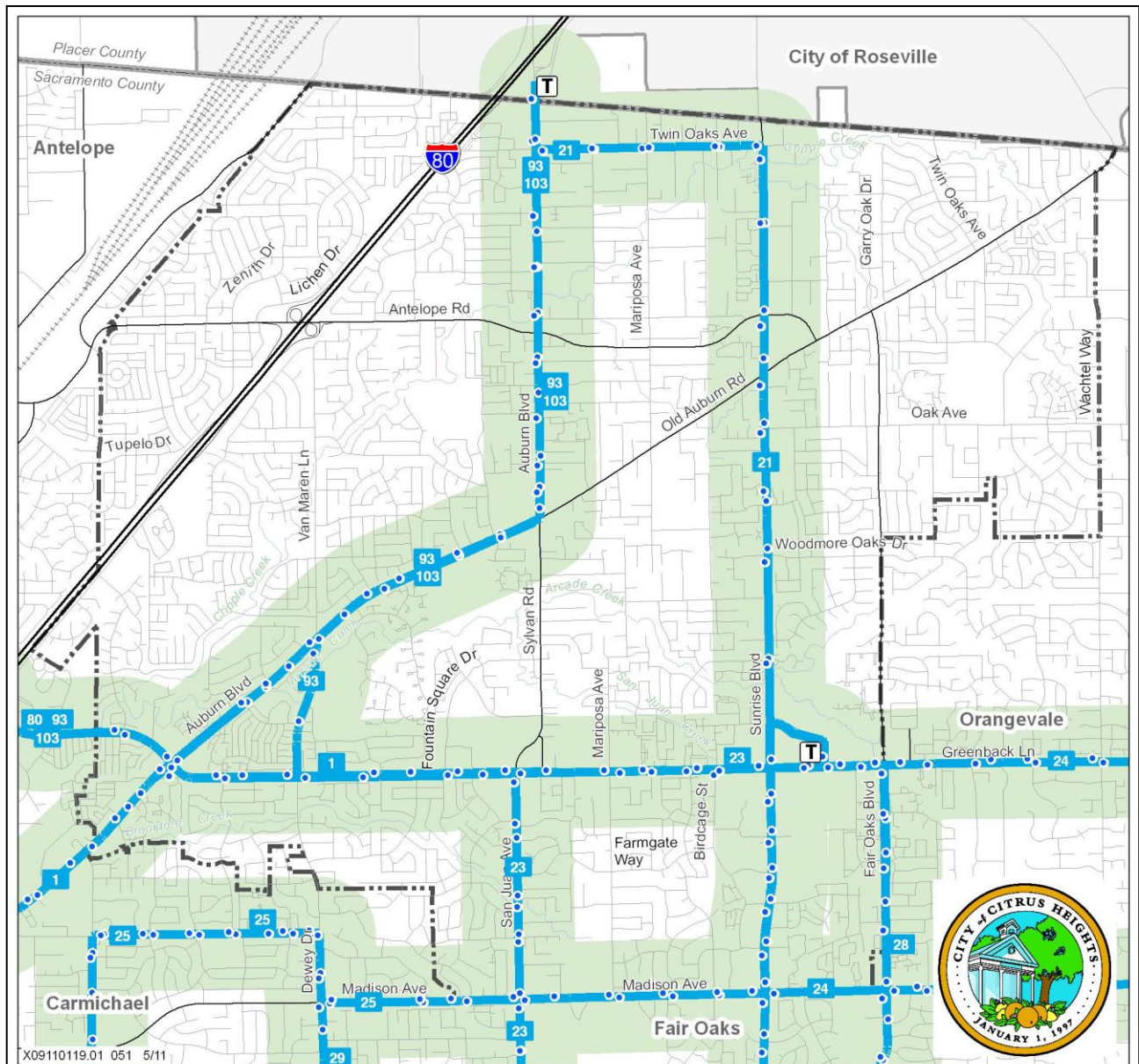
Existing and Proposed Bikeway Classification (Miles)			
Bikeway Classification	Existing	Proposed	Total
Class I	4.5	4.9	9.4
Class II	40.9	14.5	55.4
Class III	3.5	4.4	7.9
Total	48.9	23.8	72.7



Map 8
Planned Bikeway System



This page intentionally left blank.



LEGEND

- Transit Emphasis Area (1/4 Mile from Transit Lines)
will be targeted for enhancements to improve transit access, transit facilities (e.g. shelters) and transit services
- Sacramento Regional Transit Bus Stops
- T Transit Center
- Sacramento Regional Transit Bus Routes (May 2011)
- City of Citrus Heights
- County Boundary
- Creeks
- Railroad
- Other Cities



0 0.25 0.5 1
MILES

Map 10
Transit Emphasis Areas

Policy 29.2: Measure customer satisfaction related to vehicle travel using level of service (LOS) according to procedures in the latest version of the Highway Capacity Manual published by the Transportation Research Board. The City will strive to achieve LOS E or better conditions for City roadways and intersections during peak hours (these may include weekday AM, Mid-Day, and PM hours as well as Saturday Mid-Day or PM peak hours). The intent of the policy is to effectively utilize the roadway network capacity while balancing the desire to minimize potential adverse effects of vehicle travel on the environment and other modes.

Exceptions to LOS E are allowed for both roadway segments and intersections along the following streets:

- Sunrise Boulevard – south City limits to north City limits
- Greenback Lane – west City limits to east City limits
- Old Auburn Road – Sylvan Road to Fair Oaks Boulevard
- Antelope Road – I-80 to Auburn Boulevard
- Auburn Boulevard – Old Auburn Road to northern City limits

No road widening to provide additional vehicle capacity of the above listed streets will be permitted. Development projects that impact these locations according to the City's transportation impact study guidelines would require mitigation, including, but not limited to, the following items:

- actions that reduce vehicle trips or provide non-auto improvements to the transportation network or services
- lengthening of turn pockets
- signal timing modifications

Additional exceptions may be allowed by the City Council at both exempt and non-exempt locations where mitigation is infeasible or would conflict with other community values such as those listed below:

- Impacts on general safety, particularly pedestrian, bicycle, and transit safety
- The right-of-way needs and the physical impacts on surrounding private or public properties
- The visual aesthetics of the required improvement and its impact on community identity and character
- Environmental impacts including air quality and noise impacts
- Impacts on quality of life as perceived by residents

Actions A. Modify the existing traffic impact fee program to include a mitigation fee designed to reduce vehicle trips and vehicle miles of travel per capita within the City to avoid

or minimize the need to expand existing roadway capacity. This program should include a multi-modal (Complete Streets) capital improvement program (CIP) and, in conjunction with public funding, provide full funding for the City's circulation element improvements.

- B. The City shall develop and adopt transportation impact study guidelines within one year of General Plan adoption.*

Policy 29.3: Require development proposals to analyze future transportation impacts and mitigate significant impacts consistent with Policies 29.1 and 29.2.

Policy 29.4: Support safe, complete and well-connected neighborhood street, bicycle, and pedestrian access and connections that balance circulation needs with the neighborhood context.

- Actions
- A. Modify the existing street network to enable direct physical connections within neighborhoods and between neighborhoods, neighborhood-commercial areas, and commercial-commercial areas, including connections accessible only by pedestrians and bicycles on existing cul-de-sac streets.*
 - B. Provide direct connection from residential areas to neighborhood parks and open space.*
 - C. Where feasible, provide pedestrian crosswalks on all intersection approaches.*
 - D. Develop and implement an ADA Transition Plan that focuses on compliant sidewalk improvements that provide continuous pedestrian access where compatible with the surrounding area.*
 - E. Develop and implement a Pedestrian Master Plan (PMP) that indicates which streets in addition to arterials and collectors will install sidewalks and what other pedestrian facilities and amenities (such as 'resting spots') are needed to complete the pedestrian network shown in Map 9. Sidewalk widths and shade coverage should also be addressed in the context of the adjacent land use, vehicle volumes, and vehicle speeds.*
 - F. Update the Bikeway Master Plan (BMP) and complete the proposed bikeway network shown in Map 8 within 10 years and prioritize projects that close existing gaps in the network.*
 - G. Develop and implement a Safe Routes to School Plan. This effort should complement the ADA Transition Plan, the PMP, and the BMP.*

Policy 29.5: Encourage movement of goods by truck on freeways and other appropriate designated routes.

- Actions
- A. Designate official truck routes to minimize the impacts of truck traffic on residential neighborhoods and other sensitive land uses.*
 - B. Establish a protocol to minimize noise and other impacts of truck traffic, deliveries and staging in residential areas.*

- C. *Consider restricting truck traffic on City streets during peak periods in collaboration with Caltrans, California Highway Patrol, and the California Trucking Association. Consider other methods to reduce regional through trucks on City streets to the extent feasible.*
- D. *Actively enforce compliance with truck route designations and any time period restrictions.*

Policy 29.6: Collaborate with neighboring jurisdictions when updating the General Plan and preparing the Capital Improvement Program to work toward providing a regional Complete Streets transportation network for all modes.

Policy 29.7: Develop a transportation financing program that will fully fund the planned expansion of the existing transportation network and comply with Policies 29.1 and 29.2. This program will combine federal and state transportation funds with local funding sources that provide the means by which new development consistent with the general plan will fully mitigate its cumulative transportation impacts. This approach to transportation finance is intended to streamline development review for those projects consistent with the general plan.

Policy 29.8: Minimize the number of access points along arterial roadways, including by consolidating or relocating driveways to provide for more efficient traffic movement.

Goal 30: Promote attractive and well-maintained roadways and sidewalks

Policy 30.1: Improve aesthetic features along the City's roadways and maintain landscaping in an efficient and timely manner especially when it enhances the walking and biking environment.

- Actions*
- A. *Install improvements along roadway segments as identified on Map 4.*
 - B. *Update the Capital Improvement Program annually to incorporate aesthetic improvements on roadways. Seek additional funding for roadway maintenance to provide safe, functional and attractive streets and roads.*

Policy 30.2: Require public street right-of-way dedications and improvements as development occurs. Ultimate right-of-way and improvements should be installed at the time of development, except when a lesser right-of-way will avoid significant social, neighborhood or environmental impacts and perform the same traffic movement function.

- Action*
- A. *The City shall develop and adopt ultimate right-of-way dimensions for public roadways through a plan line study or equivalent. This study/plan should also address key complete street issues such as intersection and access spacing, lane widths, land use context, landscaping, transit, and non-motorized vehicles.*





CITY OF CITRUS HEIGHTS

CITY COUNCIL STAFF REPORT MEMORANDUM

DATE: April 22, 2021

TO: Mayor and City Council Members
Christopher W. Boyd, City Manager

FROM: Colleen McDuffee, Community Development Director
Stephanie Cotter, Housing and Human Services Program Coordinator

SUBJECT: CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan
and 2020 Action Plan for Coronavirus (CDBG-CV 1 & 3) Funding

Summary and Recommendation

The City of Citrus Heights has received a total of \$920,226 in Community Development Block Grant Coronavirus (CDBG-CV) funds from the Department of Housing and Urban Development (HUD) to respond to the impacts of the Novel Coronavirus (COVID-19).

On March 27, 2020, the city received a Round 1 CDBG-CV allocation of \$376,028 based on the 2020 CDBG entitlement formula. The City Council approved the CDBG-CV Round 1 allocations on May 28, 2020 (Amendment No. 1), and August 13, 2020 (Amendment No. 2). As part of these allocations, the City Council reserved \$91,672.40 in CDBG-CV Round 1 funding to cover the city's required local match for the Great Plates Delivered Program. Following this action, it was determined the Federal Emergency Management Agency (FEMA) would reimburse the city's full cost to participate in this program. Therefore, the city has \$91,672.40 in CDBG-CV Round 1 funds remaining to re-allocate to another CDBG-CV-eligible activity.

The city did not receive a Round 2 CDBG-CV allocation as it was reserved for states and insular areas. However, the city did receive a Round 3 CDBG-CV allocation in the amount of \$544,198, for a total CDBG-CV award of \$920,226. The city has not yet allocated its CDBG-CV Round 3 grant funding. The following action would reallocate the \$91,672.40 in CDBG-CV Round 1 funding from the Great Plates Delivered Program to the Supplemental Navigator Fund.

In addition, this action would allocate \$135,384 in CDBG-CV Round 3 funding to the Fiscal Year 2021-22 Community Support Applicants with CDBG-CV-eligible activities as well as associated planning and administration costs. In general, Maintenance of Effort requirements prohibit the city from increasing the CDBG allocation to an activity when the General Fund allocation has been decreased in the previous fiscal year. However, the Department of Housing and Urban Development (HUD) determined that if Citrus Heights can document the activity

meets the purpose of preparing, preventing, or responding to Coronavirus and demonstrates the lack of adequate local funds for the project, it may utilize CDBG-CV funds for eligible activities, such as the proposed Community Support projects.

The proposed CDBG-CV Round 3 allocations includes \$15,000 to Campus Life to support the afterschool program at the Sayonara Center; \$82,000 to Meals on Wheels for senior meals; \$9,100 to Single Mom Strong for distance learning support; \$6,720 to Sunrise Christian Food Ministry for the emergency food closet at Advent Lutheran Church; and \$22,564 for associated planning and administration expenses.

Staff recommends the City Council:

1. Hold a public hearing on the proposed CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and 2020 Action Plan for Coronavirus (CDBG-CV 1 & 3) Funding
2. Approve Resolution No. 2021-____ Adopting the CARES Act Amendment No 3 to the 2020-2024 CDBG Consolidated Plan and 2020 Action Plan (Exhibit A) and Authorizing the City Manager to Execute All Contracts Necessary to Carry Out the CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and 2020 Action Plan for Coronavirus (CDBG-CV 1 & 3) Funding

Fiscal Impact

The city receives federal CDBG grant funding from HUD. The CDBG program allows for the city to be reimbursed for eligible costs, including those associated with administering the grant. CDBG funding through either the special COVID-19 allocation or the annual CDBG entitlement award is sufficient to cover administrative costs associated with funding oversight and program management. If approved, this action would reduce the impact on the FY 2021-22 General Fund budget by \$112,820 by moving eligible Community Support applicants to CDBG-CV Round 3 funding.

Background and Analysis

On March 19, in response to the spread of COVID-19, the city declared a local state of emergency. Additionally, Sacramento County issued a “stay at home” public health order that, among other things, required the closure of non-essential businesses. Many workers in these businesses were furloughed or laid off, and stores experienced unexpected demand for food and household items. Ultimately, concerns about COVID-19 and the policies put in place to slow its spread, led to a loss of economic, housing, and food security for Citrus Heights residents.

State and federal policies have been implemented to address the severe economic impacts of COVID-19. California Governor Gavin Newsom issued an executive order offering tenant protections during the crisis. At the federal level, the Coronavirus Aid, Relief, and Economic Security (CARES) Act was signed into law on March 27, 2020. This \$2 trillion stimulus plan provides direct financial assistance to households, extended unemployment insurance, healthcare, and small business support, and support for state and local governments. Of the \$150

billion allocated to state and local governments, Citrus Heights received a Round 1 allocation of about \$376,028 and a Round 3 allocation of \$544,198 for a total amount of \$920,226 in special Community Development Block Grant Coronavirus (CDBG-CV) funding.

Approved Amendment No. 1 (CDBG-CV Round 1)

On April 23, 2020, the City Council approved the first phase of emergency funding to nonprofits outlined in Table 1 in order to respond to the economic impacts of COVID-19 to provide nonprofits with resources to meet the rapidly increasing demand for public services. On May 28, 2020, the City Council formally authorized Phase I of the CDBG-CV Round 1 allocations that were initially approved on April 23, 2020.

Approved Amendment No. 2 (CDBG-CV Round 1)

On August 13, 2020, the city approved Amendment No. 2 to allocate the remaining \$100,072.40 of CDBG-CV Round 1 funding. Of the remaining CDBG-CV funding, \$8,400.00 was allocated to Sacramento Self-Help Housing for temporary staffing at the Renters Helpline and the remaining \$91,672.40 was allocated to the Great Plates Delivered Program to be used as the city's required local match of 6.25 percent.

*Table 1:
Approved Amendments 1 & 2 (CDBG-CV Round 1 Funding)*

Activity	Activity Summary	Approval Date	Funding Allocation
Campus Life Connection, Sayonara Center	Operational support; purchase food and household supplies for community dinner distribution.	May 28, 2020	15,750.00
Citrus Heights Police Department, Supplemental Navigator Fund	Supplemental fund for the Homeless Navigator to help with housing deposits or motel vouchers for people experiencing homelessness who are high-risk or symptomatic and other essential services to homeless individuals sheltering in place.	May 28, 2020	125,000.00
Sunrise Christian Food Ministry, Emergency Food Closet	Operational support; purchase emergency food and household supplies for low-income residents.	May 28, 2020	30,000.00
W.E.A.V.E., Domestic Violence Response Team	Operational support; purchase motel vouchers, food, and household supplies for victims of domestic violence.	May 28, 2020	30,000.00

Sacramento Self-Help Housing, Renter's Helpline	Provides partial funding for temporary staffing at the Renter's Helpline, which is experiencing high call volume due to renter financial hardships and eviction moratorium questions. Funding provided as a part of a regional partnership.	August 13, 2020	8,400.00
Great Plates Delivered Program	Provides local match of 6.25% for the Great Plates Delivered Program, which provides three daily delivered restaurant meals to qualifying seniors.	August 13, 2020	91,672.40
Administration	Administration of the city's CDBG-CV funds in accordance with HUD's requirements and regulations.	May 28, 2020	75,205.60
Total			376,028.00

Proposed Amendment No. 3 (CDBG-CV 1 & 3 Funding)

The proposed Amendment No. 3 to the city's 2020-2024 Consolidated Plan and 2020 Action Plan would reallocate the \$91,672.40 in CDBG-CV Round 1 funding previously reserved for the Great Plates Delivered Program to the Supplemental Navigator Fund. Because it was determined the Federal Emergency Management Agency (FEMA) would reimburse the city's full cost to participate in this program, the city has \$91,672.40 in CDBG-CV Round 1 funds remaining to re-allocate to another CDBG-CV-eligible activity. Given the success of the Supplemental Navigator Fund and its need for additional funding, staff recommends reallocating the \$91,672.40 in CDBG-CV 1 funding from the Great Plates Delivered Program to the Supplemental Navigator Fund as listed in Table 2 on the following page.

Additionally, the proposed Amendment No. 3 would allocate \$135,384 in CDBG-CV Round 3 funding to the Fiscal Year 2021-22 Community Support Applicants with CDBG-CV-eligible activities as well as associated planning and administration costs as listed in Table 2 on the following page. This action will ensure the city's ability to continue these essential public service activities with CDBG-CV funding instead of General Fund revenue given the downturn in local and national economic conditions that have strained local resources. If Council approves this action, the city will have \$408,814 in CDBG-CV Round 3 funding remaining to allocate. Following this meeting, staff will convene the City Council's Quality of Life Committee to determine the next steps for allocating the remaining CDBG-CV funding.

*Table 2:
Proposed Amendment No. 3 (CDBG-CV Round 1 & 3 Funding)*

Activity	Activity Summary	CDBG-CV Funding Round	Proposed Funding Allocation
Supplemental Navigator Fund	Supplemental fund utilized by the city's Navigator to provide temporary housing vouchers, food, laundry vouchers, transportation, and security deposit assistance to current clients.	Round 1 <i>(Reallocation from Great Plates Delivered)</i>	91,672.40
Campus Life Connection, Sayonara Center	Operational support and supplies for afterschool program.	Round 3	15,000.00
Meals on Wheels	Senior meals delivered to eligible participants in Citrus Heights.	Round 3	82,000.00
Single Mom Strong	Distance learning support for children living in single-parent households in Citrus Heights.	Round 3	9,100.00
Sunrise Christian Food Ministry, Emergency Food Closet	Operational support; purchase emergency food and household supplies for low-income residents.	Round 3	6,720.00
Administration	Administration of the city's CDBG-CV funds in accordance with HUD's requirements and regulations.	Round 3	22,564.00
Total			227,056.40

Citizen Participation

In accordance with the city's Citizen Participation Plan, the CARES Act Amendment No. 3 with the proposed CDBG-CV Round 1 reallocation and Round 3 funding allocations was made available for public comment at least five days in advance of the public hearing on April 22, 2021. The city posted a public notice in *The Sacramento Bee* advertising the availability of the draft CARES Act Amendment, describing how to access and comment on the document, and announcing the April 22, 2021 public hearing date.

Attachments

1. Resolution No. 2021-____ Adopting the CARES Act Amendment No 3 to the 2020-2024 CDBG Consolidated Plan and 2020 Action Plan (Exhibit A) and Authorizing the City Manager to Execute All Contracts Necessary to Carry Out the CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and 2020 Action Plan for Coronavirus (CDBG-CV 1 & 3) Funding
2. Exhibit A, CARES Act Amendment No. 3 to the 2020-24 Consolidate Plan and 2020 Action Plan for the CDBG Program

RESOLUTION NO. 2021-_____

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CITRUS HEIGHTS,
CALIFORNIA ADOPTING THE CARES ACT AMENDMENT NO 3 TO THE 2020-2024
CDBG CONSOLIDATED PLAN AND 2020 ACTION PLAN (EXHIBIT A) AND
AUTHORIZING THE CITY MANAGER TO EXECUTE ALL CONTRACTS
NECESSARY TO CARRY OUT THE CARES ACT AMENDMENT NO. 3 TO THE
2020-2024 CONSOLIDATED PLAN AND 2020 ACTION PLAN FOR CORONAVIRUS
(CDBG-CV 1 & 3) FUNDING**

WHEREAS, the Community Development Block Grant (CDBG) Program, authorized pursuant to Title 1 of the Housing and Community Development Act of 1974, as amended (Act), requires that jurisdictions provide for the issuance of grants in order to attain the objective of providing decent housing and a suitable living environment and expanding economic opportunities, principally for persons of low- and moderate-income;

WHEREAS, with the passage of the Coronavirus Aid, Relief, and Economic Security (CARES) Act, a total of \$5 billion was included for the distribution to state and local governments through the CDBG Program in order to respond to circumstances created by the COVID-19 crisis;

WHEREAS, the city received a Round 1 allocation of \$376,028 and a Round 3 allocation of \$544,198 in CDBG-CV funds through the CDBG Program for a total CDBG-CV allocation of \$920,226 to respond to the COVID-19 crisis; and

WHEREAS, the city identified several nonprofits providing essential services, such as food and household supplies, monetary support, or significant social supports vulnerable populations, including low-wage workers, senior citizens, domestic violence victims, and people experiencing homelessness;

WHEREAS, on April 23, 2020, the City Council approved allocations of emergency funding in the amount of \$80,750 to nonprofits and \$125,000 to create a supplemental fund for the Navigator Program to use a portion of the special allocation of CDBG-CV funding through the CARES Act to provide short-term nonprofit assistance and additional support for the Homeless Navigator Program needs;

WHEREAS, on May 28, 2020, the City Council adopted the CARES Act Amendment No. 1 to incorporate the preliminary CDBG-CV allocations approved April 23, 2020, into the city's 2020-2024 Consolidated Plan and 2020 Action Plan in order to access CDBG-CV funds;

WHEREAS, on August 13, 2020, the City Council adopted the CARES Act Amendment No. 2 to allocate the remainder of the city's CDBG-CV Round 1 grant funding;

WHEREAS, the city no longer has a need to fund the local match for the Great Plates Delivered Program, but does have a need for additional homeless services to support the Homeless Navigator Program through the Supplemental Navigator Fund;

WHEREAS, due to financial hardship, the city may no longer be able to sustain support Community Support applicants with General Fund revenue and has a need to fund these nonprofits through CDBG-CV 3 funding in order to maintain essential public services in response to the impacts of COVID-19; and

WHEREAS, Citrus Heights citizens were afforded the opportunity to review and comment on the proposed CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and the 2020 Action Plan in accordance with the city's adopted Citizen Participation Plan, including a five-day public review period and public hearing.

NOW, THEREFORE, BE IT RESOLVED THAT THE CITY COUNCIL OF THE CITY OF CITRUS HEIGHTS HEREBY:

1. Approves the CARES Act Amendment No. 3 to the 2020-24 Consolidated Plan and 2020 Action Plan (Exhibit A).
2. Authorizes the City Manager, to execute all contracts necessary to carry out the CARES Act Amendment No. 3 to the 2020-2024 Consolidated Plan and the 2020 Action Plan.

PASSED AND ADOPTED by the City Council of the City of Citrus Heights the 22nd day of April 2021, by the following roll call vote:

Ayes:	Council Members:
Noes:	Council Members:
Abstain:	Council Members:
Absent:	Council Members:

Steve Miller, Mayor

ATTEST:

Amy Van, City Clerk

Exhibit

- A. CARES Act Amendment No. 3 to the 2020-24 Consolidate Plan and 2020 Action Plan for the CDBG Program

CARES Act Amendment No. 3 (Substantial Amendment) to the 2020-2024 Consolidated Plan and 2020 Action Plan

Public Review Copy
Public Hearing Date: 4/22/2021

Executive Summary

ES-05 Executive Summary – 24 CFR 91.200(c), 91.220(b)

1. Introduction

The Consolidated Plan is required by the U.S. Department of Housing and Urban Development (HUD) in order for local jurisdictions to receive federal housing and community development funds under the Community Development Block Grant (CDBG) program. The overall goal of the CDBG program is to develop viable urban communities by providing decent housing, a suitable living environment, and expanding economic opportunities.

The Consolidated Plan serves the following functions:

- A planning document for the City of Citrus Heights , which builds on a participatory process;
- An application for federal funds under HUD’s CDBG formula grants program;
- A strategy to be followed in carrying out HUD’s programs; and
- An allocation of funds to specific projects for the first program year of the Consolidated Plan.

Consolidated Plan Timeframe

The 2020-2024 Consolidated Plan covers the five-year timeframe from January 1, 2020 to December 31, 2024. During this timeframe, the city anticipates receiving only CDBG funding directly from HUD and HOME funding through the city’s consortium partnership with the Sacramento Housing and Redevelopment Agency (SHRA).

Consolidated Plan Format

In order to standardize the preparation of the Consolidated Plan across all grantees, HUD developed a tool in the Integrated Disbursement and Information System (IDIS), which is the system in which projects and funding are tracked. The Consolidated Plan tool provides data from HUD-selected sources, primarily the 2011-2015 American Community Survey (ACS) dataset. Newer data has been included wherever possible to ensure the longevity of the city’s 2020-2024 Consolidated Plan. The tool consists of a question-and-answer format with limited opportunities for customization. In some cases, the questions

are targeted toward grantees receiving other HUD funding sources, such as HOME, Emergency Shelter Grant (ESG), and Housing Opportunities for People with AIDS (HOPWA).

Income Definitions

The primary focus of the CDBG program is assisting low-income households. Throughout this document, there are several references to various income levels:

- Extremely low-income households are those earning at or below 30% of the area median income.
- Very low-income households are those earning 31% to 50% of the area median income.
- Low-income households are those earning 51% to 80% of the area median income.
- Moderate-income households are those earning between 81% and 100% of the area median income.
- Above-moderate income households are those earning 101% of the area median income and over.

The maximum income for each category is defined by HUD on an annual basis, and is adjusted based on household size. The 2019 HUD income limits by household size for the Sacramento—Roseville--Arden-Arcade, CA HUD Metro FMR Area are listed in the table below. The 2019 HUD median family income for the Citrus Heights area is \$83,600.

Income Limit Category	Persons in Family				
	1	2	3	4	5
Very Low (50%) Income Limits	30,250	34,550	38,850	43,150	46,650
Extremely Low Income Limits	18,150	20,750	23,350	26,200	30,680
Low (80%) Income Limits	48,350	55,250	62,150	69,050	74,600

Table 1 - 2020 HUD Income Limits for Selected Households

2. Summary of the objectives and outcomes identified in the Plan Needs Assessment Overview

The Needs Assessment identified several target populations:

- Extremely low-income and very low-income individuals and households;
- Homeless persons;
- Seniors; and
- Youth.

The Needs Assessment also identified several types of projects that are needed in the Citrus Heights community:

Housing Needs

The following housing needs were identified during the community survey and stakeholder consultations:

- Affordable housing. The community survey indicated a significant need for affordable housing, particularly for seniors, workforce households, and disabled persons. Nearly 50 percent of survey respondents report paying more than 30 percent of their monthly income on rent or mortgage costs. Residents prioritized increasing homeownership for single-family homes as the most needed housing program, followed by programs to create more affordable rental units and construction of new, single-family homes.
- Permanent supportive housing. The community survey indicated homelessness as a high priority for the city to address. The survey also indicated the two most important services to provide to people experiencing homelessness are mental health services and supportive services, such as job training, life skills, financial training, and health care. Given the need for additional affordable housing along with mental health and supportive services, permanent supportive housing is a possible solution to address both needs.
- Transitional housing. During the city's consultation with the Citrus Heights Homeless Assistance Response Team (HART), a need for transitional housing was expressed. This need was also noted at the Collaborative work session and in the community survey responses. The city currently does not have a transitional, or interim, house for homeless within the city limits while the city connects them with permanent housing and/or other programs and services. There is a need for transitional housing for families, including families with children. Survey respondents indicated homeless housing solutions are a high priority in the city.

Homeless Needs

The following homeless needs were identified during the community survey and stakeholder consultations:

During the city's consultation with stakeholders as part of the development of the Consolidated Plan process. The following homeless needs were identified:

- Mental health services. Over 68 percent of community survey respondents indicated the most-needed service for those experiencing homelessness in Citrus Heights are mental health services.
- Supportive services. The community survey results show 63 percent of respondents believe supportive services, such as job training, life skills, financial training, and healthcare are the most-needed service for those experiencing homelessness in Citrus Heights.
- Day center. About 33% community survey respondents indicated a need for a day center as the top need for people experiencing homelessness to have mail delivered, shower, etc.

- Transitional housing. As stated in section NA 10 Housing Needs Assessment, the community survey and the city's consultation with the Citrus Heights Homeless Assistance Response Team (HART) identified a need for transitional housing. About 31 percent of survey respondents indicated transitional housing as the top priority need for homeless services in Citrus Heights. This need was also noted at the Collaborative work session. The city currently does not have a transitional house for homeless within the city limits. There is a need for transitional housing for families, including families with children. Survey respondents indicated homeless housing solutions are a high priority in the city.

Public Facility Needs

Citrus Heights contains a large number of nonprofit organizations, most of which have facility needs. Public facility needs identified over the next five years include the following:

- Neighborhood facilities. During staff's consultation with the Citrus Heights Collaborative, several non-profits expressed the need for more recreational opportunities within Citrus Heights. Although there are a number of parks within the city, many are in need of rehabilitation and safety due to homeless activity in parks was listed as a concern. During staff's consultation with the Sunrise Recreation and Park District, funding was listed as a barrier to rehabilitating local parks and increasing services offered in Citrus Heights.
- Youth center. Similarly, in the city's online survey of various public facility needs, respondents marked a youth center, as the most desired public facility need. Although the city developed a neighborhood youth center on Sayonara Drive, the community expressed a need for more youth facilities within Citrus Heights.
- Emergency food closet. During staff's consultation with the Sunrise Christian Food Ministry, the nonprofit expressed a need for a larger emergency food closet. The group currently operates out of a small building at the Advent Lutheran Church in Citrus Heights. Due to the small size of the building, it is challenging to meet the increasing need for emergency food in the Citrus Heights community. In addition, about 19 percent of community survey respondents noted food banks as one of the most important services for those experiencing homelessness in Citrus Heights.

Public Improvement Needs

The city has a continued need for infrastructure and accessibility-related public improvements, including the following:

- Infrastructure improvements. About 62 percent of community survey respondents stated the city's street surfaces need improvement. In addition, the city has a need to improve drainage facilities.
- ADA-accessible curb ramps. Curb ramps allow disabled residents to have a safe path of travel on City sidewalks.

- Sidewalk infill. Sidewalk infill projects install sidewalk, curb, and gutter improvements in areas where there is currently no sidewalk. Generally, the sidewalk connects with existing sidewalk on both ends, or connects residents with a facility or other public improvement (e.g., trail).
- Signalized intersection improvements. Upgrades to signalized intersections to be ADA accessible.

Other ADA improvements. Occasionally, other types of ADA improvements will be identified by the City's General Services Department.

3. Evaluation of past performance

The city evaluated its past performance as part of setting the goals and strategies associated with this Consolidated Plan. The city successfully completed the following projects during the 2015-2019 Consolidated Plan period:

- Highland Avenue Accessibility Project;
- \$1.3 million in accessibility improvements;
- \$1.2 million in health and safety repairs for low-income homeowners; and
- 30 mobilehome health and safety repair loans/grants to low-income mobilehome homeowners.

The city typically devotes the full 15 percent allowed to public services, serving thousands of households with a range of services, from meals to housing counseling. Many households have come to depend on these services. The city plans to continue to devote the maximum allowable funding to public services, and to supplement it with General Fund revenue.

4. Summary of citizen participation process and consultation process

The city offered several opportunities for participation and comment throughout the development of the Consolidated Plan consistent with the city's Citizen Participation Plan. Outreach included traditional local newspaper announcements for public meetings and hearings as well as publication on the city's website. The city also included an online survey and promoted the public meetings and survey through social media platforms such as Twitter, Facebook, and Next Door. The city developed a second online survey to collect feedback from stakeholders. The city hosted a public meeting on August 28 at the Sayonara Neighborhood Center to gather input on current needs and priorities. City staff provided a report to the City Council on September 12 on the status of the Consolidated Plan development and to obtain feedback from the City Council, which was televised. On September 13, city staff held a brainstorming session at the monthly Collaborative meeting to gather input on current needs and priorities from local non-profits, health care providers, and local government agency representatives. In addition, the city posted the draft plan in advance of a 30-day public review and comment period beginning September 26. The draft plan was reviewed at the October 10 public hearing and the final plan will be considered at the October 24 public hearing.

5. Summary of public comments

Feedback received through the outreach efforts, such as meetings with nonprofits, service providers, and online survey responses, is incorporated into the Process and Needs Assessment sections and, to a lesser degree, the Market Analysis section. Comments received at the two public hearings for this plan will be incorporated into the plan and included in the final submission to HUD.

6. Summary of comments or views not accepted and the reasons for not accepting them

The city has not rejected any comments.

7. Summary

Based on the needs assessment, market analysis, and consultations with the community and stakeholders, the following are the selected key community needs for the Consolidated Plan:

- Build healthy communities;
- Improve public infrastructure;
- Enhance accessibility;
- Improve public services for priority populations;
- Improve housing access and affordability; and
- Expand economic opportunities.

These priorities align closely with three of the city Council's five goals: "diversify for a changing economy," "improve community vibrancy and engagement," and "improve streets and infrastructure."

Within those priorities, the city identified eight main goals:

- Foster affordable housing;
- Provide services for people experiencing homelessness;
- Provide services for seniors and youth;
- Provide additional public services responsive to current public needs;
- Improve accessibility;
- Construct/upgrade public facilities;
- Effectively administer CDBG program to benefit the Citrus Heights community; and
- Affirmatively further fair housing.

To address these goals, the city plans to fund the construction of affordable housing on Sayonara Drive, provide loans to homeowners for health and safety repairs, support nonprofits offering a wide-range of social services, provide services to those experiencing homelessness, assist with accessibility and other improvements to public facilities. Other types of projects may be considered as needed throughout the Consolidated Plan timeframe. The city anticipates funding activities using a variety of sources, including

CDBG, HOME, General Fund, and grants received by the City. The city will work with local and regional nonprofits, as well as affordable housing developers, to implement many of the activities.

The Process

PR-05 Lead & Responsible Agencies - 91.200(b)

1. Describe agency/entity responsible for preparing the Consolidated Plan and those responsible for administration of each grant program and funding source

The following are the agencies/entities responsible for preparing the Consolidated Plan and those responsible for administration of each grant program and funding source.

Agency Role	Name	Department/Agency
CDBG Administrator	CITRUS HEIGHTS	Community Development Department
HOME Administrator	CITRUS HEIGHTS	SHRA

Table 2– Responsible Agencies

Narrative

The City of Citrus Heights Housing and Grants Division, within the Community Development Department, serves as the lead agency for the Consolidated Plan and the administration of CDBG, HOME, and other housing and community development funds. Within the Housing and Grants Division, the Housing and Human Services Program Coordinator oversees the administration of the CDBG program. The Grants and Housing Technician assists in administering the CDBG program.

For the receipt of HOME funds, Citrus Heights is in a consortium with Sacramento County, City of Sacramento, and the City of Rancho Cordova. The Sacramento Housing and Redevelopment Agency (SHRA) is the lead agency in the city's HOME consortium.

Consolidated Plan Public Contact Information

City of Citrus Heights
Attn: Stephanie Cotter
6360 Fountain Square Drive
Citrus Heights, CA 95621
(916) 727-4768
scotter@citrusheights.net
www.citrusheights.net

PR-10 Consultation - 91.100, 91.110, 91.200(b), 91.300(b), 91.215(l) and 91.315(l)

1. Introduction

The City of Citrus Heights made efforts to consult a wide-ranging group of stakeholders during the development of the Consolidated Plan. The city's consultation efforts are summarized in the following section.

Provide a concise summary of the jurisdiction's activities to enhance coordination between public and assisted housing providers and private and governmental health, mental health and service agencies (91.215(l)).

In preparing the Consolidated Plan, the City of Citrus Heights consulted with a number of local service providers and public agencies to identify local needs and evaluation opportunities for partnership and improved coordination. The city also participated in an extensive public outreach campaign to assess community needs and priorities. The following sections will discuss the consultation process, the public comment process, and the planned coordination efforts for the next five-year Consolidated Plan cycle.

Describe coordination with the Continuum of Care and efforts to address the needs of homeless persons (particularly chronically homeless individuals and families, families with children, veterans, and unaccompanied youth) and persons at risk of homelessness

Sacramento Steps Forward is the nonprofit group that manages the regional Continuum of Care. The city cooperates with Sacramento Steps Forward whenever possible, including serving on the Continuum of Care Advisory board, participating in the Funders Collaborative, prompt response to requests for data, and ongoing participation in the Continuum of Care discussions. The city's Housing and Human Services Program Coordinator maintains a seat on the Continuum of Care Advisory Board and actively participates in regional coordination through the Funder's Collaborative and other regional subcommittees. The city supports the Citrus Heights Homeless Assistance Response Team (HART) and its efforts, including the Winter Sanctuary, Student Connect, a resource fair for students and families in transition, the Veterans Stand Down, and other events throughout the year. The city's Homeless Navigator provides outreach and services for people experiencing homelessness. The city's Homeless Navigator also provides housing counseling services to those at-risk of homelessness. Additionally, Meals on Wheels and the Sunrise Christian Food Ministry provide emergency food services to those at-risk of homelessness and those currently experiencing homelessness.

Describe consultation with the Continuum(s) of Care that serves the jurisdiction's area in determining how to allocate ESG funds, develop performance standards and evaluate outcomes, and develop funding, policies and procedures for the administration of HMIS

Sacramento Steps Forward administers the Homeless Information Management System (HMIS) as well as the available Emergency Shelter Grant (ESG) funds for the Sacramento region. The city participates in improving the HMIS system through the Continuum of Care Advisory Board. The city's Housing and Human Services Program Coordinator serves on the Continuum of Care Advisory Board and assists in the development of regional programs, policies, and procedures. The city is working to become more involved in the allocation of ESG funds through the regional Funders Collaborative.

2. Describe Agencies, groups, organizations and others who participated in the process and describe the jurisdictions consultations with housing, social service agencies and other entities

Table 3– Agencies, groups, organizations who participated

1	Agency/Group/Organization	City of Citrus Heights
	Agency/Group/Organization Type	Other government - Local
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Public Housing Needs Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs Economic Development
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	Staff consulted with representatives from the Community Development Department, General Services Department, and Police Department as part of the development of the Consolidated Plan. Stakeholder surveys were emailed to representatives from the three departments listed above. In addition, staff held meetings with department representatives to further discuss department priorities and opportunities for increased coordination. In addition, all city staff members were emailed a link to participate in the community survey. In discussions with other departments, staff in the Citrus Heights Police Department identified a need for additional resources for the Citrus Heights Homeless Navigator. While she has been successful, there is a need for additional housing, both temporary and permanent, and resources for related costs to eliminate barriers to housing, such as application fees and transportation. Staff in the General Services Department identified a need for accessibility improvements to public infrastructure, such as upgrades to signalized intersections and ADA-compliant sidewalks and crosswalks. The Community Development Department identified a need for improvements to parks and public facilities within the city as well as a need to rehabilitate aging housing stock.

2	Agency/Group/Organization	Citrus Heights Collaborative
	Agency/Group/Organization Type	Housing Services - Housing Services-Children Services-Elderly Persons Services-Persons with Disabilities Services-Victims of Domestic Violence Services-homeless Services-Health Services-Education Services-Employment Service-Fair Housing Other government - Local Civic Leaders
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Public Housing Needs Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs Anti-poverty Strategy

	<p>How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?</p>	<p>The Citrus Heights Collaborative is an informal network of community members representing government agencies, nonprofits, religious institutions, businesses, and neighborhood groups. Specifically, the group is comprised of local government, nonprofits, and community members, fraternal and faith-based organizations. City staff held a work session at the Collaborative meeting on September 12, 2019 Collaborative meeting to obtain stakeholder feedback. A total of 21 stakeholders attended the Collaborative work session. Citrus Heights Staff: Colleen McDuffee, Community Development Director Mary Poole, Operations Manager, General Services Organizations: San Juan Unified School District, Natalia Aguirre, Elizabeth Thomas, Christina Sparks, Mariela Silva, Debbie Chiguina-Owens Aetna Better Health - Merrett Sheridan, Martin Gonzales CH Resident (Antelope Neighborhood), Ken Horner Sylvan Middle School Neighborhood Liaison, April Jacek Sunrise Recreation Park District, Becky Henz The Glass Slipper, Jackie Guzman Sayonara Center, Julie Habeeb Crossroads, Matthew Rorario, Maihina Lee Stakeholders from the Collaborative noted the clients they serve have a need for increased access to transportation, free after school programs, mental health services, affordable housing (particularly for transitional youth), increased number of foster families, employment opportunities, emergency food, and recreational opportunities.</p>
3	Agency/Group/Organization	SUNRISE RECREATION AND PARK DISTRICT
	Agency/Group/Organization Type	<p>Services-Children</p> <p>Services-Elderly Persons</p> <p>Services-Persons with Disabilities</p> <p>Services-homeless</p> <p>Other government - Local</p>

	What section of the Plan was addressed by Consultation?	Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	The Sunrise Recreation Park District (SRPD) participated via the city's online stakeholder survey and at the September 13 Collaborative meeting. SRPD staff noted a lack of funding as a factor that impacts its ability to provide services to the Citrus Heights community. SRPD staff also stated generally how the homeless population continues to grow and parks are being heavily impacted by transient and drug activity. SRPD also noted a need to get the word out about available services and programs.
4	Agency/Group/Organization	About Kidz
	Agency/Group/Organization Type	Services-Children
	What section of the Plan was addressed by Consultation?	Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	About Kidz participated in the city's online stakeholder survey. The nonprofit provides school supplies, sports program sponsorships, and tutoring services for low-income students. The nonprofit noted the following as their greatest needs include: school supplies, access to youth sports, tutoring, transpiration, and mentoring. Funding was listed as the organizations greatest need.
5	Agency/Group/Organization	Sacramento Self Help Housing
	Agency/Group/Organization Type	Services-Children Services-Elderly Persons Services-Persons with Disabilities Services-homeless

	What section of the Plan was addressed by Consultation?	Housing Need Assessment Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	Sacramento Self-Help Housing (SSHH) provides housing counseling and homeless navigation services for the City of Citrus Heights. The city's Homeless Navigator completed the city's online stakeholder survey and identified housing as the greatest need of her clients.
6	Agency/Group/Organization	Campus Life Connection, Sayonara After School Program
	Agency/Group/Organization Type	Services-Children
	What section of the Plan was addressed by Consultation?	Homeless Needs - Families with children Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	Campus Life Connection participated in the city's online stakeholder survey and the Collaborative work session on September 13. The nonprofit operates the Sayonara Center and provides after-school tutoring, mentoring, and food to Citrus Heights students. The following were identified as the client's greatest needs: nonperishable food for children when school is out (weekends & holidays), mentorship, safe space, community, homework assistance and support, recreational opportunities for youth. The nonprofit noted its organizations greatest needs are locating enough food to provide good meals each day and funding to staff the Center.
7	Agency/Group/Organization	WEAVE
	Agency/Group/Organization Type	Services-Children Services-Persons with Disabilities Services-Victims of Domestic Violence Services-Health Services - Victims Child Welfare Agency

	What section of the Plan was addressed by Consultation?	Homeless Needs - Families with children Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	WEAVE completed the city's online survey. The organization stated their clients greatest need is safety, including emergency shelter, protective orders, legal assistance, advocacy, and counseling. WEAVE noted barriers to providing services include transportation and funding. Existing shelters are not located in Citrus Heights which can create transportation barriers for victims who need shelter but also continue to work or have children in Citrus Heights.
8	Agency/Group/Organization	City of Woodland
	Agency/Group/Organization Type	Other government - Local
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Public Housing Needs Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs

	<p>How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?</p>	<p>Staff sent an email to representatives of neighboring local governments asking these questions.1)What are your housing and community development priorities? 2)Is there anything the City of Citrus Heights can do to improve coordination between local governments/agencies in order to advance regional housing and community development priorities? Dan Sokolow, Senior Planner, noted the City of Woodland have the following high priority needs including: Public Services: emergency food and shelter for the homeless and at risk homeless, youth services, employment training, crime awareness, fair housing counseling, and health services. Infrastructure: public facilities and improvements (general, including accessibility for physically disabled, ADA), youth centers, parks, recreational facilities, rehabilitation of single family homes, and administration for single family rehabilitation program.</p>
9	Agency/Group/Organization	San Juan Unified School District
	Agency/Group/Organization Type	<p>Services-Children</p> <p>Services-Persons with Disabilities</p> <p>Services-homeless</p> <p>Services-Education</p>
	What section of the Plan was addressed by Consultation?	<p>Homelessness Strategy</p> <p>Homeless Needs - Chronically homeless</p> <p>Homeless Needs - Families with children</p> <p>Homelessness Needs - Unaccompanied youth</p> <p>Non-Homeless Special Needs</p>
	<p>How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?</p>	<p>City staff consulted representatives from the San Juan School District (SJUSD) via an online stakeholder survey and the Collaborative work session, SJUSD representatives noted a need among clients for free after school programs, expanded mental health services, one-on-one mentoring programs, community events, housing for transitional aged youth, and more foster families. SJUSD stated barriers to provide services in Citrus Heights include a lack of funding, transportation, and trust issues related to immigration status.</p>

10	Agency/Group/Organization	SACRAMENTO HOUSING AND REDEVELOPMENT AGENCY
	Agency/Group/Organization Type	PHA Services - Housing Regional organization
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Public Housing Needs Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	City staff consulted with SHRA via online stakeholder survey as well as targeted emails and phone calls. SHRA provided the city with information related to public housing properties; this information has been included in the Market Analysis and Needs Assessment sections.
11	Agency/Group/Organization	First Call Hospice
	Agency/Group/Organization Type	Services-Elderly Persons Services-Health
	What section of the Plan was addressed by Consultation?	Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	City staff consulted with Rachelle Doty via stakeholder survey as well as targeted emails and phone calls. Rachele Doty noted the greatest need for her clients include caregiving, caregiving support, and financial assistance. The survey response indicates there is a need for more community outreach to inform the public of hospice benefits currently available to community members as part of the Medicare program.

12	Agency/Group/Organization	Citrus Heights Homeless Assistance Resource Team
	Agency/Group/Organization Type	Services - Housing Services-Children Services-Elderly Persons Services-Persons with Disabilities Services-Persons with HIV/AIDS Services-Victims of Domestic Violence Services-homeless Services-Health Services-Education Services-Employment Service-Fair Housing Services - Victims
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	City staff consulted with Kathilynn Carpenter, Executive Director of HART via email. Ms. Carpenter noted a significant need for funds to provide housing assistance. In addition, Ms. Carpenter expressed a need for a HART transitional home to provide emergency housing in Citrus Heights. Two HART volunteers who filed out the online stakeholder survey pointed out the need for more housing, clothing, and support for those experiencing homelessness in Citrus Heights. The group expressed a need for an additional homeless navigator and more funding to support services. The biggest barriers cited by HART include lack of housing for vulnerable individuals and empathy from community members toward people in need.
13	Agency/Group/Organization	Citrus Heights Chamber of Commerce
	Agency/Group/Organization Type	Business and Civic Leaders
	What section of the Plan was addressed by Consultation?	Non-Homeless Special Needs

	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	City staff consulted representatives of the Citrus Heights Chamber of Commerce via an online stakeholder survey. Ilene Martzen, Chamber of Commerce Board Chair, stated the clients are primarily business owners. The Chambers greatest needs include opportunities for business owners to meet other business owners in order to be referred to business associates, friends, and families to grow their businesses. The Chamber is working to increase the number of signature economic development events offered annually in Citrus Heights.
14	Agency/Group/Organization	Sacramento County Behavioral Health Services
	Agency/Group/Organization Type	Services-Children Services-Elderly Persons Services-Persons with Disabilities Services-Persons with HIV/AIDS Services-Victims of Domestic Violence Services-homeless Services-Health Services-Education Services - Victims Health Agency
	What section of the Plan was addressed by Consultation?	Housing Need Assessment Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	Staff consulted with Sacramento County Division of Behavioral Health Services via the city's online stakeholder survey. Monica Rocha-Wyatt submitted a survey response indicating there is a need for affordable housing, educational support and opportunities, and supporting navigating systems. One barrier noted is the lack of adult mental health service provider in Citrus Heights.
15	Agency/Group/Organization	Development Services - City of Elk Grove
	Agency/Group/Organization Type	Other government - Local

	What section of the Plan was addressed by Consultation?	Housing Need Assessment Public Housing Needs Homelessness Strategy Homeless Needs - Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth Non-Homeless Special Needs
	How was the Agency/Group/Organization consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?	City staff consulted with City of Elk Grove via email. Alicia Tutt, Development Services Housing with City of Elk Grove provided input. What are your agency's top priorities related to housing and community Development? 1. New construction of affordable housing units. 2. Addressing missing middle housing.3. Strategic land acquisition4. Improve accessibility. Is there anything the city can do to help advance your agency's housing community development priorities? 1. Continue to be an active partner in regional collaborations and partnerships.

Identify any Agency Types not consulted and provide rationale for not consulting

No agency types were excluded from consultation.

Other local/regional/state/federal planning efforts considered when preparing the Plan

Name of Plan	Lead Organization	How do the goals of your Strategic Plan overlap with the goals of each plan?
Continuum of Care	Sacramento Steps Forward	Addressing housing and service needs for homeless individuals and families.
Housing Element	City of Citrus Heights	Addressing multi-family housing sites, actions to encourage development of new affordable housing, and barriers to new affordable housing.

Table 4– Other local / regional / federal planning efforts

Describe cooperation and coordination with other public entities, including the State and any adjacent units of general local government, in the implementation of the Consolidated Plan (91.215(l))

The City of Citrus Heights will continue to cooperate and coordinate with other public agencies to help meet the goals identified in the Consolidated Plan. These agencies include the California Department of Housing and Community Development, Sacramento Housing and Redevelopment Agency, Sacramento Steps Forward, HUD, Department of Veterans Affairs, CalVet, San Juan Unified School District, Sacramento County Department of Health and Human Services, Sacramento County Department of Human Assistance, Sacramento County, City of Sacramento, City of Rancho Cordova, City of Elk Grove, as well as any other agencies or units of local government that may assist the city in reaching its Strategic Plan goals.

Narrative

As part of the Funders Collaborative, city staff coordinates regularly with Sacramento County, City of Elk Grove, City of Rancho Cordova, Sacramento Housing and Redevelopment Agency, and a number of other local agencies. Citrus Heights recently completed in a regional Analysis of Impediments in participation with a number of local agencies. Citrus Heights also partners with Sacramento County, SHRA, City of Rancho Cordova, and the City of Folsom on a regional Renters Helpline service. Lastly, the city is partnering with the State of California (through the No Place Like Home Program), Sacramento County, and SHRA on the Sunrise Pointe Apartments Project - a 46-unit permanent supportive housing project in Citrus Heights. The City of Citrus Heights allocated its future HOME funding to support this project and it is currently awaiting tax credits to finalize the project financing

PR-15 Citizen Participation - 91.105, 91.115, 91.200(c) and 91.300(c)

1. Summary of citizen participation process/Efforts made to broaden citizen participation Summarize citizen participation process and how it impacted goal-setting

The City of Citrus Heights went through an expensive public comment process. All public comments were accepted throughout the public participation process, and the survey results and public comment feedback received were used to help shape the Needs Assessment and Strategic Plan sections of the Consolidated Plan and Annual Action Plan.

2020-24 Consolidated Plan and 2020 Annual Action Plan:

The City of Citrus Heights completed a robust citizen participation process during the development of the 2020-2024 Consolidated Plan. Outreach included traditional local newspaper announcements for meetings, public hearings, and publications on the city's website. The city also conducted two online surveys – one for community members and one targeted at stakeholders. The city promoted the public meetings and survey through social media platforms such as Twitter, Facebook, and Next-door; published news items on the city's website, encouraged local press coverage, and held a community meeting in a lower-income neighborhood to increase participation.

September 25, 2019 – 2020-24 Consolidated Plan and 2020 Annual Action Plan was noticed for public comment in The Sacramento Bee.

October 10, 2019 – A public hearing took place at the City of Citrus Heights Council meeting to review draft 2020-24 Consolidated Plan and 2020 Annual Action Plan. Several public comments were received and accepted. See public comments listed in the table below.

CDBG-CV Funding (Phase I):

April 23, 2020 – A virtual public meeting was held to discuss the need to provide emergency funding to the city's CDBG grantees to expand existing capacity due to increasing demand from the impacts of COVID-19. Several public comments were received and accepted. See public comments listed in the table below.

May 22, 2020 – Draft Amended Citizen Participation Plan and CARES Act Amendment was noticed for public comment in The Sacramento Bee.

May 28, 2020 – A public hearing took place at the City of Citrus Heights Council meeting to review draft Amended Citizen Participation Plan and CARES Act Amendment. No public comment was received.

CDBG-CV Funding (Phase II):

June 19, 2020 – A public notice was published in The Sacramento Bee to discuss the second phase allocations for the city's CDBG-CV funding.

June 25, 2020 – City held a virtual public meeting to discuss and receive feedback on the second phase of the city's CDBG-CV funding allocations. No public comment was received.

August 7, 2020 – A public notice was published in The Sacramento Bee to advertise the review of the CARES Act Amendment for the second phase of the city's first award of CDBG-CV funding.

August 13, 2020 – City held a virtual public meeting to discuss and receive feedback on the CARES Act Amendment for the second phase of CDBG-CV funding allocations. No public comment was received.

The table below outlines specific public out reach efforts and public comments.

Citizen Participation Outreach

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
1	Community Needs Survey	<p>Minorities</p> <p>Non-English Speaking - Specify other language: Spanish</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Public Service Stakeholders</p>	<p>The city received 342 survey responses.</p> <p>The survey was available in both online and paper copies that were available for completion at City Hall and through key service providers.</p>	<p>Survey results indicated the highest priority for residents is to provide services to people experiencing homelessness. The survey responses also indicated a desire for more community events and activities to increase opportunities to socialize; a need for increased transportation options to improve access to services; and a need for improvements to public infrastructure and recreational areas as well as public safety. Complete survey responses are included in the attached appendix.</p>	<p>The city accepted all comments.</p>	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
2	Public Meeting	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Neighborhood Residents & Stakeholders</p>	Two stakeholders attended a CDBG grant application workshop on July 10, 2019.	Potential applicants were encouraged to apply for funding, and the application process was explained.	The city accepted all comments.	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
3	Public Meeting	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Neighborhood Residents & Stakeholders</p>	<p>Approximately 50 people attended the Community Priorities Workshop on August 28, 2019 at the Sayonara Neighborhood Center. The workshop was publicly noticed 7 days prior to the workshop in The Sacramento Bee, and the public notice is attached.</p>	<p>The meeting identified the top two needs as 1) expanding homeless housing and services and 2) foster affordable housing. Participants also noted a need for a wide-range of public services. Participants also expressed a desire to focus resources on extremely-low income households, families with children, and persons with mental disabilities. A summary of the workshop responses is attached in the appendix.</p>	<p>The city accepted all comments.</p>	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
4	Public Meeting	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Neighborhood Residents & Stakeholders</p>	City Council meeting on September 12, 2019.	No comments were received.	The city did not reject any comments.	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
5	Public Meeting	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Neighborhood Residents & Stakeholders</p>	The draft Consolidated Plan was noticed for public comment in The Sacramento Bee on September 25, 2019. A public hearing to review the draft plan took place on October 10. The notice is in the appendix.	No comments were received.	The city did not reject any comments.	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
6	Community Needs Survey	Non-profit service providers	The city provided an online survey to non-profit service providers, affordable housing developers, public agencies, health service providers, advocacy groups, and other stakeholders to gather feedback on current needs and priorities of their clients and assess any barriers they face in providing services to the Citrus Heights community.	12 stakeholders responded to the survey. The responses indicate a need for housing; homeless services and transitional/emergency housing; public services, such as childcare and youth programs; as well as networking and employment opportunities.	The city accepted all comments.	

7	Public Meeting	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Residents and Service Providers in Citrus Heights</p>	<p>The city held a virtual public meeting on April 23, 2020, to discuss the need to provide emergency funding to the city's CDBG grantees to expand existing capacity due to increasing demand from the impacts of COVID-19.</p>	<p>Scott Young, the Homeless Outreach Supervisor for Sacramento Self-Help Housing, spoke in support of additional funding for homeless services, including motel rooms for unhoused individuals and families to prevent the contraction and spread of COVID-19 by providing a safe place to remain socially distant. Rocky Peterson, Sunrise Christian Food Ministry Board President, spoke about the food bank's need for emergency food and supplies to keep up with increasing demand and associated costs. Beth Hassett, representing WEAVE, commented about the increasing</p>	<p>The city did not reject any comments.</p>	
---	----------------	---	--	---	--	--

				<p>need for domestic violence services in response to COVID-19. WEAVE requested additional CDBG funding to purchase motel rooms and emergency supplies for clients. John Foley, Executive Director of Sacramento Self-Help Housing, commented that moving people experiencing homelessness into motels is the primary need in response to COVID-19. Dan Palmer and Julie Habeeb spoke on behalf of Campus Life. They stated the Sayonara Center is feeding an average of 50 children and youth on a daily basis and is requesting additional funding to provide free meals to low-income families when the Sayonara</p>		
--	--	--	--	---	--	--

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
				Center is closed due to COVID-19. The Sayonara Center had to cancel its annual fundraiser due to COVID-19, and at the same time, is experiencing an increased need for services due to COVID-19.		
8	Newspaper Ad	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Residents of Citrus Heights and Service Providers</p>	The city posted a public notice in the Sacramento Bee on May 22, 2020, to notify the public about the public comment period for the draft amended Citizen Participation Plan and CARES Act Amendment, and advertising the date of the public hearing on May 28 to consider those documents.	No comments were received.	The city did not reject any comments.	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
9	Public Hearing	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Citrus Heights Residents and Service Providers</p>	<p>The city held a virtual public hearing on May 28, 2020, to receive public feedback on the draft amended Citizen Participation Plan and the draft CARES Act Amendment for the first phase of the city's CDBG-CV allocations.</p>	<p>The city did not receive any comments.</p>	<p>The city did not reject any comments.</p>	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
10	Newspaper Ad	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Citrus Heights Residents and Service Providers</p>	The city posted a public notice in the Sacramento Bee on June 19, 2020, advertising a Public Meeting on June 25, 2020, to discuss the second phase allocations for the city's CDBG-CV funding.	The city did not receive any comments.	The city did not reject any comments.	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
11	Public Meeting	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Citrus Heights residents and service providers</p>	The city held a virtual Public Meeting with the City Council on June 25, 2020, to discuss and receive feedback on the second phase of the city's CDBG-CV funding allocations.	The city did not receive any comments.	The city did not reject any comments.	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
12	Newspaper Ad	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Citrus Heights residents and service providers</p>	The city posted a Public Notice in the Sacramento Bee on August 7, 2020, to advertise the public review and comment period and the upcoming public hearing on August 13, 2020, to review the CARES Act Amendment for the second phase of the city's first award of CDBG-CV funding.	The city did not receive any comments.	The city did not reject any comments.	

Sort Order	Mode of Outreach	Target of Outreach	Summary of response/attendance	Summary of comments received	Summary of comments not accepted and reasons	URL (If applicable)
13	Public Meeting	<p>Minorities</p> <p>Persons with disabilities</p> <p>Non-targeted/broad community</p> <p>Residents of Public and Assisted Housing</p> <p>Citrus Heights residents and service providers</p>	The city held a virtual Public Hearing on August 13, 2020, to review and receive feedback on the CARES Act Amendment for the second phase of CDBG-CV funding allocations.	The city did not receive any comments.	The city did not reject any comments.	

Table 5– Citizen Participation Outreach

Needs Assessment

NA-05 Overview

Needs Assessment Overview

The Needs Assessment collects and analyzes data on the housing needs for residents of the City of Citrus Heights. The assessment focuses especially on affordable housing, special needs housing, community development, and homelessness. Information is gathered through a community survey, stakeholder consultations, and various online data sources. Through this data collection, the city estimates its housing needs for the next five year plan period. First, the assessment addresses the characteristics of citywide housing including income level, tenure, household type, and housing problems, including cost burden, overcrowding, and substandard housing conditions. Second, the Assessment looks at whether any of the above housing characteristics are concentrated among minority or certain racial groups. Third, the Assessment discusses the number and character of existing public housing units in the city, as well as the demand and number of waitlisted residents. Fourth, the extent and nature of homelessness is discussed. The number and type of available housing and services for the homeless, and the challenges and risk factors for persons and families becoming homeless, are also discussed. Fifth, specific populations that are at risk of becoming homeless are reviewed for their needs, including elderly, frail elderly, persons with disabilities, persons with alcohol or other drug addictions, persons with HIV/AIDS and their families, public housing residents, and victims of domestic and other similar types of violence. Lastly, the Needs Assessment will address the needs for public facilities, public improvements, public services, and other eligible Community Development Block Grant (CDBG) uses.

NA-50 Non-Housing Community Development Needs - 91.415, 91.215 (f)

Describe the jurisdiction's need for Public Facilities:

Citrus Heights contains a large number of nonprofit organizations, most of which have facility needs.

How were these needs determined?

Public facility needs identified over the next five years include the following:

- Neighborhood facilities. During staff's consultation with the Citrus Heights Collaborative, several non-profits expressed the need for more recreational opportunities within Citrus Heights. Although there are a number of parks within the city, many are in need of rehabilitation and safety due to homeless activity in parks was listed as a concern. During staff's consultant with the Sunrise Recreation and Park District, funding was listed as a barrier to rehabilitating local parks and increasing services offered in Citrus Heights.
- Youth center. Similarly, in the city's online survey of various public facility needs, respondents marked a youth center, as the most desired public facility need. Although the city developed a neighborhood youth center on Sayonara Drive, the community expressed a need for more youth facilities within Citrus Heights.
- Emergency food closet. During staff's consultation with the Sunrise Christian Food Ministry, the nonprofit expressed a need for a larger emergency food closet. The group currently operates out of a small building at the Advent Lutheran Church in Citrus Heights. Due to the small size of the building, it is challenging to meet the increasing need for emergency food in the Citrus Heights community. In addition, about 19 percent of community survey respondents noted food banks as one of the most important services for those experiencing homelessness in Citrus Heights.

Describe the jurisdiction's need for Public Improvements:

The city has a continued need for accessibility-related public improvements, including the following:

- Infrastructure improvements. About 62 percent of community survey respondents stated the city's street surfaces need improvement.
- ADA-accessible curb ramps. Curb ramps allow disabled residents to have a safe path of travel on City sidewalks.
- Sidewalk infill. Sidewalk infill projects install sidewalk, curb, and gutter improvements in areas where there is currently no sidewalk. Generally, the sidewalk connects with existing sidewalk on both ends, or connects residents with a facility or other public improvement (e.g., trail).
- Signalized intersection improvements. Upgrades to signalized intersections to be ADA accessible.

- Other ADA improvements. Occasionally, other types of ADA improvements will be identified by the city's General Services Department.

How were these needs determined?

These needs were determined in coordination with the City's General Services Department and other stakeholder consultations as well as through community survey results.

Describe the jurisdiction's need for Public Services:

Citrus Heights is served by several local and regional nonprofits. Nonprofits provide services ranging from case management to after-school programs to landlord/tenant mediation. Needs identified over the next five years include the following:

- Senior services. Seniors need continued access to meals, both delivered for homebound seniors and Rusch Park, for more mobile seniors. Seniors also need access to transit that provides service close to their homes, medical facilities, shopping, and other services.
- Youth services. Youth in general need access to after-school and extracurricular programs that help them maintain education and gang involvement.
- Tenant/Landlord services. Renters need access to free advisory services when faced with a tenant landlord dispute.
- Homeless Assistance. Many survey respondents and Collaborative members mentioned services to the homeless as a high priority need in the city. Respondents mentioned mental health services are a particularly needed.
- Job training/education. Many low-income and English as a second language individuals need specialized job training and mentoring in order to fill the needs of Citrus Heights' employers.

How were these needs determined?

These needs were determined upon consultation with the Citrus Heights Collaborative, various non-profits and service providers, input from a community workshop, and an online survey for community members and stakeholders.

Housing Market Analysis

MA-05 Overview

Housing Market Analysis Overview:

The City of Citrus Heights' housing stock is primarily composed of single-family homes, with a combination of owned and rental properties. Most single-family homes have at least three bedrooms and there is an adequate number of housing units to accommodate most of the city's residents, as evidenced by the low rate of overcrowding. The City of Citrus Heights is 98 percent built out, so there is little land available for new housing developments. Additionally, Citrus Heights is characterized by aging housing stock, with a majority of housing built before 1979, which can result in increased need for repair and rehabilitation. Therefore, the primary components of the city's housing strategy are to preserve and rehabilitate its existing housing stock through housing repair and rental inspection programs, and to continue to operate its First-Time Homebuyer Program (FTHB). Community engagement efforts expressed similar support for these programs. When asked to prioritize affordable housing needs, residents selected the following four preferences: emergency repair (21 percent), acquisition/rehab multi-family (21 percent), accessibility repairs grant (13 percent), and rehabilitation of existing single-family homes (13 percent). When surveyed, 18 percent of renters said they believed their home had a code violation, and all but one said they had reported it already to code enforcement. When why they had not reported it, 40 percent said they were afraid the owner would retaliate in some way. Moreover, when ask what are the top three housing program for the city to focus on, they said: homeownership - affordable single-family homes; rentals: affordable rental units, new construction of single-family, and home improvements for low-income homeowners. The city has adequate housing for moderate and above-moderate income households, but many lower-income households struggle with different challenges as a result of economic factors. The city offers a mix of conventional housing units and assisted housing units both public-housing complexes and privately-owned. No affordable housing units are at risk of being converted to market rate rents during the upcoming plan period. Nonetheless, the city will continue to pursue additional affordable housing. The city will also continue to seek and develop opportunities for redevelopment, foster and develop an educated workforce through job training services, and expand jobs in the local community. This is especially important because most residents commute to outside of Citrus Heights to work.

Community Feedback

The city gathered community feedback on housing affordability issues as well. When surveyed about their satisfaction with housing, renters said they were most unsatisfied with frequency of rent increases and affordability of rent. When asked about prioritizing city programs, residents ranked housing issues as the top two most important activities: 1) Expand homeless housing and services, 2) foster affordable housing, 3) offer a variety of public services, 4) construction/upgrade public facilities, and 5) improve

accessibility. When asked what groups have the greatest need for affordable housing, respondents prioritized these three groups: seniors, disabled persons, and homeless persons. All of these groups typically need financial assistance with repairs and repairs to their home.

The city has made efforts to 1) take the lead in the design, construction, and funding of public improvements; 2) improve the appearance of commercial districts; and 3) stimulate private investment. These efforts have included streetscape enhancement projects and a campaign to revitalize the Auburn Boulevard Specific Plan Area. The campaign included assisting business owners with improving their customer base, one-on-one consulting, and a variety of grants and fee waivers to assist the property and business owners along the Auburn Boulevard corridor.

MA-45 Non-Housing Community Development Assets - 91.410, 91.210(f)

Introduction

The city is interested in bringing more jobs and in addition, high-quality jobs to the city. The 2011 Citrus Heights Economic Development Strategy Report identified that the city's aging stock of retail space, particularly space in small unanchored strip retail centers, was one of its key challenges. Recently the city has been in the process of a Specific Plan amendment to redevelop the Sunrise Mall. The city also has been improving its Comprehensive Transit Plan to improve service operations for the commuting public.

Economic Development Market Analysis

Business Activity

Business by Sector	Number of Workers	Number of Jobs	Share of Workers %	Share of Jobs %	Jobs less workers %
Agriculture, Mining, Oil & Gas Extraction	334	0	1	0	-1
Arts, Entertainment, Accommodations	4,091	3,398	15	22	7
Construction	2,391	711	9	5	-4
Education and Health Care Services	5,614	3,130	20	21	1
Finance, Insurance, and Real Estate	2,275	1,045	8	7	-1
Information	596	151	2	1	-1
Manufacturing	1,536	38	5	0	-5
Other Services	1,248	560	4	4	0
Professional, Scientific, Management Services	2,835	1,117	10	7	-3
Public Administration	0	0	0	0	0
Retail Trade	4,951	4,886	18	32	14
Transportation and Warehousing	855	58	3	0	-3
Wholesale Trade	1,359	145	5	1	-4
Total	28,085	15,239	--	--	--

Table 6 - Business Activity

Data Source: 2011-2015 ACS (Workers), 2015 Longitudinal Employer-Household Dynamics (Jobs)

Labor Force

Total Population in the Civilian Labor Force	44,390
Civilian Employed Population 16 years and over	38,850
Unemployment Rate	12.44
Unemployment Rate for Ages 16-24	34.23
Unemployment Rate for Ages 25-65	8.42

Table 7 - Labor Force

Data Source: 2011-2015 ACS

Occupations by Sector		Number of People
Management, business and financial	7,900	
Farming, fisheries and forestry occupations	1,830	
Service	4,620	
Sales and office	11,515	
Construction, extraction, maintenance and repair	3,545	
Production, transportation and material moving	2,150	

Table 8 – Occupations by Sector

Data Source: 2011-2015 ACS

Travel Time

Travel Time	Number	Percentage
< 30 Minutes	23,750	65%

Travel Time	Number	Percentage
30-59 Minutes	10,640	29%
60 or More Minutes	2,285	6%
Total	36,675	100%

Table 9 - Travel Time

Data Source: 2011-2015 ACS

Education:

Educational Attainment by Employment Status (Population 16 and Older)

Educational Attainment	In Labor Force		Not in Labor Force
	Civilian Employed	Unemployed	
Less than high school graduate	2,505	535	1,470
High school graduate (includes equivalency)	7,730	1,030	3,360
Some college or Associate's degree	14,250	1,785	4,750
Bachelor's degree or higher	6,570	510	1,230

Table 10 - Educational Attainment by Employment Status

Data Source: 2011-2015 ACS

Educational Attainment by Age

	Age				
	18-24 yrs	25-34 yrs	35-44 yrs	45-65 yrs	65+ yrs
Less than 9th grade	65	500	460	585	645
9th to 12th grade, no diploma	950	875	540	1,545	755
High school graduate, GED, or alternative	2,970	3,405	2,480	6,255	3,800
Some college, no degree	3,655	4,455	3,465	7,225	3,545
Associate's degree	405	1,725	1,330	2,655	1,285
Bachelor's degree	480	1,860	1,315	2,950	1,690

	Age				
	18–24 yrs	25–34 yrs	35–44 yrs	45–65 yrs	65+ yrs
Graduate or professional degree	0	400	475	1,325	1,000

Table 11 - Educational Attainment by Age

Data Source: 2011-2015 ACS

Educational Attainment – Median Earnings in the Past 12 Months

Educational Attainment	Median Earnings in the Past 12 Months
Less than high school graduate	21,000
High school graduate (includes equivalency)	27,080
Some college or Associate's degree	34,456
Bachelor's degree	46,590
Graduate or professional degree	55,596

Table 12 – Median Earnings in the Past 12 Months

Data Source: 2011-2015 ACS

Based on the Business Activity table above, what are the major employment sectors within your jurisdiction?

A majority of Citrus Heights residents work in the education and health care services sector, followed by retail trade, and arts, entertainment, and accommodations sectors. A majority of Citrus Heights residents in highly skilled professions work outside Citrus Heights.

Describe the workforce and infrastructure needs of the business community:

In the community engagement survey, the types of businesses that were ranked as needed most were entertainment venues, locally owned businesses, and performing and visual arts.

In 2015, per the 2011–2015 ACS 5-Year estimates, there were 5,797 companies in Citrus Heights. The mean travel time to work was 25 minutes in 2015. A large portion of the population commutes outside of the city for work. According to Trulia, in 2017, about 29 percent of the population leaves the city during the day to commute to jobs outside of the city while 17.4 percent of workers live and work in the City of Citrus Heights. Increasing the availability of jobs in the city is one of the needs for the community.

The local business community expressed the need for an educated workforce during the annual city and Chamber of Commerce 2014 Business Walk. Infrastructure needs are typically related to maintaining street improvement and maintenance and technology needs, including sufficient data capacity infrastructure, reliable internet access, etc.

Describe any major changes that may have an economic impact, such as planned local or regional public or private sector investments or initiatives that have affected or may affect job and business growth opportunities during the planning period. Describe any needs for workforce development, business support or infrastructure these changes may create.

There are several major redevelopment projects proposed for the City of Citrus Heights over the next five-year period. There will likely be new retail and office-related jobs generated as part of this redevelopment/new development. Workforce development is likely to be met through the existing employment training and placement services including organizations like (Crossroads, Asian Resources).

For example, the city will be revamping Sunrise Mall. The plan would likely make way for a mixed-use property. The current goal is to transform Sunrise Mall into an area where residents and visitors shop, work, live, and play. Sunrise Mall covers 100 acres of prime property in Citrus Heights. The city is working on a General Plan amendment that recognizes the importance of Sunrise Mall in the region and which would require the development of a specific plan to ensure the future redevelopment of Sunrise Mall is conducted in a comprehensive manner. These future changes are anticipated to have a positive impact on the community by diversifying the economy with more businesses and jobs.

The city also has been implementing its Comprehensive Transit Plan, which was adopted by City Council in October 2018, to improve service operations for the commuting public. The operating enhancements will address how to improve transit travel times through actions such as transit signal priority, peak hour bus-only lanes, and queue jump lanes.

How do the skills and education of the current workforce correspond to employment opportunities in the jurisdiction?

The majority of persons in each age group (except 65+) has completed some college, but did not obtain a degree. Currently, the retail trade sector represents the second largest business sector by number of workers (4,951 workers) and the largest business sector by share of jobs (30 percent). While college degree (associate and above) is not necessary for some retail positions, several respondents at the Citrus Heights Collaborative meeting and on the online survey expressed the need for job training services. One of the main goals of the city's Community and Economic Development Department is to attract highly-skilled workers and businesses to expand its businesses offerings beyond its strong retail base.

Describe any current workforce training initiatives, including those supported by Workforce Investment Boards, community colleges and other organizations. Describe how these efforts will support the jurisdiction's Consolidated Plan.

The city partners with Crossroads Diversified Services, when possible; Crossroads, a nonprofit which receives SETA designated funding for the Sacramento County, provides resources and services to employers and job seekers in Sacramento County. In addition, the city has recently provided CDBG funding to Crossroads Diversified Services for the Youth and Employment Readiness Program, which offers employment readiness and soft skill development workshops to high school students.

Asian Resources provides a variety of comprehensive workforce training programs and resources for Citrus Heights youth, adults, and recent immigrants and refugees. Training programs include job placement opportunities, English classes, and technical skills training, such as computer skills and financial literacy.

The San Juan Unified School District offers workforce training through the Career and Technical Education program, which provides pathway programs to enter a number of local sectors, including health, manufacturing, construction trades, hospitality, and information technology.

Finally, the Sacramento County libraries, including Sylvan Oaks Library in the city, offer career counseling, skills training, and free amenities, such as WiFi and computers, to support residents entering the workforce or searching for a job.

Overall, these community organizations and their programs support this Consolidated Plan through expanding economic opportunities for youth and low-income residents.

Does your jurisdiction participate in a Comprehensive Economic Development Strategy (CEDS)?

If so, what economic development initiatives are you undertaking that may be coordinated with the Consolidated Plan? If not, describe other local/regional plans or initiatives that impact economic growth.

No, the city does not currently participate in the CEDS.

Discussion

The city is actively engaging in efforts to increase local jobs, so that residents have the option of working close to home in highly skilled jobs. There may be opportunities for new businesses to work with local service providers to encourage job training related to meet their needs.

The city makes efforts to strengthen the retail base to ensure its own fiscal stability, provide needed goods and services, and promote the vitality of its commercial districts and nodes. In coordination with the General Plan, the city made efforts to take the lead in the design, construction, and funding of public improvements, including streetscape enhancements, to improve the appearance of commercial districts and stimulate private investment.

For example, the city has completed several streetscape projects, including Sunrise Boulevard and portions of Auburn Boulevard. The city also recently launched a campaign to revitalize the Auburn Boulevard Specific Plan Area. The campaign includes assisting business owners in improving their customer base, one-on-one consulting, and a variety of grants and fee waivers to assist the property and business owners along the Auburn Boulevard corridor. This campaign is about halfway completed and is now currently in the second phase.

The city continually seeks ways to improve transportation services for the commuting public, which will make it easier to get to and from work. The city is also in the process of a General Plan amendment regarding redevelopment of Sunrise Mall. It would require the development of a specific plan to ensure that the future redevelopment of Sunrise Mall is conducted in a comprehensive manner. These future changes are anticipated to have a positive impact on the community by diversifying the economy and by providing more access to businesses and jobs.

MA-50 Needs and Market Analysis Discussion

Are there areas where households with multiple housing problems are concentrated? (include a definition of "concentration")

Historically, the city has focused many resources on Sayonara Drive, which is the street in the city most subject to crime, calls for service, code enforcement issues, and building deficiencies. The city purchased and demolished 15 complexes and 1 eight-plex on the street with the intent of replacing them with a new affordable housing development. The city is currently in the pre-planning phase of this project.

The city will pursue housing resources consistent with the priorities outlined in the city's Housing Element and Consolidated Plan, including pursuing funds for the city's Housing Trust Fund, FTHB, the Sayonara neighborhood, other pockets of low-income neighborhoods, and the city's ten mobile home communities.

In general, there is not a large concentration of multiple housing problems in other areas of the city. The city recently started rental housing inspection program, which will also assist with identifying any building deficiencies in the city.

Are there any areas in the jurisdiction where racial or ethnic minorities or low-income families are concentrated? (include a definition of "concentration")

Racial and ethnic minorities and low-income families are not concentrated to an extent in neighborhoods that would require a specific targeting of CDBG funds. HUD has developed a census tract-based definition of R/ECAPs. The definition involves a racial/ethnic concentration threshold and a poverty test. The racial/ethnic concentration threshold is: R/ECAPs must have a non-white population of 50 percent or more. Regarding the poverty threshold, neighborhoods of extreme poverty must be within census tracts with 40 percent or more of individuals living at or below the poverty line. None of the parcels within the city have that combined level of concentration.

What are the characteristics of the market in these areas/neighborhoods?

As a majority of Citrus Heights housing was constructed prior to 1989, many neighborhoods and areas face similar challenges in terms of housing maintenance.

Are there any community assets in these areas/neighborhoods?

Regarding Sayonara Drive, the Citrus Heights Children and Youth Center is located on Sayonara Drive and was constructed with CDBG funds in the 2010-2014 Consolidated Plan period. The street maintains

easy access to regional transit.

Are there other strategic opportunities in any of these areas?

The city anticipates constructing an affordable housing development project on Sayonara Drive in this Consolidated Plan period.

MA-60 Broadband Needs of Housing occupied by Low- and Moderate-Income Households - 91.210(a)(4), 91.310(a)(2)

Describe the need for broadband wiring and connections for households, including low- and moderate-income households and neighborhoods.

Countywide, 82 percent of households have a desktop or laptop computer and 87 percent have a smartphone - higher than the US overall. Seventy-six percent of households have broadband access by cable, fiber, or DSL and 89 percent have some type of broadband access- 10 percent only have internet through their cellular data plan. American Community Survey (ACS) data indicate that access is much lower for low- and moderate-income households. In Sacramento County, just three percent of households earning \$75,000 or more per year are without any internet subscription compared to 29 percent of households earning less than \$20,000 per year and 12 percent of households earning between \$20,000 and \$75,000 per year. There is Coalition for Digital Inclusion in Sacramento, made up of government entities, for profits, and nonprofits that are concerned with digital equity. The Coalition pulls together resources and data to move the needle on digital inclusion and determine what efforts to focus on. Their goal is for everyone in the region to have the ability to access services (skills), hardware (access to or own), and connection (wiring/infrastructure, broadband speeds). There are three "spheres" of digital equity: 1) Economic: the ability to apply for a job or complete tasks. 2) Civic: for example, fill out the Census, apply for services, access healthcare. Cultural: communicate with family and friends, use propriety applications for entertainment. The attached map is from a market assessment of the Sacramento Region and shows how broadband access varies by neighborhood.

Describe the need for increased competition by having more than one broadband Internet service provider serve the jurisdiction.

According to the Federal Communications Commission database, Sacramento is served by more than five broadband providers.

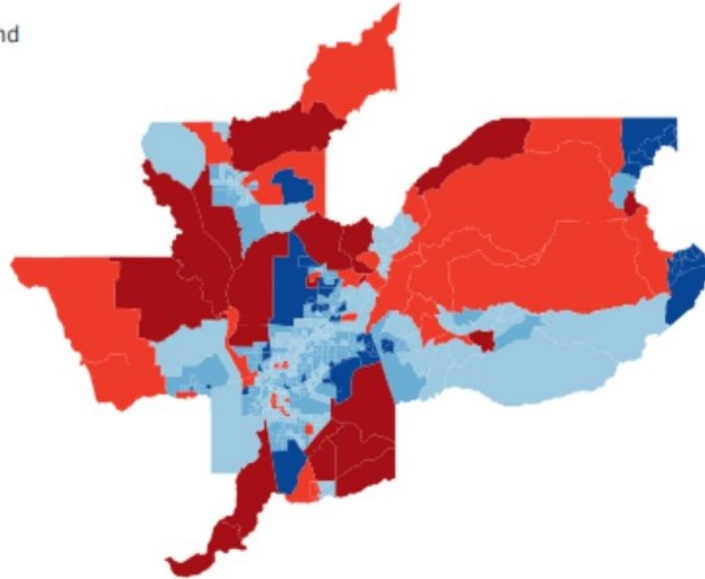
FIGURE 29

Broadband subscription rates differ considerably in the Sacramento region

Neighborhood broadband subscription rates by census tract, 2015

Neighborhood broadband
subscription rates

- 0-20%
- 20-40%
- 40-60%
- 60-80%
- 80-100%
- NA



Source: "Signs of Digital Distress: Mapping Broadband Availability and Subscription in American Neighborhoods," Brookings, 2017

Broadband Subscription Rates: Sacramento County

MA-65 Hazard Mitigation - 91.210(a)(5), 91.310(a)(3)

Describe the jurisdiction's increased natural hazard risks associated with climate change.

The Citrus Heights Emergency Operations Plan (EOP) is the city's all hazards plan that establishes an Emergency Management Organization and assigns functions and tasks consistent with California's Standard Emergency Management System (SEMS) and the National Incident Management System (NIMS). It provides for the integration and coordination of planning efforts of multiple departments within Citrus Heights, multijurisdictional special districts, Sacramento County and the State of California. It addresses actions for all disasters as well as additional actions appropriate for specific hazards.

Citrus Heights depends on the federal government, State of California, California Master Mutual Aid System and other formal and informal agreements for outside disaster and emergency response assistance. The level and sources of outside assistance depend on the nature and scope of the incident and the availability of specific resources. When a local state of emergency has been declared and the Citrus Heights Emergency Operations Center (EOC) is activated, all requests for outside resources will go through the City EOC to the Sacramento OA EOC .

Citrus Heights is vulnerable to the following natural hazards that can be associated with climate change. All these hazards have the potential to cause impacts ranging from slight to catastrophic, severely disrupting city operations and resulting in extensive damage, fatalities and injuries:

- Severe Weather
- Flood
- Earthquake
- Volcano
- Fire

Any of the hazards above impacting Sacramento County and Southern Placer County have the potential to affect Citrus Heights. Those hazards that do not directly affect Citrus Heights have the potential impact of producing evacuees from the impacted regions flowing into or passing through Citrus Heights. This could cause overloading of major streets, a requirement to open and maintain shelters, and overburdened traffic control.

Describe the vulnerability to these risks of housing occupied by low- and moderate-income households based on an analysis of data, findings, and methods.

Natural hazards risks and problems include reaching people who are homeless before or during a disaster, especially those who are camping out or who may not have a phone that receives emergency alerts. In addition, seniors with mobility issues may not be able to evacuate without assistance. For

homes in the flood plain, insurance can be a large problem and many residents don't have this type of insurance because of unaffordable premiums. Another issue is that some residents bought homes without knowing they were in a flood plain, and therefore they didn't know they needed to carry flood insurance. The biggest issue that low-income residents experience after a disaster is shelter. They often don't have the means to stay in a hotel, so low-income residents are most likely to be stuck in a shelter.

Strategic Plan

SP-05 Overview

Strategic Plan Overview

Citrus Heights will use a need-based strategy, as opposed to a place-based strategy, over the course of the planning period.

At the community workshop in September 2019, when the public was asked what the top two priorities were that the city should prioritize, the following priorities were listed as follows: 29 percent of respondents chose expand homeless housing and services 26 percent chose foster affordable housing; 19 percent chose offer a variety of public services, 18 percent chose construct/ upgrade facilities, and 8 percent chose improve accessibility.

When the public was asked what were the top priority populations for the city to provide assistance, the following groups were selected: extremely low income, families with children and elderly, families with children, mentally ill homeless, homeless veterans, chronically homeless, persons with mental disabilities, elderly, frail elderly, persons with developmental disabilities, victims of domestic violence.

Based on the needs assessment, market analysis, and consultations with the community and stakeholders, the following are the selected key community needs for the Consolidated Plan:

- Build healthy communities;
- Improve public infrastructure;
- Enhance accessibility;
- Improve public services for priority populations;
- Improve housing access and affordability; and
- Expand economic opportunities.

These priorities align closely with three of the city Council's five goals: diversify for a changing economy; improve community vibrancy and engagement; and improve streets and infrastructure.

Identifying Goals

Within those priorities, the city identified eight main goals:

- Foster affordable housing
- Provide services for people experiencing homelessness
- Provide services for seniors and youth
- Provide additional public services responsive to current public needs

- Improve accessibility
- Construct/upgrade public facilities
- Effectively administer CDBG program to benefit the Citrus Heights community
- Affirmatively further fair housing

Addressing Goals

To address these goals, the city is in the pre-planning process for future construction of new affordable housing development on Sayonara Drive. The city also plans to continue to provide loans to homeowners needing health-related and safety repairs, support nonprofits offering a wide range of social services and support services to the homeless populations, and assist with accessibility and new facility projects.

To help address homelessness, the city has funded Sacramento Self-Help Housing (Housing Counseling/Navigator) to provide housing counseling services to households at-risk of becoming homeless and navigator services to households that are currently homeless.

Other types of projects may be considered as needed throughout the Consolidated Plan period. The city anticipates funding activities using a variety of sources, including CDBG, HOME, General Fund, and grants received by the city. The city will work with local and regional nonprofits, as well as affordable housing developers, to implement many of the activities.

Funded programs that provide services for youth and seniors include the following: Campus Life Connection which operates an after-school center that provides low-income youth with recreational activities, activities include mentoring, tutoring, educational games, technology center, and daily nutritious meals; Crossroads Diversified Services which runs a Youth and Employment Readiness Program (YERP) that includes employment readiness and soft skill development workshops to high school students; Terra Nova Counseling which operates a Juvenile Diversion and Education Program (JDEP) that provides individual and family therapy to decrease crime recidivism and improve quality of life for Citrus Heights youth; and Meals on Wheels which provides balanced, nutritious meals throughout the week, 250 days per year to seniors.

The city will also undertake public improvements using internal staff and contractors. For example, the city recently is in process of and has completed projects relating to the rehabilitation of areas, improving public services, and improving accessibility. Sunrise Boulevard Complete Streets Phase II- the Sunrise Boulevard Rehabilitation and Complete Streets Project (multiple Phases) is a comprehensive and realistic Complete Streets project that will rehabilitate and reconstruct a regionally significant arterial, adding much needed Complete Streets element throughout the project area. In September, Phase 2A of the revitalization project was completed. The phase saw the installation of new storm drain system, new

curb, gutter and sidewalk, and new streetlights. In addition, restriping of the boulevard decreased travel lane widths and placed dedicated bike lanes.

One project that helps improve living conditions and affirmatively further fair housing is the Rental Housing Inspection Program. In October 2018, the City Council approved moving forward with a Rental Housing Inspection Program. The program promotes compliance with health and safety standards to reduce the number of substandard rental housing conditions. In addition, the Sacramento Self-Help Housing (Renters Helpline) provides a telephone and internet-based “Renters Helpline” as well as counseling, dispute resolution, and fair housing services.

SP-10 Geographic Priorities - 91.415, 91.215(a)(1)

Geographic Area

Table 13 - Geographic Priority Areas

1	Area Name:	CDBG Target Area
	Area Type:	Local Target area
	Other Target Area Description:	
	HUD Approval Date:	
	% of Low/ Mod:	
	Revital Type:	Other
	Other Revital Description:	Accessibility Improvements
	Identify the neighborhood boundaries for this target area.	This target area includes all census blocks with 55% or more low- and moderate-income households, based on CPD maps information.
	Include specific housing and commercial characteristics of this target area.	This target area includes all low- and moderate-income census blocks. Citrus Heights is a primarily residential area. However, there are some commercial areas, generally located along the main arterial corridors.
	How did your consultation and citizen participation process help you to identify this neighborhood as a target area?	There is a general need for assistance in the low- and moderate-income neighborhoods. Therefore, the low- and moderate-income census blocks were selected as the CDBG target area.
	Identify the needs in this target area.	The CDBG target area is need of infrastructure improvements, accesibility improvements, housing preservation activities, park improvements, and public services.
	What are the opportunities for improvement in this target area?	There is an opportunity to partner with the General Services Department and Sunrise Recreation and Park District to leverage CDBG funding with other funding sources.
	Are there barriers to improvement in this target area?	The amount of funding available is the main barrier for improvements within the city's low- and moderate-income census blocks. The city continues to make progress with our annual efforts.
	Area Name:	City-wide

2	Area Type:	
	Other Target Area Description:	
	HUD Approval Date:	
	% of Low/ Mod:	
	Revital Type:	Other
	Other Revital Description:	Accessibility
	Identify the neighborhood boundaries for this target area.	
	Include specific housing and commercial characteristics of this target area.	
	How did your consultation and citizen participation process help you to identify this neighborhood as a target area?	
	Identify the needs in this target area.	
	What are the opportunities for improvement in this target area?	
	Are there barriers to improvement in this target area?	

General Allocation Priorities

Describe the basis for allocating investments geographically within the state

Citrus Heights will use a need-based strategy, as opposed to a place-based strategy, over the course of the planning period. There are no geographic priorities, except for the low- and moderate-income census tracts within Citrus Heights. The city prioritizes funding primarily within the low-income neighborhoods (at least 51% of low- and moderate-income neighborhoods). The majority of the activities the city plans to conduct will be offered to eligible households citywide. For example, the Citywide Accessibility Project funded in 2019 is focused on the low- and moderate-income census tracts. These locations were selected in coordination with the city's General Services Department based on resident input and timing with other planned projects in order to leverage non-CDBG funds.

The city plans to continue to identify the need for proposed projects on an annual basis through the collection of data (quantitative and qualitative) during the application process. The city also plans to consult service providers in determining the level of need for proposed activities.

The summary analysis of the result of the community engagement meeting that took place in September 2019, resulted in the following priorities: 1) expand homeless housing and services, 2) foster

affordable housing, 3) offer a variety of public services, 4) construction/upgrade public facilities, and 5) improve accessibility. Given the difficulties that already exist in implementing these types of projects, it was decided to not limit services to certain geographic areas in the city. Most of the services available in the city are open to all residents.

However, during the community workshop, the public in attendance was asked to share their thoughts about particular geographic areas within the city that should be prioritized for future CDBG funding. They placed pins on a map of the city, broken into different census block groups. Census block 8139 was designated as a top priority. If given the opportunity in the future, this priority area will be taken into account for during the priority needs assessment process.

SP-25 Priority Needs - 91.415, 91.215(a)(2)

Priority Needs

Table 14 – Priority Needs Summary

1	Priority Need Name	Building Healthy Communities
	Priority Level	High
	Population	Extremely Low Low Large Families Families with Children Elderly Chronic Homelessness Individuals Mentally Ill Chronic Substance Abuse veterans Victims of Domestic Violence Unaccompanied Youth Frail Elderly Persons with Mental Disabilities Persons with Physical Disabilities Persons with Developmental Disabilities Victims of Domestic Violence Non-housing Community Development
	Geographic Areas Affected	CDBG Target Area
	Associated Goals	Foster affordable housing Effectively administer CDBG program Affirmatively further fair housing

	Description	Through this goal the city plans to: <ul style="list-style-type: none"> • Foster affordable housing • Provide services for people experiencing homelessness • Provide services for seniors and youth • Provide additional public services responsive to current public needs • Improve accessibility • Construct/upgrade public facilities • Effectively administer the CDBG program to benefit the Citrus Heights community • Affirmatively further fair housing
	Basis for Relative Priority	When surveyed, Citrus Heights residents expressed a strong desire to improve the health and prosperity of the communities and neighborhoods that they live in.
2	Priority Need Name	Improve Public Infrastructure
	Priority Level	High
	Population	Extremely Low Low Moderate Large Families Families with Children Elderly Chronic Homelessness Individuals Frail Elderly Persons with Physical Disabilities Persons with Developmental Disabilities Non-housing Community Development
	Geographic Areas Affected	CDBG Target Area
	Associated Goals	Improve accessibility Construct/upgrade public facilities
	Description	Through this goal the city plans to: <ul style="list-style-type: none"> • Construct/upgrade public facilities

	Basis for Relative Priority	Survey results indicated that the most important infrastructure projects for the city are street surface repairs, reducing utility rates, and energy programs. Additionally, survey respondents specifically identified the following public facilities improvements as the most important for the city to support homeless facilities 21 percent; youth centers 17 percent, senior center 12 percent, parks and rec facilities 8 percent, street and sidewalk infill 8 percent, and 7 percent health facilities.
3	Priority Need Name	Enhance Accessibility
	Priority Level	High
	Population	Extremely Low Low Moderate Large Families Families with Children Elderly Chronic Homelessness Individuals Families with Children Mentally Ill Chronic Substance Abuse veterans Persons with HIV/AIDS Victims of Domestic Violence Unaccompanied Youth Frail Elderly Persons with Mental Disabilities Persons with Physical Disabilities Persons with Developmental Disabilities Persons with Alcohol or Other Addictions Persons with HIV/AIDS and their Families Victims of Domestic Violence Non-housing Community Development
	Geographic Areas Affected	CDBG Target Area
	Associated Goals	Improve accessibility Construct/upgrade public facilities

	Description	Through this goal the city plans to: <ul style="list-style-type: none"> • Improve accessibility
	Basis for Relative Priority	Outreach with residents identified the need for persons with disabilities, seniors, and low-income families to have better access to transportation as well as to facilities through ADA-compliant and other type improvements.
	4	
	Priority Need Name	Improve Public Services for Priority Populations
	Priority Level	High
	Population	Extremely Low Low Moderate Large Families Families with Children Elderly Chronic Homelessness Individuals Families with Children Mentally Ill Chronic Substance Abuse veterans Persons with HIV/AIDS Victims of Domestic Violence Unaccompanied Youth Frail Elderly Persons with Mental Disabilities Persons with Physical Disabilities Persons with Developmental Disabilities Persons with Alcohol or Other Addictions Persons with HIV/AIDS and their Families Victims of Domestic Violence Non-housing Community Development
	Geographic Areas Affected	CDBG Target Area

	Associated Goals	<p>Services for people experiencing homelessness</p> <p>Services for seniors and youth</p> <p>Provide public services responsive to current need</p> <p>Improve accessibility</p> <p>Effectively administer CDBG program</p>
	Description	<p>Through this goal the city plans to:</p> <ul style="list-style-type: none"> • Provide services for people experiencing homelessness • Provide services for seniors and youth • Provide additional public services responsive to current public needs • Improve accessibility • Effectively administer the CDBG program to benefit the Citrus Heights community
	Basis for Relative Priority	<p>Community Engagement meeting identified the need to improve public spaces including accessibility for disabled populations.</p> <p>Additionally, surveyed residents said that children's and youth highest priority needs as after-school programs and affordable child-care. Services for senior populations were also highly prioritized as needing social activities, general financial assistance, and affordable transportation.</p> <p>Consultations with service providers suggested that access to affordable housing and transportation would help these populations as well.</p>
5	Priority Need Name	Improve Housing Access and Affordability
	Priority Level	High

	Population	Extremely Low Low Moderate Large Families Families with Children Elderly Chronic Homelessness Individuals Families with Children Mentally Ill Chronic Substance Abuse veterans Persons with HIV/AIDS Victims of Domestic Violence Unaccompanied Youth Frail Elderly Persons with Mental Disabilities Persons with Physical Disabilities Persons with Developmental Disabilities Persons with Alcohol or Other Addictions Persons with HIV/AIDS and their Families Victims of Domestic Violence
	Geographic Areas Affected	CDBG Target Area
	Associated Goals	Foster affordable housing Improve accessibility Affirmatively further fair housing
	Description	Through this goal the city plans to: <ul style="list-style-type: none"> • Foster affordable housing • Improve accessibility • Affirmatively further fair housing

	Basis for Relative Priority	<p>Surveyed residents selected the following housing program priorities: homeownership for affordable single-family homes (64 percent), and 48 percent of those surveyed indicated the priority for affordable rental units. Additionally, almost half of the respondents are cost burdened with 38 percent paying between 30 and 50 percent of their monthly income towards rent/ mortgage, and another 11 percent paying more than 50 percent towards their rent/ mortgage.</p> <p>Affordable housing was also the top priority in the survey for seniors.</p>
6	Priority Need Name	Expand Economic Opportunities
	Priority Level	High
	Population	<p>Extremely Low</p> <p>Low</p> <p>Moderate</p> <p>Large Families</p> <p>Families with Children</p> <p>Elderly</p> <p>Chronic Homelessness</p> <p>Individuals</p> <p>Families with Children</p> <p>Mentally Ill</p> <p>Chronic Substance Abuse</p> <p>veterans</p> <p>Persons with HIV/AIDS</p> <p>Victims of Domestic Violence</p> <p>Unaccompanied Youth</p> <p>Frail Elderly</p> <p>Persons with Mental Disabilities</p> <p>Persons with Physical Disabilities</p> <p>Persons with Developmental Disabilities</p> <p>Persons with Alcohol or Other Addictions</p> <p>Persons with HIV/AIDS and their Families</p> <p>Victims of Domestic Violence</p>
	Geographic Areas Affected	CDBG Target Area
	Associated Goals	<p>Services for people experiencing homelessness</p> <p>Services for seniors and youth</p> <p>Provide public services responsive to current need</p>

	Description	<p>Through this goal the city plans to:</p> <ul style="list-style-type: none"> • Foster affordable housing • Improve accessibility • Affirmatively further fair housing
	Basis for Relative Priority	<p>Surveyed residents selected teen employment services (49%) as a need for the city's youth. In addition, over 51% of survey respondents listed job opportunities as an important service for persons with disabilities in Citrus Heights. Supportive services, including jobs, for people experiencing homelessness also ranked high (63%) among service respondents. Given the high cost of housing and survey results, expanding economic opportunities is included as a high-priority need for Citrus Heights.</p>

Narrative (Optional)

During the community survey engagement meeting, when asked what are the top two priorities that the city should prioritize, the following priorities were listed: 29 percent of respondents chose expand homeless housing and services, 26 percent chose foster affordable housing, 19 percent chose offer a variety of public services, 18 percent chose construct/ upgrade facilities, and 8 percent chose improve accessibility. Respondents also selected the following priority populations as needing the most assistance: extremely low and low-income individuals; families with children, elderly individuals; persons experiencing homelessness (particularly homeless families with children, mentally ill, veterans, and chronically homeless); persons with mental disabilities, frail elderly, persons with developmental disabilities, and victims of domestic violence.

SP-35 Anticipated Resources - 91.420(b), 91.215(a)(4), 91.220(c)(1,2)

Introduction

The city of Citrus Heights is a CDBG entitlement (EN) jurisdiction. Citrus Heights received a 2020 EN grant amount of \$639,115 and a CDBG-CV Round 1 allocation of \$376,028. The city estimates receiving \$50,000 in program income. To date, we have received \$32,103 in loan repayments and we have \$727,778 in rollover funding from prior years. The expected amount available for the remainder of the Con Plan period assumes the city will receive \$600,000 in entitlement funds annually. Additionally, Citrus Heights is in a regional HOME Consortium, administered by SHRA. The city works closely with SHRA to allocate HOME funds that are available for use in Citrus Heights.

Along with these federal entitlements, Citrus Heights works with SHRA, the County, and developers when possible to utilize Low-Income Housing Tax Credits, the State of California's housing programs, and other available opportunities to support housing development. The County of Sacramento received a No Place Like Home grant for the Sunrise Pointe Apartments in Citrus Heights. This project has been a regional collaboration with various funding sources.

On March 27, 2020, Coronavirus Aid, Relief, and Economic Security Act (CARES), Public Law 116-136 was signed by the President of the United States. Funding allocations are in table below.

The Anticipated Resources Matrix outlines each of these funds, expected amounts available in Year 1 and a projection of resources between FY 2021 and FY 2024, as well as a list of eligible uses of funds per HUD regulations. The amounts include funds subject to administrative caps, which will not be spent on programming identified in the Goals section of SP-45. These include city staff salary and fringe benefits, as well as other planning and administrative activities.

Other resources that may be employed include funds provided under other HUD programs, grants from the Federal Emergency Management Agency, State of California Department of Housing and Community Development, federal tax credits and mortgage credit certificates, City of Citrus Heights General Funds, and other federal or state grant programs.

One of the main obstacles to meeting community needs is inadequate resources for programs that could address these needs. During the period of time leading up to this Consolidated Plan, the State of California and local governments experienced a dearth of resources to finance affordable housing and affordable housing production has lagged as a result.

For the period of this current Consolidated Plan, the State of California has dedicated additional resources and we expect that the production volume will increase. This will be made possible by a \$4 billion dollar statewide affordable housing bond approved by the voters in November of 2018 and a new, ongoing source of affordable housing approved by the legislature in 2017 (Senate Bill 2). The State has significantly increased the amount of resources available to support housing for homeless and mentally ill; however, most of those resources are allocated to the County and Continuum of Care agencies and the city does not receive any funding directly.

Most of the new resources require affordable housing developers to compete with one another for a limited amount of funding. The city will continue to work with developers and regional partners to demonstrate local support and financial leverage to place developments in the best position to obtain these limited dollars.

Anticipated Resources

Program	Source of Funds	Uses of Funds	Expected Amount Available Year 1				Expected Amount Available Remainder of ConPlan \$	Narrative Description
			Annual Allocation: \$	Program Income: \$	Prior Year Resources: \$	Total: \$		
CDBG	public - federal	Acquisition Admin and Planning Economic Development Housing Public Improvements Public Services	639,115	50,000	727,778	1,416,893	2,600,000	The city is a CDBG entitlement jurisdiction. This includes a 2020 grant amount of \$639,213 and a CDBG-CV Phase I allocation of \$376,028. To date, we have received \$32,103 in loan repayments and we have \$727,778 in rollover funding from prior years. The expected amount assumes the city will receive \$600,000 in entitlement funds annually.
General Fund	public - local	Public Services	134,100	0	0	134,100	536,000	The city allocates General fund money to nonprofits to benefit public services.

Program	Source of Funds	Uses of Funds	Expected Amount Available Year 1				Expected Amount Available Remainder of ConPlan \$	Narrative Description
			Annual Allocation: \$	Program Income: \$	Prior Year Resources: \$	Total: \$		
Other	public - federal	Acquisition Housing	2,280,000	0	0	2,280,000	0	The city uses HOME funds as part of the HOME Consortium. On January 23, 2020, the Citrus Heights City Council approved an allocation of \$2,280,000 of the HOME funds reserved for the City of Citrus Heights to be used for the Sunrise Pointe Apartments. The 46-unit permanent supportive housing project has been approved and is waiting for federal tax credits to proceed with construction. SHRA approved to advance the city its future allocation of HOME funds in order to provide a competitive local match to the project and increase the chances it receives federal tax credits.
Other	public - federal	Admin and Planning Public Services	920,226	0	0	920,226	0	The city received a CDBG-CV 1 award of \$376,028 to prepare for and respond to the impacts of COVID-19.
Other	public - state	Housing	50,000	0	0	50,000	200,000	The city receives program income from prior years Calhome grants.

Table 15 - Anticipated Resources

Explain how federal funds will leverage those additional resources (private, state and local funds), including a description of how matching requirements will be satisfied

There is no federal requirement for the city to match CDBG funds with other non-federal program resources. In Fiscal Year 2019-20, the city allocated \$130,000 in General Fund revenue to nonprofits providing a community service in Citrus Heights, in addition to the CDBG investments. All capital improvement projects funded with CDBG have been able to take advantage of other federal and state funding sources, including Measure A funds and Drainage funds, to complete design, engineering, and construction work that exceeds the city's CDBG allocation for these projects. With respect to public services, the city requires all subrecipients to identify other resources that will be utilized during the program year to operate and implement CDBG-supportive activities. It is the city's intent to ensure that adequate non-federal and private funds are available, thus minimizing the dependence on federal funds.

If appropriate, describe publically owned land or property located within the state that may be used to address the needs identified in the plan

Some projects that may help address the needs identified in the plan include the Sayonara affordable housing project, which is currently in the pre-planning phase. Previously, the city purchased and demolished 15 complexes and 1 eight-plex on the street with the intent of replacing them with a new affordable housing development. In that same neighborhood, on Sayonara Drive, the Citrus Heights Children and Youth Center was recently funded completed with CDBG funds in the 2010-2014 Consolidated Plan period.

Citrus Heights currently contains 90 units of public housing, which is generally in good condition. SHRA, which oversees public housing, has policies in place to inspect units, perform regular maintenance, and offer support to households attempting to achieve self-sufficiency. This helps provide affordable housing to the low-income populations in the community.

The city has made efforts to 1) take the lead in the design, construction, and funding of public improvements; 2) improve the appearance of commercial districts; and 3) stimulate private investment. These efforts have included streetscape enhancement projects and a campaign to revitalize the Auburn Boulevard Specific Plan Area. The campaign included assisting business owners with improving their customer base, one-on-one consulting, and a variety of grants and fee waivers to assist the property and business owners along the Auburn Boulevard corridor.

Discussion

Affordable housing developers and the city must be as creative as possible to find other sources of funding from state programs targeted to low- and moderate-income households, other non-HUD federal sources, private loan funds, federal and state tax-credits, and local funding. In order to win tax credits, multiple sources of funding are required and the minimum matching requirements are typically far exceeded in each project.

These resources include funds provided under other HUD programs, the Economic Development Administration, the Federal Emergency Management Agency, federal, state and local energy efficiency programs, federal and State tax credits and mortgage credit certificates, and other federal grant programs as may be identified.

SP-40 Institutional Delivery Structure - 91.415, 91.215(k)

Explain the institutional structure through which the jurisdiction will carry out its consolidated plan including private industry, non-profit organizations, and public institutions.

Responsible Entity	Responsible Entity Type	Role	Geographic Area Served
City of Citrus Heights	Government	Economic Development Homelessness Non-homeless special needs Ownership Planning Rental neighborhood improvements public facilities public services	Jurisdiction
Sacramento Self Help Housing	Non-profit organizations	Homelessness Ownership Rental public services	Jurisdiction
Sunrise Christian Food Ministry	Non-profit organizations	Homelessness public services	Jurisdiction
SACRAMENTO STEPS FORWARD	Non-profit organizations	Homelessness	Region
Citrus Heights Code Enforcement	Government	neighborhood improvements	Jurisdiction
Citrus Heights Police Department	Government	Homelessness neighborhood improvements public services	Jurisdiction

Table 16 - Institutional Delivery Structure

Assess of Strengths and Gaps in the Institutional Delivery System

The city's Community Development Department is responsible for the administration of the city's community development programs, including some of the local programs that assist target-income residents. The department includes the city's Housing and Grant Division, which has the primary responsibility for management and implementation of the city's affordable housing programs, including the Consolidated Plan and local documents. Other divisions involved in providing services include Planning, Building and Safety, General Services, and Neighborhood Enhancement. Additionally, Citrus

Heights benefits from working closely with several local and regional nonprofits that focus their efforts on target-income households.

The primary gaps in the delivery system relate to the lack of services for target-income households in Citrus Heights. Although some services are located in Citrus Heights, many services are located outside Citrus Heights, usually in Sacramento. The city's City Ride and bus system connects with Sacramento Regional Transit, but many residents have noted that it is not convenient to get to Sacramento on transit due to the cost, number of transfers, and length of trips. The city is working on a Comprehensive Transit Plan, which was adopted by City Council in October 2018, to improve service operations for the commuting public.

Additionally, in 2016, Citrus Heights funded a full-time homeless navigator position with local community support funds for the first time to identify and assist people who are homeless in the city. The navigator proactively seeks to connect with homeless persons who need resources. The navigator is based out of the Sacramento Self-Help Housing (SSHH) non-profit agency will continue to build relationships with community service providers and Citrus Heights Homeless Resource Team (HART) members as well as city and Citrus Heights Police Department staff to assist the homeless population.

The city's Housing and Human Services Program Coordinator has a seat on the Continuum of Care Advisory Board and is working with the CoC to develop a regional gaps analysis. City staff will use the gaps analysis results to identify gaps in the institutional delivery system.

Availability of services targeted to homeless persons and persons with HIV and mainstream services

Homelessness Prevention Services	Available in the Community	Targeted to Homeless	Targeted to People with HIV
Homelessness Prevention Services			
Counseling/Advocacy	X	X	
Legal Assistance	X		
Mortgage Assistance	X		
Rental Assistance	X		
Utilities Assistance	X		
Street Outreach Services			
Law Enforcement	X	X	
Mobile Clinics			
Other Street Outreach Services	X	X	
Supportive Services			
Alcohol & Drug Abuse			
Child Care	X		

Supportive Services			
Education	X		
Employment and Employment Training	X		
Healthcare	X		
HIV/AIDS			
Life Skills	X		
Mental Health Counseling	X		
Transportation	X		
Other			

Table 17 - Homeless Prevention Services Summary

Describe how the service delivery system including, but not limited to, the services listed above meet the needs of homeless persons (particularly chronically homeless individuals and families, families with children, veterans and their families, and unaccompanied youth)

The City of Citrus Heights, in conjunction with surrounding cities in Sacramento County, offers an extensive and encompassing provision of services for individuals who are homeless and/or at risk of being homeless. When analyzing the services offered by Citrus Heights alone, there are several homeless services and organizations dedicated to assisting homeless and at-risk populations in the city.

The primary homeless service providers locally are the Sunrise Christian Food Bank, SSHH, and Sacramento Steps Forward:

- Sunrise Christian Food Ministry provides food bank services to local residents and provides referrals to available services where possible.
- SSHH provides housing counseling services and navigator services to low-income persons and persons who are homeless or at risk of becoming homeless. The city's navigator engages and interacts with individuals located in Citrus Heights. SSHH also maintains a database of affordable housing resources in the Sacramento region. SSHH maintains regular office hours at the One-Stop Shop Career Center.
- The city maintains strong involvement with Sacramento Steps Forward as it manages the Continuum of Care.
- The Citrus Heights Police Department works closely with the homeless navigator. Police officers who are in contact with the homeless population connect with the navigator to conduct outreach along the trails, creeks, and other known homeless camps.
- The City of Citrus Heights is a participant in the regional Homeless Emergency Aid Program (HEAP) efforts. The City Council adopted an emergency shelter crisis declaration in January 2019 and is working with the Continuum of Care to implement and monitor the HEAP-funded programs in Sacramento County.

Describe the strengths and gaps of the service delivery system for special needs population and persons experiencing homelessness, including, but not limited to, the services listed above

Some of the strengths listed below include coordinated services between multiple agencies and government service providers and coordinated intake. For example, city staff participates in HART, a group of agencies, nonprofits, faith-based organizations, members of the business community, and interested individuals dedicated to addressing Citrus Heights' homeless issues. The goal of HART is to provide homeless individuals with resources so they may better access housing options. The navigator also provides referrals and resources.

Also, the city is a participant in the regional HEAP efforts. The City Council adopted an emergency shelter crisis declaration in January 2019 and is working with the Continuum of Care to implement and monitor the HEAP-funded programs in Sacramento County.

Provide a summary of the strategy for overcoming gaps in the institutional structure and service delivery system for carrying out a strategy to address priority needs

The priority needs for homeless populations addressed in the community survey included mental health services and supportive services. While Citrus Heights currently does not have a shelter providing services within the city limits, residents are referred to agencies such as St. John's, Volunteers of America, Sacramento Housing Prevention and Rapid Rehousing Program, Sacramento Area Emergency Housing, and Union Gospel Mission for different shelter options.

The institutional structure in place makes efforts to help connect with and provide services to these populations through a collaborative intake system with the homeless navigator. The homeless navigator proactively seeks to connect with the homeless individuals who have not yet sought help directly.

The homeless navigator builds relationships with the community, service providers, Citrus Heights HART members, and city and Citrus Heights Police Department staff to assist the homeless population. The future tiny homes that will be built by the HEAP program will also help by providing temporary shelter to people experiencing homelessness.

SP-45 Goals - 91.415, 91.215(a)(4)

Goals Summary Information

Sort Order	Goal Name	Start Year	End Year	Category	Geographic Area	Needs Addressed	Funding	Goal Outcome Indicator
1	Foster affordable housing	2020	2024	Affordable Housing	City-wide CDBG Target Area	Building Healthy Communities Improve Housing Access and Affordability	CDBG: \$500,000 Calhome: \$350,000 HOME: \$850,000	Rental units constructed: 35 Household Housing Unit Homeowner Housing Rehabilitated: 8 Household Housing Unit Direct Financial Assistance to Homebuyers: 8 Households Assisted
2	Services for people experiencing homelessness	2020	2024	Homeless	City-wide CDBG Target Area	Improve Public Services for Priority Populations Expand Economic Opportunities	CDBG: \$200,000 General Fund: \$650,000	Public service activities other than Low/Moderate Income Housing Benefit: 400 Persons Assisted Homelessness Prevention: 500 Persons Assisted
3	Services for seniors and youth	2020	2024	Non-Homeless Special Needs	City-wide CDBG Target Area	Improve Public Services for Priority Populations Expand Economic Opportunities	CDBG: \$350,000 General Fund: \$500,000	Public service activities other than Low/Moderate Income Housing Benefit: 2500 Persons Assisted

Sort Order	Goal Name	Start Year	End Year	Category	Geographic Area	Needs Addressed	Funding	Goal Outcome Indicator
4	Provide public services responsive to current need	2020	2024	Non-Homeless Special Needs	City-wide	Improve Public Services for Priority Populations Expand Economic Opportunities	CDBG: \$100,000	Public service activities other than Low/Moderate Income Housing Benefit: 500 Persons Assisted
5	Improve accessibility	2020	2024	Non-Housing Community Development	City-wide CDBG Target Area	Improve Public Infrastructure Enhance Accessibility Improve Public Services for Priority Populations Improve Housing Access and Affordability	CDBG: \$1,000,000	Public Facility or Infrastructure Activities other than Low/Moderate Income Housing Benefit: 5000 Persons Assisted
6	Construct/upgrade public facilities	2020	2024	Non-Housing Community Development	City-wide CDBG Target Area	Improve Public Infrastructure Enhance Accessibility	CDBG: \$300,000	Public Facility or Infrastructure Activities other than Low/Moderate Income Housing Benefit: 500 Persons Assisted
7	Effectively administer CDBG program	2020	2024	Program Administration	City-wide	Building Healthy Communities Improve Public Services for Priority Populations	CDBG: \$600,000	

Sort Order	Goal Name	Start Year	End Year	Category	Geographic Area	Needs Addressed	Funding	Goal Outcome Indicator
8	Affirmatively further fair housing	2020	2024	Affordable Housing	City-wide CDBG Target Area	Building Healthy Communities Improve Housing Access and Affordability	CDBG: \$100,000	Public service activities other than Low/Moderate Income Housing Benefit: 800 Persons Assisted

Table 18 – Goals Summary

Goal Descriptions

1	Goal Name	Foster affordable housing
	Goal Description	The city will foster affordable housing by constructing new units where possible, rehabilitating units in need of health-related, and safety repairs, and providing down payment assistance to low-income homebuyers.
2	Goal Name	Services for people experiencing homelessness
	Goal Description	The city plans to support homeless housing efforts where possible and continue to fund homeless prevention and mental health services.
3	Goal Name	Services for seniors and youth
	Goal Description	The city will continue to support programs and services that focus on senior and youth populations. Services include affordable child-care, counseling, after-school programs, youth development, meal programs, and elderly-care programs that include social activities and affordable transportation.
4	Goal Name	Provide public services responsive to current need
	Goal Description	The city will continue to support public service programs and efforts based on community needs and changing public needs. Additionally, the city will continue to support public service providers that offer essential support services to low- and moderate-income individuals.

5	Goal Name	Improve accessibility
	Goal Description	The city will fund public works projects to provide residents with accessible routes in the city.
6	Goal Name	Construct/upgrade public facilities
	Goal Description	The city will continue to construct or upgrade public facilities, such as infrastructure improvements, improvements to parks and recreation facilities, and other public improvements as necessary.
7	Goal Name	Effectively administer CDBG program
	Goal Description	The city will continue to prioritize effectively administering the CDBG program. CDBG funds will be used for the betterment of the Citrus Heights community.
8	Goal Name	Affirmatively further fair housing
	Goal Description	The city will continue to fund programs that seek to maintain fair housing through fair housing testing, housing counseling, and anti-discriminatory efforts.

Estimate the number of extremely low-income, low-income, and moderate-income families to whom the jurisdiction will provide affordable housing as defined by HOME 91.315(b)(2)

The city has approximately 1,133 units that have been assisted with federal, state, and local programs. In the City of Citrus Heights, there are ten affordable housing complexes, six of which are public housing units and four, which are conventional housing units that are privately owned. The six public housing complexes contain 90 units and the privately owned complexes contain 650 units for 740 affordable housing units. Complexes are available to low-income renters, while some are available to both very low- and low-income renters. The apartment complexes are funded through a variety of sources: Low-Income Housing Tax Credit (LIHTC) properties, county multifamily funds, and HUD Section 202 funds. The city has two senior complexes, Normandy Park and Vintage Oaks, with 317 units available to Citrus Heights' population ages 55 and over. SHRA also offers vouchers that can be used at multiple sites. In addition, the city will offer to the Housing Repair and FTHB programs to residents.

SP-65 Lead-based Paint Hazards - 91.415, 91.215(i)

Actions to address LBP hazards and increase access to housing without LBP hazards

While most housing units were built prior to 1978, an estimated 5,312 units that are occupied by target-income households may contain lead-based paint. The city will provide lead-abatement assistance for residential units through its Housing Repair Program. The city contracts with a third-party firm to administer its Housing Repair Program. Independent contractors are also employed to develop the appropriate lead hazard reduction plans and or abatement scopes of work. The programs will comply with the Residential Lead Based Paint Hazard Reduction Act of 1992 (Title X) and subsequent changes in September 1999. The procedures regarding lead-based paint in all repair programs will include notification and identification.

The City of Citrus Heights' goal is to accurately assess and quantify risks associated with childhood lead poisoning and implement effective measures to significantly reduce or eliminate such risks. Local efforts will be directed at achieving the following major tasks or objectives:

- Increase coordination between relevant public health, environmental, educational, and housing programs;
- Achieve greater awareness and participation by the private sector in addressing lead-based paint problems;
- Advocate for increased federal and state funding and other support for lead-based paint testing, abatement, and public information activities; and
- Comply with Title X requirements in all city-funded housing programs.

How are the actions listed above integrated into housing policies and procedures?

The city's housing program guidelines include specific policies related to testing and abatement. When lead-based paint is present or presumed to be present, lead-safe work practices are required. In addition, all of the city's loan agreements for new projects prohibit the use of any lead-based paint.

SP-70 Anti-Poverty Strategy - 91.415, 91.215(j)

Jurisdiction Goals, Programs and Policies for reducing the number of Poverty-Level Families

The city's anti-poverty strategy is based on providing a range of employment opportunities and supportive services aimed at enabling those in poverty to move into the workforce or obtain benefits to which they are entitled (e.g., social security, disability). During the 2019 program year, these activities will include SSHH counseling services and Crossroads Diversified unemployment services. The city will also continue to support activities that preserve and expand the supply of housing affordable to low-income households. Also, in the community survey, respondents prioritized the following services: mental health 13 percent, senior services 10 percent, domestic violence and preventions 8 percent, employment training 7 percent, substance abuse services 7 percent, youth services 7 percent. In the stakeholder survey results, several stakeholders identified services for youth such as job training, and reliable transportation for their clients as the greatest needs that clients face.

How are the Jurisdiction poverty reducing goals, programs, and policies coordinated with this affordable housing plan

Citrus Heights' goals include several that align with the goal of reducing poverty, including the creation of affordable housing where possible, expanding housing services for homeless people, and providing job training and social services. The city plans to fund specific activities that address each of these goals during the Consolidated Plan period.

The city works to strengthen its institutional structure to maximize the outcomes on addressing affordable housing issues. The Housing and Grants Division operates in close consultation with the city's advisory committees, the Citrus Heights Collaborative, the General Services Department, and the director of the Community and Economic Development Department.

Also, the city works in coordination with public and private housing and social services agencies. The city will continue to collaborate with neighboring jurisdictions, such as the County and the City of Sacramento and the SHRA, to address the regional issues that affect the needs of low-income persons and special needs populations. The city will also continue to work with many of the local nonprofits that provide a range of services to low-income Citrus Heights residents, as well as with other entitlement jurisdictions in Sacramento County.

For example, to help households in poverty meet their basic needs, the Citrus Heights Homeless Assistance Resource Team (HART) organizes local programs and organizations offering services, including food banks, meals, animal resources, clothing donations, and laundry services, to increase awareness and accessibility to local resources. To help youth, the city also funds projects such as the Sayonara After-School Program to provide tutoring and improve quality of life for Citrus Heights youth. To help keep low-income people in their homes by avoiding homelessness, the city funds

Sacramento Self-Help Housing (Housing Counseling) which provides housing counseling services to households at-risk of becoming homeless.</p>

SP-80 Monitoring - 91.230

Describe the standards and procedures that the jurisdiction will use to monitor activities carried out in furtherance of the plan and will use to ensure long-term compliance with requirements of the programs involved, including minority business outreach and the comprehensive planning requirements

The City of Citrus Heights has developed a monitoring system to ensure that the activities carried out in furtherance of the Consolidated Plan are done so in a timely manner in accordance with federal monitoring requirements of 24 CFR 570.501(B) and 2 CFR Part 200 and all other applicable laws, regulations, policies, and sound management and accounting practices. The objectives of monitoring are:

To assure the subrecipients are carrying out their program/project as described;

To assure that subrecipients are implementing the program/project in a timely manner;

To assure that subrecipients are assessing costs to the program/project which are eligible under CDBG regulations and the contract;

To assure that subrecipients are conforming with other applicable laws, regulations and terms of the agreement;

To assure that the program/project is operating in a manner that minimizes the opportunity for fraud, waste, and mismanagement;

To assure that subrecipients have the capacity to carry out the approved project/program; and

To assure that subrecipients are carrying out their program/project as described in their agreement.

The city informs subrecipients of their obligations under their subrecipient agreement at the beginning of each program year; reviews quarterly reports submitted by subrecipients; provides feedback regarding compliance; and conducts annual desk assessment of subrecipient activities. The city also recommends that all subrecipients read "Playing by the Rules" guide produced by HUD. The city provides technical assistance to each Subrecipient throughout the year as needed. The city will track and report on its progress toward meeting its housing and community development goals, and report these on an annual basis in the CAPER.

Expected Resources

AP-15 Expected Resources - 91.420(b), 91.220(c)(1,2)

Introduction

The city of Citrus Heights is a CDBG entitlement (EN) jurisdiction. Citrus Heights received a 2020 EN grant amount of \$639,115 and a CDBG-CV Round 1 allocation of \$376,028. The city estimates receiving \$50,000 in program income. To date, we have received \$32,103 in loan repayments and we have \$727,778 in rollover funding from prior years. The expected amount available for the remainder of the Con Plan period assumes the city will receive \$600,000 in entitlement funds annually. Additionally, Citrus Heights is in a regional HOME Consortium, administered by SHRA. The city works closely with SHRA to allocate HOME funds that are available for use in Citrus Heights.

Along with these federal entitlements, Citrus Heights works with SHRA, the County, and developers when possible to utilize Low-Income Housing Tax Credits, the State of California's housing programs, and other available opportunities to support housing development. The County of Sacramento received a No Place Like Home grant for the Sunrise Pointe Apartments in Citrus Heights. This project has been a regional collaboration with various funding sources.

On March 27, 2020, Coronavirus Aid, Relief, and Economic Security Act (CARES), Public Law 116-136 was signed by the President of the United States. Funding allocations are in table below.

The Anticipated Resources Matrix outlines each of these funds, expected amounts available in Year 1 and a projection of resources between FY 2021 and FY 2024, as well as a list of eligible uses of funds per HUD regulations. The amounts include funds subject to administrative caps, which will not be spent on programming identified in the Goals section of SP-45. These include city staff salary and fringe benefits, as well as other planning and administrative activities.

Other resources that may be employed include funds provided under other HUD programs, grants from the Federal Emergency Management Agency, State of California Department of Housing and Community Development, federal tax credits and mortgage credit certificates, City

of Citrus Heights General Funds, and other federal or state grant programs.

One of the main obstacles to meeting community needs is inadequate resources for programs that could address these needs. During the period of time leading up to this Consolidated Plan, the State of California and local governments experienced a dearth of resources to finance affordable housing and affordable housing production has lagged as a result.

For the period of this current Consolidated Plan, the State of California has dedicated additional resources and we expect that the production volume will increase. This will be made possible by a \$4 billion dollar statewide affordable housing bond approved by the voters in November of 2018 and a new, ongoing source of affordable housing approved by the legislature in 2017 (Senate Bill 2). The State has significantly increased the amount of resources available to support housing for homeless and mentally ill; however, most of those resources are allocated to the County and Continuum of Care agencies and the city does not receive any funding directly.

Most of the new resources require affordable housing developers to compete with one another for a limited amount of funding. The city will continue to work with developers and regional partners to demonstrate local support and financial leverage to place developments in the best

position to obtain these limited dollars.

Anticipated Resources

Program	Source of Funds	Uses of Funds	Expected Amount Available Year 1				Expected Amount Available Remainder of ConPlan \$	Narrative Description
			Annual Allocation: \$	Program Income: \$	Prior Year Resources: \$	Total: \$		
CDBG	public - federal	Acquisition Admin and Planning Economic Development Housing Public Improvements Public Services	639,115	50,000	727,778	1,416,893	2,600,000	The city is a CDBG entitlement jurisdiction. This includes a 2020 grant amount of \$639,213 and a CDBG-CV Phase I allocation of \$376,028. To date, we have received \$32,103 in loan repayments and we have \$727,778 in rollover funding from prior years. The expected amount assumes the city will receive \$600,000 in entitlement funds annually.
General Fund	public - local	Public Services	134,100	0	0	134,100	536,000	The city allocates General fund money to nonprofits to benefit public services.

Program	Source of Funds	Uses of Funds	Expected Amount Available Year 1				Expected Amount Available Remainder of ConPlan \$	Narrative Description
			Annual Allocation: \$	Program Income: \$	Prior Year Resources: \$	Total: \$		
Other	public - federal	Acquisition Housing	2,280,000	0	0	2,280,000	0	The city uses HOME funds as part of the HOME Consortium. On January 23, 2020, the Citrus Heights City Council approved an allocation of \$2,280,000 of the HOME funds reserved for the City of Citrus Heights to be used for the Sunrise Pointe Apartments. The 46-unit permanent supportive housing project has been approved and is waiting for federal tax credits to proceed with construction. SHRA approved to advance the city its future allocation of HOME funds in order to provide a competitive local match to the project and increase the chances it receives federal tax credits.
Other	public - federal	Admin and Planning Public Services	920,226	0	0	920,226	0	The city received a CDBG-CV 1 award of \$376,028 to prepare for and respond to the impacts of COVID-19.
Other	public - state	Housing	50,000	0	0	50,000	200,000	The city receives program income from prior years Calhome grants.

Table 19 - Expected Resources – Priority Table

Explain how federal funds will leverage those additional resources (private, state and local funds), including a description of how matching requirements will be satisfied

There is no federal requirement for the city to match CDBG funds with other non-federal program resources. In Fiscal Year 2019-20, the city allocated \$130,000 in General Fund revenue to nonprofits providing a community service in Citrus Heights, in addition to the CDBG investments. All capital improvement projects funded with CDBG have been able to take advantage of other federal and state funding sources, including Measure A funds and Drainage funds, to complete design, engineering, and construction work that exceeds the city's CDBG allocation for these projects. With respect to public services, the city requires all subrecipients to identify other resources that will be utilized during the program year to operate and implement CDBG-supportive activities. It is the city's intent to ensure that adequate non-federal and private funds are available, thus minimizing the dependence on federal funds.

If appropriate, describe publically owned land or property located within the jurisdiction that may be used to address the needs identified in the plan

Some projects that may help address the needs identified in the plan include the Sayonara affordable housing project, which is currently in the pre-planning phase. Previously, the city purchased and demolished 15 complexes and 1 eight-plex on the street with the intent of replacing them with a new affordable housing development. In that same neighborhood, on Sayonara Drive, the Citrus Heights Children and Youth Center was recently funded completed with CDBG funds in the 2010-2014 Consolidated Plan period.

Citrus Heights currently contains 90 units of public housing, which is generally in good condition. SHRA, which oversees public housing, has policies in place to inspect units, perform regular maintenance, and offer support to households attempting to achieve self-sufficiency. This helps provide affordable housing to the low-income populations in the community.

The city has made efforts to 1) take the lead in the design, construction, and funding of public improvements; 2) improve the appearance of commercial districts; and 3) stimulate private investment. These efforts have included streetscape enhancement projects and a campaign to revitalize the Auburn Boulevard Specific Plan Area. The campaign included assisting business owners with improving their customer base, one-on-one consulting, and a variety of grants and fee waivers to assist the property and business owners along the Auburn Boulevard corridor.

Discussion

Affordable housing developers and the city must be as creative as possible to find other sources of funding from state programs targeted to low- and moderate-income households, other non-HUD federal sources, private loan funds, federal and state tax-credits, and local funding. In order to win tax credits, multiple sources of funding are required and the minimum matching requirements are typically far exceeded in each project.

These resources include funds provided under other HUD programs, the Economic Development Administration, the Federal Emergency Management Agency, federal, state and local energy efficiency programs, federal and State tax credits and mortgage credit certificates, and other federal grant programs as may be identified.

Annual Goals and Objectives

AP-20 Annual Goals and Objectives - 91.420, 91.220(c)(3)&(e)

Goals Summary Information

Sort Order	Goal Name	Start Year	End Year	Category	Geographic Area	Needs Addressed	Funding	Goal Outcome Indicator
1	Foster affordable housing	2020	2024	Affordable Housing	City-wide CDBG Target Area	Building Healthy Communities Improve Housing Access and Affordability	CDBG: \$352,266	Homeowner Housing Rehabilitated: 8 Household Housing Unit Direct Financial Assistance to Homebuyers: 2 Households Assisted
2	Services for people experiencing homelessness	2020	2024	Homeless	City-wide	Improve Public Services for Priority Populations	CDBG: \$141,000 General Fund: \$35,400	Homelessness Prevention: 312 Persons Assisted
3	Services for seniors and youth	2020	2024	Non-Homeless Special Needs	City-wide	Building Healthy Communities Improve Public Services for Priority Populations	CDBG: \$123,422 General Fund: \$93,000	Public service activities other than Low/Moderate Income Housing Benefit: 585 Persons Assisted

Sort Order	Goal Name	Start Year	End Year	Category	Geographic Area	Needs Addressed	Funding	Goal Outcome Indicator
4	Provide public services responsive to current need	2020	2024	Non-Homeless Special Needs	City-wide	Improve Public Services for Priority Populations	CDBG: \$74,430 General Fund: \$5,715	Public service activities other than Low/Moderate Income Housing Benefit: 8574 Persons Assisted
5	Improve accessibility	2020	2024	Non-Housing Community Development	City-wide CDBG Target Area	Improve Public Infrastructure Enhance Accessibility	CDBG: \$585,000	Public Facility or Infrastructure Activities other than Low/Moderate Income Housing Benefit: 1000 Persons Assisted
6	Construct/upgrade public facilities	2020	2024	Non-Housing Community Development	CDBG Target Area		CDBG: \$200,000	Public Facility or Infrastructure Activities other than Low/Moderate Income Housing Benefit: 500 Persons Assisted
7	Effectively administer CDBG program	2020	2024	Program Administration	City-wide	Building Healthy Communities Improve Housing Access and Affordability	CDBG: \$204,332	
8	Affirmatively further fair housing	2020	2024	Affordable Housing	City-wide CDBG Target Area	Building Healthy Communities Improve Public Services for Priority Populations	CDBG: \$29,540	Public service activities other than Low/Moderate Income Housing Benefit: 485 Persons Assisted

Table 20 – Goals Summary

Goal Descriptions

1	Goal Name	Foster affordable housing
	Goal Description	The city will foster affordable housing by rehabilitating units in need of health-related , and safety repairs, and provide down payment assistance to low-income homebuyers.
2	Goal Name	Services for people experiencing homelessness
	Goal Description	The city plans to support homeless housing efforts where possible and continue to fund the Housing Counseling and Navigator Program.
3	Goal Name	Services for seniors and youth
	Goal Description	The city will continue to support programs and services that focus on senior and youth populations. Services include the Meals on Wheels Senior Program and Sayonara after-school program.
4	Goal Name	Provide public services responsive to current need
	Goal Description	The city will continue to support public service programs and efforts based on community needs and changing public needs. Additionally, the city will continue to support essential services including domestic violence and an emergency food closet to low- and moderate-income persons.
5	Goal Name	Improve accessibility
	Goal Description	The city will fund public works projects to provide residents with accessible routes in the city.
6	Goal Name	Construct/upgrade public facilities
	Goal Description	The city will continue to provide improvements to public facilities, including park and recreational facility improvements.

7	Goal Name	Effectively administer CDBG program
	Goal Description	The city will continue to prioritize effectively by administering the CDBG program. CDBG funds will be used for the betterment of the City of Citrus Heights.
8	Goal Name	Affirmatively further fair housing
	Goal Description	The city will continue to fund programs that seek to maintain fair housing through the Renters Helpline. The services provided will include fair housing testing, housing counseling, and anti-discriminatory efforts.

AP-35 Projects - 91.420, 91.220(d)

Introduction

The table below summarizes the city's 2020 Community Development Block Grant funding as well as the city's CARES Act CDBG-CV Round 1 and first phase of the city's CDBG-CV Round 3 funding. On September 1, 2019, the Quality of Life Committee recommended funding the city's Program Year 2020 CDBG EN-funded projects. On October 10, 2019, the City Council heard public comment on the draft 2020 Action Plan, on October 24, 2019; the City Council adopted the final 2020 Action Plan. The city's final, revised 2020 EN grant amount was \$639,115.

On April 2, 2020, the city received a special \$376,028 allocation of CDBG-CV funds authorized by the CARES Act to respond to the impacts of COVID-19. These funds have been incorporated into the city's Program Year 2020 Action Plan. The initial CDBG-CV allocations were approved by the City Council on April 23, 2020. Residents were provided with a 7-day notice of the meeting and the opportunity to comment on the recommended funding allocations. The city held a public review and comment period from May 23-28, 2020, and a public hearing on May 28, 2020, to approve the first the CARES Act Amendment to the city's PY 2020 Action Plan with the first phase of funding allocations. On August 13, 2020, the city held a public hearing and allocated the city's remaining CDBG-CV Round 1 funding as part of the CARES Act Amendment No. 2. The draft CARES Act Amendment with the CDBG-CV allocations was made available for public review and comment consistent with the city's Citizen Participation Plan.

The proposed CARES Act Amendment No. 3 re-allocates the unused funding from CDBG-CV Round 1 from the Great Plates Delivered Program local match to the Supplemental Navigator Fund and allocates the first phase of the city's CDBG-CV Round 3 funding. The draft CARES Act Amendment No. 3 was made available for public review in accordance with the city's Citizen Participation Plan. The City Council will hold a public hearing to consider adoption of the Substantial Amendment to the city's 2020-2024 Consolidated Plan and 2020 Action Plan on April 22, 2021.

#	Project Name
1	Public Services (2020)
2	Public Infrastructure Projects
3	Housing Preservation Programs
4	Planning & Administration (2020)
5	Revolving Loan Fund (Ongoing)
6	Public Services (CDBG-CV 1)
7	Planning and Administration (CDBG-CV 1)
8	Public Services (CDBG-CV 3)
9	Planning and Administration (CDBG-CV 3)

Table 21 – Project Information

Describe the reasons for allocation priorities and any obstacles to addressing underserved

needs

In making project recommendations, consideration is given based on a variety of thresholds that projects must meet to comply with CDBG objectives, including meeting one of the national objectives and addressing one of the community priorities set out in the Consolidated Plan. Staff took into account activity need and justification, cost reasonableness and effectiveness, activity management and implementation, experience with similar activities, past performance, leveraged funds, and completeness of the application.

While there are several constraints to meeting the needs of low-income residents, the primary obstacle is the lack of funding to fully address all needs. The economic challenges facing the nation have forced many nonprofits to cut services at a time when governmental entities and others are least able to provide them. In response to the level of need, the city has maintained its commitment to providing local funding to nonprofits, even though budget cuts that have been required in a number of areas.

A second obstacle to meeting underserved needs is the location of many available services is in the City of Sacramento. Citrus Heights works closely with the regional transit agencies to improve access, and there are several daily public transportation linkages between Citrus Heights and downtown Sacramento.

AP-38 Project Summary

Project Summary Information

1	Project Name	Public Services (2020)
	Target Area	City-wide
	Goals Supported	Services for people experiencing homelessness Services for seniors and youth Provide public services responsive to current need Affirmatively further fair housing
	Needs Addressed	Building Healthy Communities Improve Public Services for Priority Populations Expand Economic Opportunities
	Funding	CDBG: \$90,000 General Fund: \$134,100
	Description	Provide public services to low- and moderate-income households and individuals, including services for seniors, persons with disabilities, and homeless persons, youth, and families.
	Target Date	12/31/2020
	Estimate the number and type of families that will benefit from the proposed activities	About 9,021 individuals and 497 households will benefit from the proposed activities, including seniors, youth, persons with disabilities, persons experiencing homeless, and low-income households.
	Location Description	Citywide to eligible participants

Planned Activities	<p>PY 2020 EN Activities:</p> <p><u>Campus Life Connection:</u></p> <p>Matrix Code - 05D; National Objective - LMC</p> <p>Description - Operate an after-school center that provides low-income youth with recreational activities. Activities include mentoring, tutoring, educational games, technology center, and daily nutritious meals to an estimated 105 youth. Approximately 16% of available public service funding, estimated at \$14,430.</p> <p><u>Meals on Wheels:</u></p> <p>Matrix Code - 05A; National Objective - LMC</p> <p>Description - Provide a balanced, nutritious meals Mon-Fri, 250 days per year to an estimated 280 seniors either dining at Rusch Park Community Center five days a week or by delivery to homebound seniors. Approximately 16% of available public service funding, estimated at \$14,000.</p> <p><u>Sacramento Self-Help Housing (Housing Counseling/Navigator):</u></p> <p>Matrix Code - 05Z; National Objective - LMC</p> <p>Description - To provide housing counseling services to approximately 100 households at-risk of becoming homeless and navigator services to approximately 112 households currently homeless. Approximately 18% of available public service funding, estimated at \$16,000.</p> <p><u>Sacramento Self-Help Housing (Renters Helpline):</u></p> <p>Matrix Code - 05K; National Objective - LMC</p> <p>Description - To provide a telephone and Internet-based "Renters Helpline" as well as counseling, dispute resolution, and fair housing services to approximately 385 households. Approximately 23% of available public service funding, estimated at \$21,140.</p> <p><u>Sunrise Christian Food Ministry:</u></p> <p>Matrix Code - 05W; National Objective - LMC</p> <p>Description - To provide ongoing emergency food to approximately 8,500 low-income and homeless persons in Citrus Heights. Approximately 16% of available public service funding, estimated at \$14,430.</p>
---------------------------	---

		<u>W.E.A.V.E.:</u> Matrix Code - 05G; National Objective - LMC Description - Operate a Violence Reduction Team (VRT) to provide response services in partnership with the CHPD to approximately 24 individuals. Approximately 11% of available public service funding estimated at \$10,000.
2	Project Name	Public Infrastructure Projects
	Target Area	CDBG Target Area
	Goals Supported	Improve accessibility Construct/upgrade public facilities
	Needs Addressed	Building Healthy Communities Improve Public Infrastructure Enhance Accessibility
	Funding	CDBG: \$785,000 General Fund: \$440,849
	Description	Complete improvements to parks, public facilities, and public infrastructure improvements located in the CDBG target area or that predominantly serve low-income households.
	Target Date	12/31/2020
	Estimate the number and type of families that will benefit from the proposed activities	Approximately 1,500 persons will receive benefits, including seniors, youth, homeless persons, and low-income households. Additionally, some neighborhoods will experience area benefits from park and public infrastructure improvements.

	Location Description	The 2020 Accessibility Improvement Project, Park and Public Facility Improvements, and Signalized Intersection Improvement Project will all take place primarily in the CDBG Target Area, which includes all census blocks with 51% or higher concentration of low- and moderate-income households. Both projects were approved in PY 2019 but did not start until PY 2020.
	Planned Activities	<p><u>1. 2019-20 Annual Accessibility and Drainage (ADA) Project (Matrix Code 03L; National Objective: LMA):</u> Complete accessibility improvements to sidewalks, such as installing missing or repairing damaged sidewalks, installing ADA-compliant pedestrian ramps, and relocating drainage inlets as necessary at various locations yet to be determined, primarily located in CDBG target areas. This project was approved in PY 2019, but did not start until PY 2020. Consists of \$339,000 + \$98,250 for AD (approved in PY 2019 and split with city general fund based on overall percentage of project cost; CDBG portion estimated @ \$491,250.</p> <p><u>2. Park and Public Facility Improvements (Matrix Code: 03F; National Objective: LMA):</u> Complete improvements to parks and public facilities located in the CDBG target area or that predominantly serve low-income households. Funding allocation is 51% of available 2020 EN capital funding @ \$215,958.92.</p> <p><u>3. Signalized Intersection Project: (Matrix Code: 03L; National Objective: LMA):</u> Complete construction of four curb ramps located at two intersections - Sunrise/Macy Plaza and Sunrise/Birdcage Center. This project will increase the pedestrian safety throughout the city. This project was approved in PY 2019 (accessibility and public facilities improvements), but did not begin until PY 2020. Consists of \$75,000 + \$18,750 for AD (approved in PY 2019 and split based on percentage of project cost). Funding allocation; estimated @ \$93,750.</p> <p>The CDBG funds will pay for the construction of 4 curb ramps on the east side of Sunrise Blvd. Two at Sunrise/Macy Plaza Drive and two at Sunrise/Birdcage Center Drive (the signalized intersection just south of Greenback Lane).</p>
3	Project Name	Housing Preservation Programs
	Target Area	City-wide
	Goals Supported	Foster affordable housing

	Needs Addressed	Building Healthy Communities
	Funding	CDBG: \$205,333
	Description	Housing repair loans, mobile home grants, and accessibility grants for low-income homeowners, and administration of these activities.
	Target Date	12/31/2020
	Estimate the number and type of families that will benefit from the proposed activities	Approximately four low- and moderate-income households will be assisted.
	Location Description	Citywide
	Planned Activities	1. Housing Rehabilitation Grants and Loans (Matrix Code: 14A; National Objective: LMH): Housing repair loans, mobile home grants, and accessibility grants for low-income homeowners. Funding allocation (49% of available 2020 EN capital funding) @ \$205,333.08.
4	Project Name	Planning & Administration (2020)
	Target Area	City-wide
	Goals Supported	Effectively administer CDBG program
	Needs Addressed	Building Healthy Communities Improve Public Services for Priority Populations
	Funding	CDBG: \$137,823
	Description	Provide general staff administration of CDBG programs and activities, including Integrated Disbursement and Information System (IDIS) administration, program setup, reporting, planning, and subrecipient training and monitoring. A National Objective designation does not apply to activities completed under this project.
	Target Date	12/31/2020

	Estimate the number and type of families that will benefit from the proposed activities	N/a
	Location Description	N/a
	Planned Activities	General Planning and Administration, funding is 20 percent of annual grant amount (\$639,115) plus 20 percent of estimated program income (\$50,000), estimated at \$137,823.
5	Project Name	Revolving Loan Fund (Ongoing)
	Target Area	City-wide
	Goals Supported	Foster affordable housing
	Needs Addressed	Building Healthy Communities
	Funding	CDBG: \$162,266
	Description	Receipts from previous CDBG-funded loans are used to make new loans. This is the current amount, plus we will add any future RL receipts.
	Target Date	12/31/2020
	Estimate the number and type of families that will benefit from the proposed activities	Approximately four low- and moderate-income households will be assisted.
	Location Description	Citywide
	Planned Activities	1. Housing Preservation Program (Matrix Code: 14A; National Objective: LMH): Housing repair loans (up to \$60,000), mobile home grants (up to \$10,000), and accessibility grants (up to \$5,000) for low-income homeowners. Funding allocation; estimated @ \$162,265.64.
	Project Name	Public Services (CDBG-CV 1)

6	Target Area	City-wide
	Goals Supported	Services for people experiencing homelessness Services for seniors and youth Provide public services responsive to current need Affirmatively further fair housing
	Needs Addressed	Building Healthy Communities Improve Public Services for Priority Populations Improve Housing Access and Affordability
	Funding	CDBG: \$300,822
	Description	Public services allocated as part of the city's CARES Act CDBG-CV funding to prepare and respond to the impacts of COVID-19.
	Target Date	12/31/2023
	Estimate the number and type of families that will benefit from the proposed activities	We estimate a total of approximately 9,224 unduplicated individuals and 75 households will benefit from the proposed activities. These families include low-income families and individuals, seniors, youth, domestic violence survivors, and people at-risk of/experiencing homelessness.
	Location Description	The activities will be provided citywide to eligible participants. Campus Life will provide services from the Sayonara Center, located at 7836 Sayonara Drive. The Sunrise Christian Food Ministry operates out of the Advent Lutheran Church property at 5901 San Juan Avenue.

Planned Activities	<p>CARES Act (CDBG-CV Round 1) Activities:</p> <p><u>Phase I Funding Allocations</u></p> <p><u>Campus Life Connection:</u> Matrix Code: 05W; National Objective: LMC</p> <p>Description: Distribute emergency food and household supplies to low-income households from the Sayonara Center five days per week. Estimated to serve 100 individuals. Funding estimate is \$15,750.</p> <p><u>Citrus Heights Navigator Program, Supplemental Navigator Fund:</u> Matrix Code: 05Q; National Objective: LMC</p> <p>Description: Supplemental fund to provide subsistence payments, such as motel and laundry vouchers to homeless individuals and security deposit assistance to help people obtain permanent housing. Estimated to serve 100 homeless individuals in Citrus Heights. <u>Funding allocation: \$216,672.40.</u> <i>Note: Funding increased as part of CARES Act Amendment No. 3.</i></p> <p><u>Sunrise Christian Food Ministry:</u> Matrix Code: 05W; National Objective: LMC</p> <p>Description: Provide emergency food and household supplies to approximately 8,500 low-income and homeless individuals in Citrus Heights. <u>Funding allocation: \$30,000.</u></p> <p><u>WEAVE:</u> Matrix Code: 05Q; National Objective: LMC</p> <p>Description: Provide domestic violence response services to Citrus Heights residents referred by the Citrus Heights Police Department or via the 24-hour support line, such as counseling, motel vouchers, food, and household supplies. Estimated to serve 24 individuals. Funding allocation: \$30,000.</p> <p><u>Phase II Funding Allocations</u></p> <p><u>Sacramento Self-Help Housing, Renter's Helpline:</u> Matrix Code: 05K; National Objective: LMC</p>
--------------------	--

		<p>Description: Provides partial funding for temporary staffing at the Renter's Helpline, which is experiencing a high call volume due to renter financial hardships and eviction questions. Funding is provided as part of a regional contract. Estimated to serve 50 individuals. Funding estimated at \$8,400.</p> <p><u>Great Plates Delivered Program: Matrix Code: 05A; National Objective: LMC</u></p> <p>NOTE: ACTIVITY CANCELLED</p> <p>Description: Provides three nutritious meals daily to eligible seniors as part of the city's local program match of 6.25 percent. Estimated to serve 500 individuals. FEEMA is no longer requiring the city to provide a local match. As a result, the proposed CARES Act Amendment No. 3 would cancel the Great Plates Delivered activity and re-allocate the 91,672.40 in CDBG-CV Round 1 funding to the Supplemental Navigator Fund.</p>
7	Project Name	Planning and Administration (CDBG-CV 1)
	Target Area	City-wide
	Goals Supported	Effectively administer CDBG program
	Needs Addressed	Building Healthy Communities Improve Public Services for Priority Populations
	Funding	CDBG: \$75,206
	Description	Planning and administration activities associated with the city's CDBG-CV Round 1 funds.
	Target Date	12/31/2023
	Estimate the number and type of families that will benefit from the proposed activities	The city's CDBG-CV Round 1 funding is expected to serve approximately 9,300 individuals.
	Location Description	The public services will be provided to eligible individuals within Citrus Heights.
	Planned Activities	Plan, administer, and monitor the city's CDBG-CV funding and associated activities. Funding is 20 percent of the city's CDBG-CV Round 1 funding award, estimated at \$75,205.60.

8	Project Name	Public Services (CDBG-CV 3)
	Target Area	City-wide
	Goals Supported	Services for people experiencing homelessness Services for seniors and youth Provide public services responsive to current need
	Needs Addressed	Building Healthy Communities Improve Public Services for Priority Populations
	Funding	CDBG-CV 3: \$112,820
	Description	Public services provided in partnership with local nonprofits to meet the increased demand resulting from the impacts of COVID-19. Proposed activities as part of the CDBG-CV 3 funding allocations include an after-school program, senior meals, distance learning support, and emergency food.
	Target Date	4/2/2023
	Estimate the number and type of families that will benefit from the proposed activities	Campus Life plans to serve approximately 75 households in the after-school program located at the Sayonara Center. Participating families are also provided with weekly food and household supplies. Meals on Wheels anticipates serving approximately 150 senior households with nutritious meals. Single Mom Strong plans to provide distance learning support to approximately 50 female-headed households. Lastly, Sunrise Christian Food Ministry anticipates providing emergency food to approximately 2,000 households. In total, the city estimates the proposed CDBG-CV Round 3 activities will serve approximately 2,275 households.
	Location Description	The proposed CDBG-CV 3 activities will take place throughout the city. The Campus Life after school program takes place at the Sayonara Center on Sayonara Drive. Meals on Wheels delivers meals to eligible participants who reside in Citrus Heights; a congregate meal is also offered at Rusch Park as permitted by County health orders.

	Planned Activities	<p>Proposed CDBG-CV Round 3 public service activities include the following:</p> <ol style="list-style-type: none"> 1. Campus Life Connection: Operational support and supplies to operate an afterschool program for low-income households at the Sayonara Center on Sayonara Drive. <u>Proposed funding allocation: \$15,000.</u> 2. Meals on Wheels: Senior meals delivered to eligible participants in Citrus Heights. <u>Proposed funding allocation: \$82,000.</u> 3. Single Mom Strong: Distance learning support for children living in single-parent households in Citrus Heights. <u>Proposed funding allocation: \$9,100.</u> 4. Sunrise Christian Food Ministry: Operational support, purchase emergency food and household supplies for low-income residents. <u>Proposed funding allocation: \$6,720.</u>
9	Project Name	Planning and Administration (CDBG-CV 3)
	Target Area	
	Goals Supported	Effectively administer CDBG program
	Needs Addressed	Building Healthy Communities Improve Public Services for Priority Populations
	Funding	CDBG-CV 3: \$22,564
	Description	Effectively administer the city's CDBG-CV Round 3 grant funds and associated activities in accordance with HUD's requirements and regulations.
	Target Date	4/2/2023
	Estimate the number and type of families that will benefit from the proposed activities	N/A
	Location Description	N/A

	Planned Activities	1. Planning and administration of the city's CDBG-CV Round 3 grant funds and associated activities. <u>Proposed funding (20% of Phase 1 CDBG-CV 3 activities): \$22,564.</u>
--	---------------------------	--

AP-50 Geographic Distribution - 91.420, 91.220(f)

Description of the geographic areas of the entitlement (including areas of low-income and minority concentration) where assistance will be directed

The geographic distribution is predicted on the nature of the activity to be funded. The majority of the activities the city plans to conduct will be offered to eligible persons citywide.

Projects are focused on the low- and moderate-income census blocks, known as the CDBG target area. Project locations are selected in coordination with the city's General Services Department that is based on resident input and timing with other planned projects in order to leverage non-CDBG funds.

Geographic Distribution

Target Area	Percentage of Funds
City-wide	15
CDBG Target Area	85

Table 22 - Geographic Distribution

Rationale for the priorities for allocating investments geographically

The city prioritizes funding primarily within the low-income neighborhoods (at least 51% of low- and moderate-income neighborhoods).

Discussion

Geographic priorities include low- and moderate-income census tracts within the City of Citrus Heights.

AP-85 Other Actions - 91.420, 91.220(k)

Introduction

While there are several constraints to meeting the needs of target-income residents, the primary obstacle is the lack of funding to fully address all needs. Economic challenges in recent years forced many nonprofits to cut services.

Actions planned to address obstacles to meeting underserved needs

The economic challenges in recent years have forced many nonprofits to cut services at time when government entities and other are least able to provide them. There is no federal requirement for the city to match CDBG funds; however, the city has maintained a commitment to provide local funding to nonprofits despite other budget cuts. As such, the city's general fund commitment for FY 19/20 is \$133,115.

Another obstacle to meeting underserved needs is the locations of many available services are in the City of Sacramento. Citrus Heights works closely with the regional transit to improve access, and there are several daily public transportation linkages between Citrus Heights and downtown Sacramento.

Another obstacle is the city's lack of vacant land available for development. The city will focus heavily on preservation of the existing housing stock through the Housing Repair Program.

Actions planned to foster and maintain affordable housing

In 2020, the city will offer several programs to foster and maintain affordable housing: The Housing Repair Program will offer low-interest loans/grants to homeowners making health and safety repairs to their homes. This program will be funded using prior-year CDBG funds and loan repayments. The city's First-Time Homebuyer Program will offer 30-year deferred loans to first-time homebuyers for down payment assistance. The city will continue to support Code Enforcement programs, such as the Rental Housing Inspection Program, that assure low-income households have a safe, decent, and appropriate place to live. The city will continue to support public services through nonprofits funded by CDBG that serve the community's youth, seniors, domestic violence victims, families, and those with special needs. The city will work with developers of the Sunrise Pointe affordable permanent supportive housing project to identify additional funding sources to fill the remaining funding gap. The city approved the project in 2018 and allocated \$2,280,000 in HOME Consortium funds to the 47-unit project planned for Sunrise Boulevard in Citrus Heights. The project is currently under consideration for federal tax credits.

Actions planned to reduce lead-based paint hazards

While most housing units were built prior to 1978, target income households that may contain lead-based paint occupy an estimated 5,312 units. The city will provide lead-abatement assistance for residential units through the Housing Repair Program. The city contracts with a third-party firm to

administer its Housing Repair Program.

Currently, the city's consultant, NeighborWorks Homeownership Center Sacramento Region, has staff qualified to evaluate lead-based paint hazards and implement lead-safe work practices. Independent contractors are employed to develop the appropriate lead hazard reduction plans and or abatement scopes of work. The programs will comply with the Residential Lead Based Paint Hazard Reduction Act of 1992 (Title X) and subsequent changes in September 1999. The procedures regarding lead-based paint in all repair programs will include notification and identification.

Actions planned to reduce the number of poverty-level families

The city's anti-poverty strategy is based on providing a range of supportive services aimed at enabling those in poverty to move into the workforce or obtain benefits to which they are entitled (social security, disability). During the 2020 program year, Sacramento Self-Help Housing will provide housing counseling and supportive services. The city will also continue to support activities that preserve and expand the supply of housing affordable to low-income households.

Actions planned to develop institutional structure

The city's Housing and Grants Division is responsible for the management, implementation, and monitoring of the Consolidated Plan documents, including the Annual Action Plan. The Housing Division works in close consultation with the city's advisory committees, Citrus Heights Collaborative, General Services Department, and with the Director of Community Development Department.

Actions planned to enhance coordination between public and private housing and social service agencies

The city will continue to work with the neighboring jurisdictions, such as the County and the City of Sacramento and Sacramento Housing Redevelopment Agency, to address the regional issues that affect the needs of low-income persons as well as special needs populations. The city will also continue to work with many of the local nonprofits that provide a range of services to low-income Citrus Heights residents. In addition, the city plans to work with other entitlement jurisdictions in the Sacramento County, City of Elk Grove, and City of Rancho Cordova to research issues of interest to all jurisdictions and to coordinate on shared subrecipient monitoring. In 2016, the city entered into a Memorandum of Understanding (MOU) with Sacramento Housing and Redevelopment Agency, Roseville Housing Authority, Housing Authority of the County of Yolo, and seven other local entitlement jurisdictions to conduct a regional fair housing assessment. The Affirmatively Furthering Housing Assessment is expected to be completed in Program Year 2020.

Program Specific Requirements

AP-90 Program Specific Requirements - 91.420, 91.220(I)(1,2,4)

Introduction

Projects planned with all CDBG funds expected to be available during the year are identified in the Projects Table. The following identifies program income that is available for use that is included in the projects to be carried out.

Community Development Block Grant Program (CDBG)

Reference 24 CFR 91.220(I)(1)

Projects planned with all CDBG funds expected to be available during the year are identified in the Projects Table. The following identifies program income that is available for use that is included in projects to be carried out.

1. The total amount of program income that will have been received before the start of the next program year and that has not yet been reprogrammed	0
2. The amount of proceeds from section 108 loan guarantees that will be used during the year to address the priority needs and specific objectives identified in the grantee's strategic plan.	0
3. The amount of surplus funds from urban renewal settlements	0
4. The amount of any grant funds returned to the line of credit for which the planned use has not been included in a prior statement or plan	0
5. The amount of income from float-funded activities	0
Total Program Income:	0

Other CDBG Requirements

1. The amount of urgent need activities	0
2. The estimated percentage of CDBG funds that will be used for activities that benefit persons of low and moderate income. Overall Benefit - A consecutive period of one, two or three years may be used to determine that a minimum overall benefit of 70% of CDBG funds is used to benefit persons of low and moderate income. Specify the years covered that include this Annual Action Plan.	70.00%

Discussion

The city's loan payments are used for the city's Revolving Loan Fund (80%). The remaining 20% of program income received is used for costs in accordance with CDBG program regulations.